

May 2024

HOUNSLOW CHARACTER, SUSTAINABILITY AND DESIGN CODES SPD

PART B9 BRENTFORD

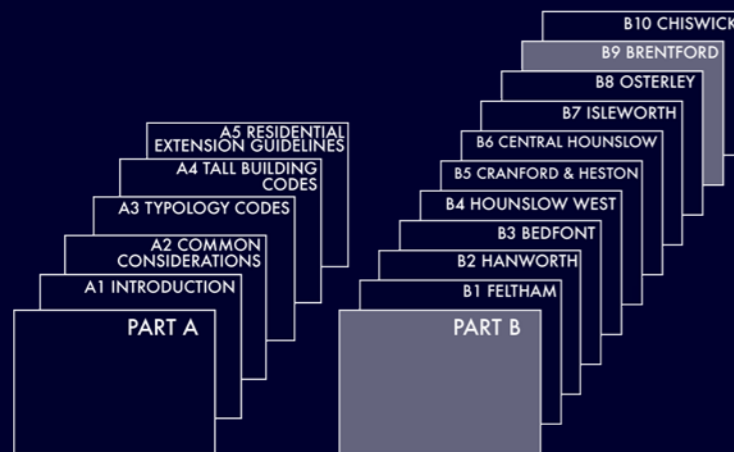
PLACES

Document structure

This Character, Sustainability and Design Codes SPD is comprised of the following documents;

- Part A: Introduction & Design Codes
- Part B: Places

These are individual files that must be opened separately to view. Use this diagram to help navigate the structure of the Character, Sustainability and Design Codes SPD.



FELTHAM

HANWORTH

BEDFONT

HOUNSLOW WEST

HESTON AND CRANFORD

CENTRAL HOUNSLOW

ISLEWORTH

OSTERLEY

BRENTFORD

CHISWICK

* This document is **Brentford**
To view other places please open the relevant documents as named above.

Introducing Hounslow's places

Neighbourhoods and Places

- B9.1 This section, Part B, explores character at a scale best understood by local people - at the neighbourhood and place level. The Borough is comprised of a series of places and neighbourhoods which each have a subtle, or sometimes distinct, character of their own. The map overleaf shows an interpretation of Hounslow's neighbourhoods. It has been developed using the previous study areas set out in the 2014 characterisation, together with local residents.
- B9.2 Identifying where neighbourhoods begin and end is inherently subjective - there will be overlaps and blurred edges between boundaries, even disagreements, as places in the Borough mean different things to different people. Influencing factors like physical infrastructure (roads, railway lines), natural assets (waterways, open spaces), local parades and centres, ward boundaries, can all help to inform the drawing of neighbourhoods that make sense. The degree of consistency or diversity of different aspects, like a particular typology or ethnic group, is also key to how neighbourhoods are perceived.
- B9.3 The purpose of dividing the Borough into these smaller areas is to provide detail on the special qualities and specific challenges for local people in each neighbourhood. This will help to ensure future growth is fairer, greener and more inclusive, and will help shape policies to ensure that the existing identity and character of neighbourhoods is a key consideration. The 37 neighbourhoods have been grouped into ten larger places. The neighbourhoods have been grouped by similarities in character and sense of place informed by: discussions at community

workshops to understand Hounslow's diverse communities; the mapping of physical assets such as topography, landscape and urban morphology; the historic evolution of each area; analysis of land use and housing typologies and their future growth direction.

Structure of Part B - Places

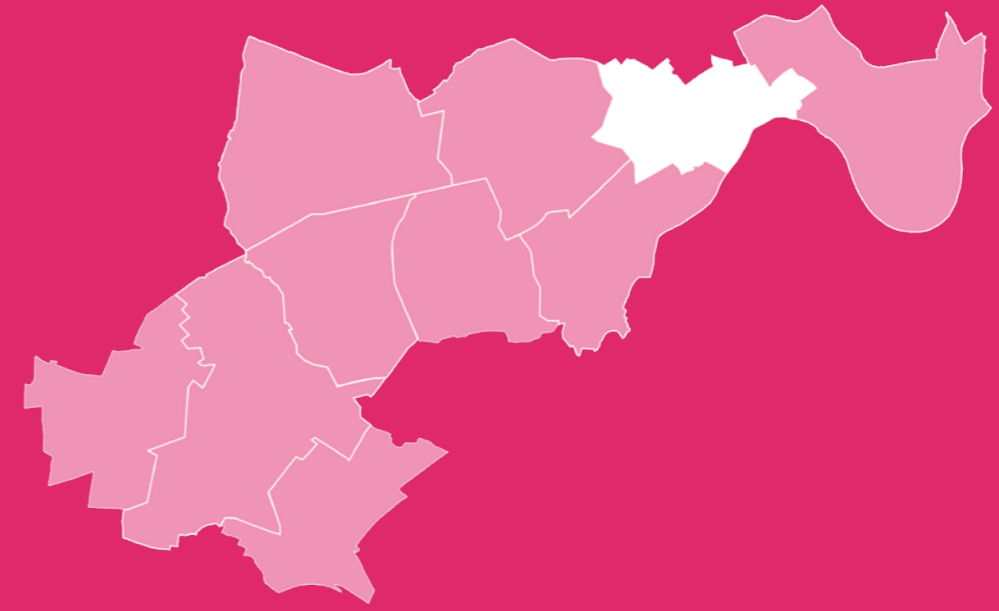
B9.4 Part B describes the ten places in the Borough. This chapter has four sections:

1. **Place-level analysis** - this section sets out the baseline analysis for the place and the vision for the place, which has been shaped by the contribution of local people through an online survey and workshops, and have been developed by considering:
 - What are the area's key assets that people love and that should be celebrated and conserved?
 - What are the current social, economic, health and environmental issues of a particular area that we want to address?
 - What opportunities for growth exist in this area and where? This includes thinking about planned investment in infrastructure, Council aspirations for growth and wider trends e.g. changing role of high streets, shift to active travel informed by existing or proposed transport hubs/links/interchanges.
2. **Neighbourhoods** - this is design guidance that sets out the special qualities for each neighbourhood in turn, and what that means for development in the area. Each neighbourhood page includes photos, key materials and features, the distribution of typologies and where possible, quotes from

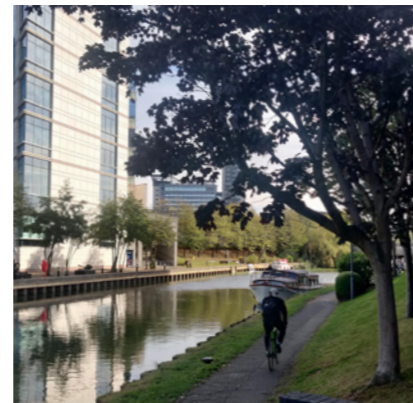
local people to help convey what makes each neighbourhood unique.

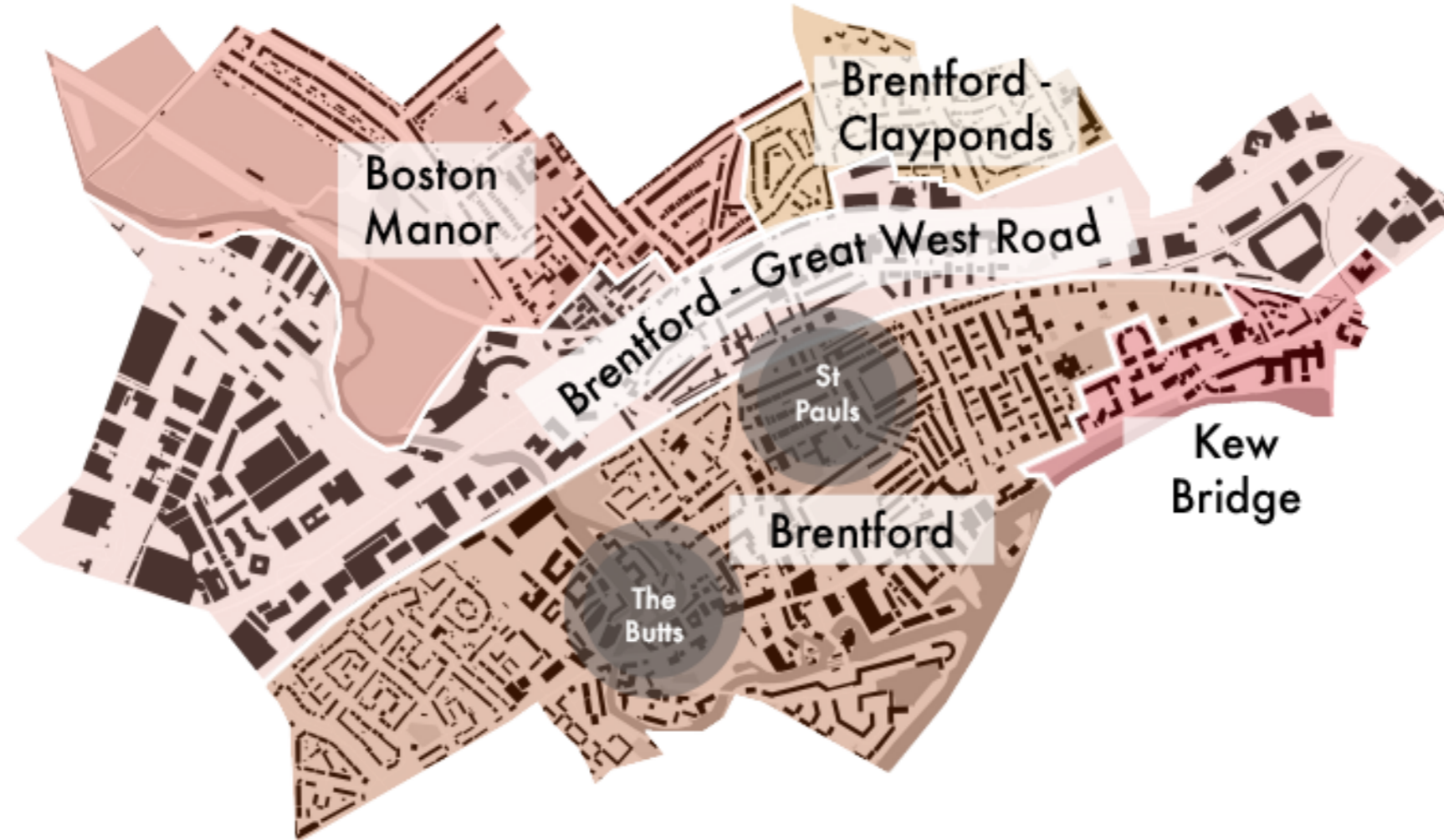
3. **Sites** - this section draws on the design codes set out in Part A and for the Neighbourhoods to provide design codes at a site-specific scale on key sites identified by the Council.
4. **Town Centre** - this section sets out specific design guidance for the town centre, drawing on the town centre visions and masterplans, to deliver design improvements in the town centre.

BRENTFORD



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Towards a greener, healthier, cleaner, safer, thriving and liveable... Brentford

- B9.5 Brentford has a high concentration of historic and natural assets and has a fine-grained diversity, from distinct conservation areas like The Butts, Boston Manor, St. Paul's and Grand Union Canal to Art Deco buildings along the Great West Road and the real variance within the town centre from new to old. This diversity will require specific context-led responses.
- B9.6 The area is well connected to inner London with high PTAL levels (4) in and around the town centre. It is a highly desirable place to live, work and spend time, and thus under pressure to accommodate growth. Brentford Town Centre needs support to evolve in order to continue to provide for user needs and preferences, and remain viable, and the quality of the Thames and Brent riverside characters need protection.
- B9.7 Interventions to support improvements in air quality need to be prioritised, with the worst air quality found along major routes such as the M4/A4, Brentford High Street and Boston Manor Road.
- B9.8 In a relatively narrow corridor, the Great West Road comes together with some of the most historic parts of the Borough, ancient routes, large landscaped parks and major

its also a highly significant industrial and employment hub, with investments such as the Creative Enterprise Zone providing an opportunity to further support industry in this area and to create better connections between the big creative businesses on the Great West Corridor and other smaller companies in and around Brentford town centre. The challenge in this area is to allow each section of the corridor to evolve in its own way, reflective of each locality's unique character, whilst enabling the whole area to work comfortably together.

B9.9 The Brentford area is focused on the town centre, as well as Kew Bridge (also known locally as Brentford East), the Great West Road piece and communities to the north around Clayponds estate and Boston Manor park and the area south of London Road. The area is home to a number of ethnic groups, predominately white British, but also significant Black African, Polish, Irish, Indian, Black Caribbean and Chinese communities (2011 Census). About a third of residents are from BAME backgrounds.



Fig B9.2 Air Quality Poorer air quality is found along the major routes

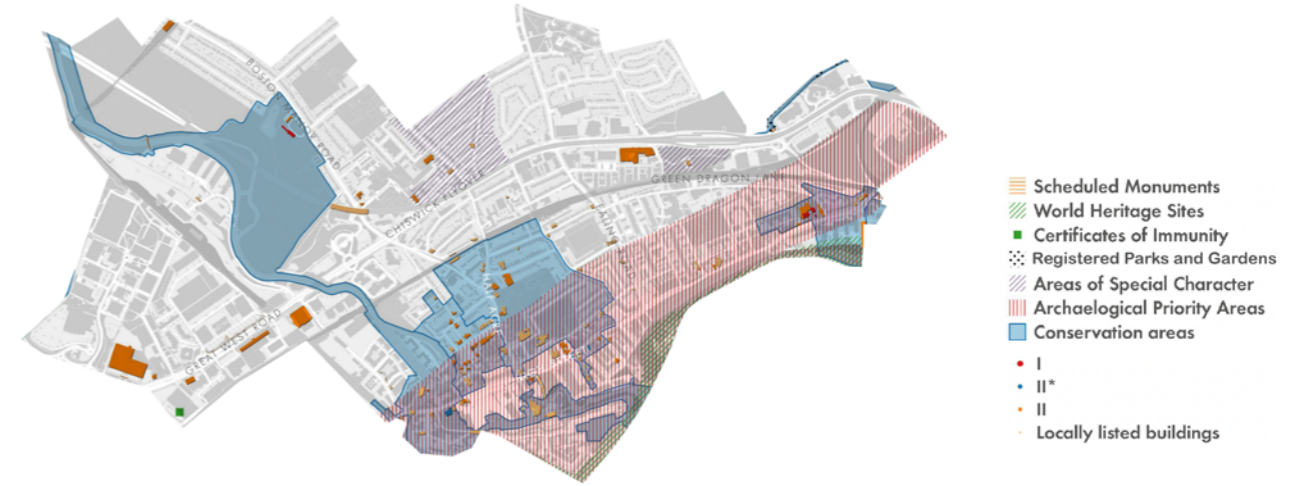


Fig B9.3 Heritage. Blue areas show conservation areas. Red hatch shows archaeological priority area.



Fig B9.4 Proximity to centres (15 min walk) Light blue shows areas within a 15 min walk from a centre (dark blue)

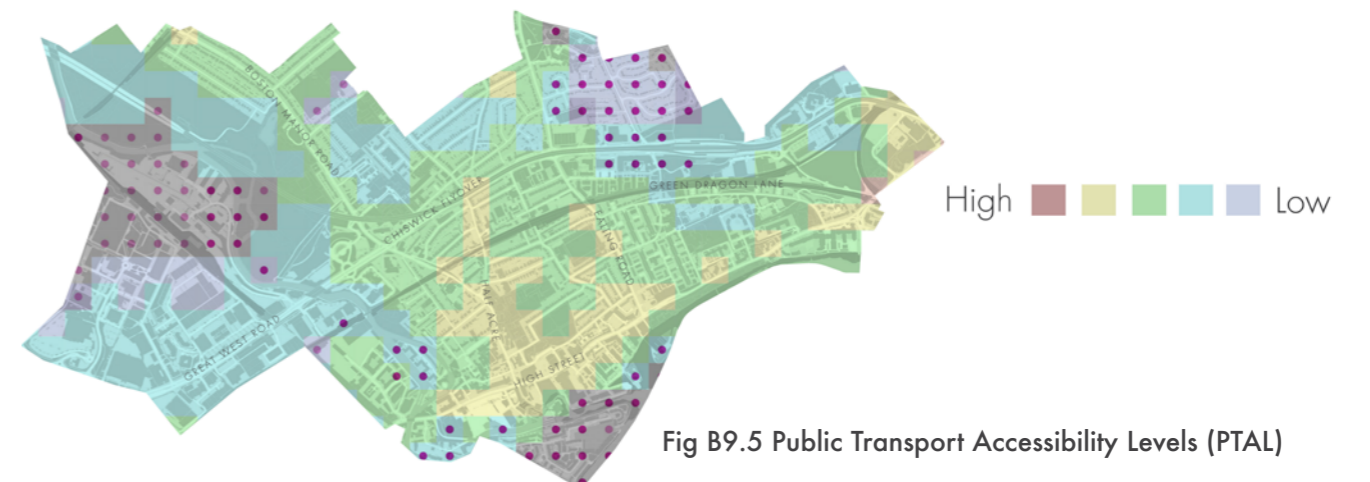


Fig B9.5 Public Transport Accessibility Levels (PTAL)

B9.10 Brentford ward is home to a much larger proportion of 20-29 year olds than elsewhere in the Borough (GLA, 2015), and is therefore one of the youngest places, with a median age of 31, compared with 34 for the Borough as a whole.

B9.11 Brentford is the second most deprived area in Hounslow and has the second largest number of unemployed people across Hounslow (1,057 – 9.6% of the working age population). There are 2,507 (16.62%) people who are income deprived, of whom, 722 are children and 544 are older people. There are, some pockets of Brentford that have a higher level of deprivation than others, including estates bordering Gunnersbury.

B9.12 This area marks the confluence of key corridors in the green infrastructure network, namely the River Thames and River Brent.

B9.13 As Figure B9.11 illustrates, there is some variance across the area in how well street blocks contribute to the existing amount of greening.

B9.14 Most residential blocks generally achieve a score of 0.5, with many in the north of the area such as the cottage-style estates well exceeding this level. In contrast it is the non-residential parts

of the area that score least well. Industrial and office locations in the west of the area have the least urban greenery, but low scores are also found on sites along Brentford High Street. Whilst it is more challenging to achieve higher scores on non-residential sites, there will need to be sensitive interventions in these broader locations to enhance their resilience. The Green Infrastructure Strategy has identified most of this area as an urban greening priority area.



Fig B9.7 Key town centre streets in Brentford such as the High Street need to see significant urban greening to ensure they are attractive and comfortable environments into the future.

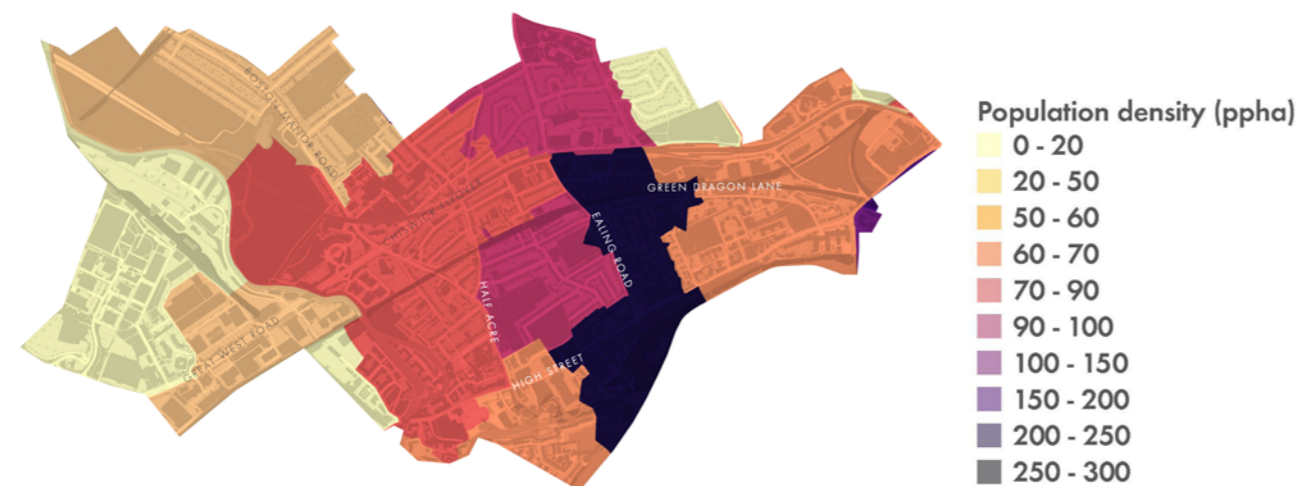


Fig B9.6 Population Density. Darker shades indicate a higher density.



Fig B9.8 Ethnic groups predominance.



Fig B9.9 Index of Multiple Deprivation Decile (where 1 is most deprived 10%). Red shows more relatively deprived areas, dark green shows least



Fig B9.10 Access to green spaces. Dark green show open space, light green shows 400m radius from green space.

B9.15 The prevailing height of existing buildings in residential areas is two to four storeys. However, along the Thames riverside, Brentford High Street and London Road some greater building heights are found, with a predominance of five to eight storey development alongside a sprinkling of taller elements over eight storeys.

B9.16 This area is undergoing significant change and so it is important to ensure the best aspects of existing typologies are reinvented to inform new development. An emphasis on mid-rise development is encouraged as a way to balance the historic fabric and underlying structure with transformative change. This may require a 50% uplift in building heights in some locations (for example, the potential for three storeys in two storey areas).

B9.17 New development in this area will range from the reuse of historic buildings for new uses through to new high density developments that support mixed communities. In the main, the

prevailing height will be maintained with uplift in focused areas of greater transformation. New developments must have particular regard to the priorities in this area which include the upgrading and sustainable, sensitive retrofit of existing built stock, protecting the character of historic buildings and the setting of historic assets, the enhancement of public realm and connectivity and the balancing of enhancing sustainable travel modes with the need for access and strategic movement. Crucially, new development should seek to bring wider enhancements that address the high levels of deprivation in parts of Brentford, raising the quality of life for existing residents.

B9.18 Most residential blocks generally achieve a score of 0.5, with many in the north of the area such as the cottage-style estates well exceeding this level. In contrast it is the non-residential parts of the area that score least well. Industrial and office locations in the west

of the area have the least urban greenery, but low scores are also found on sites along Brentford High Street. Whilst it is more challenging to achieve higher scores on non-residential sites, there will need to be sensitive interventions in these broader locations to enhance their resilience.



Fig B9.12 Approximate existing building heights.



Fig B9.14 Opportunities for high density mid-rise typologies which can sensitively manage the transition between densities in this area are to be encouraged. Precedent example: Hafer Road © Peter Barber Architects

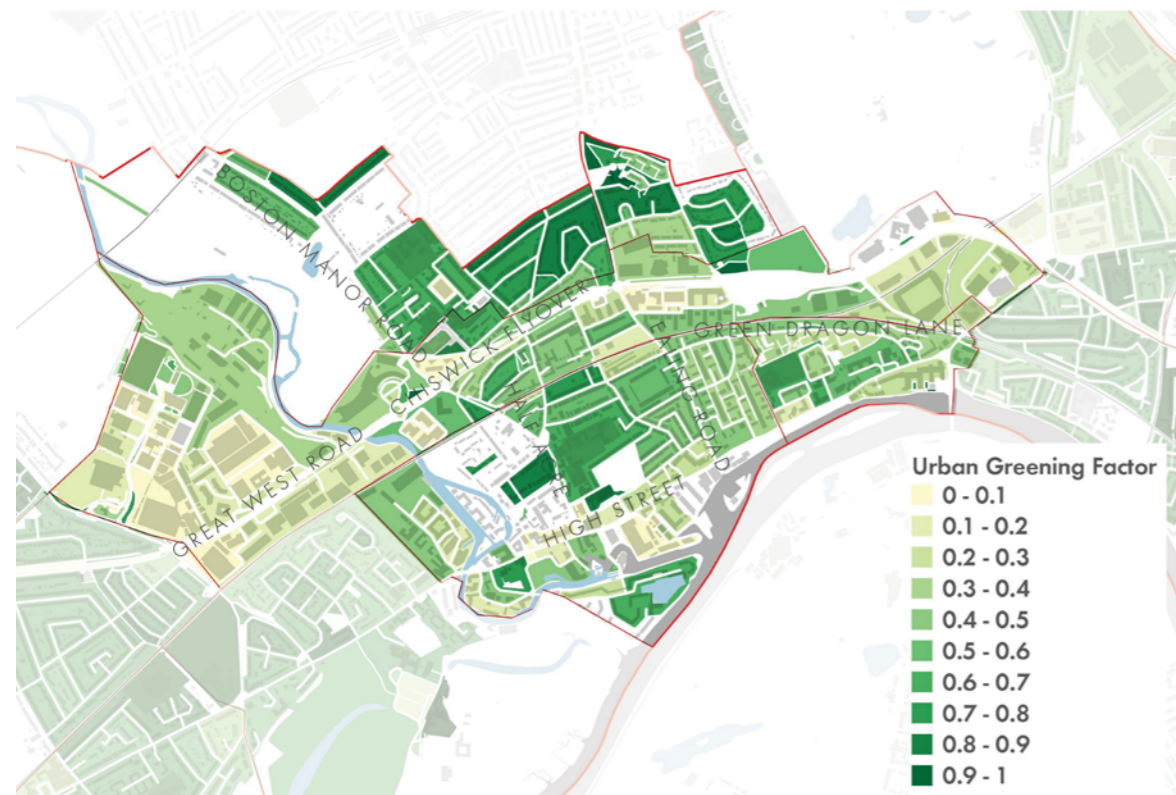
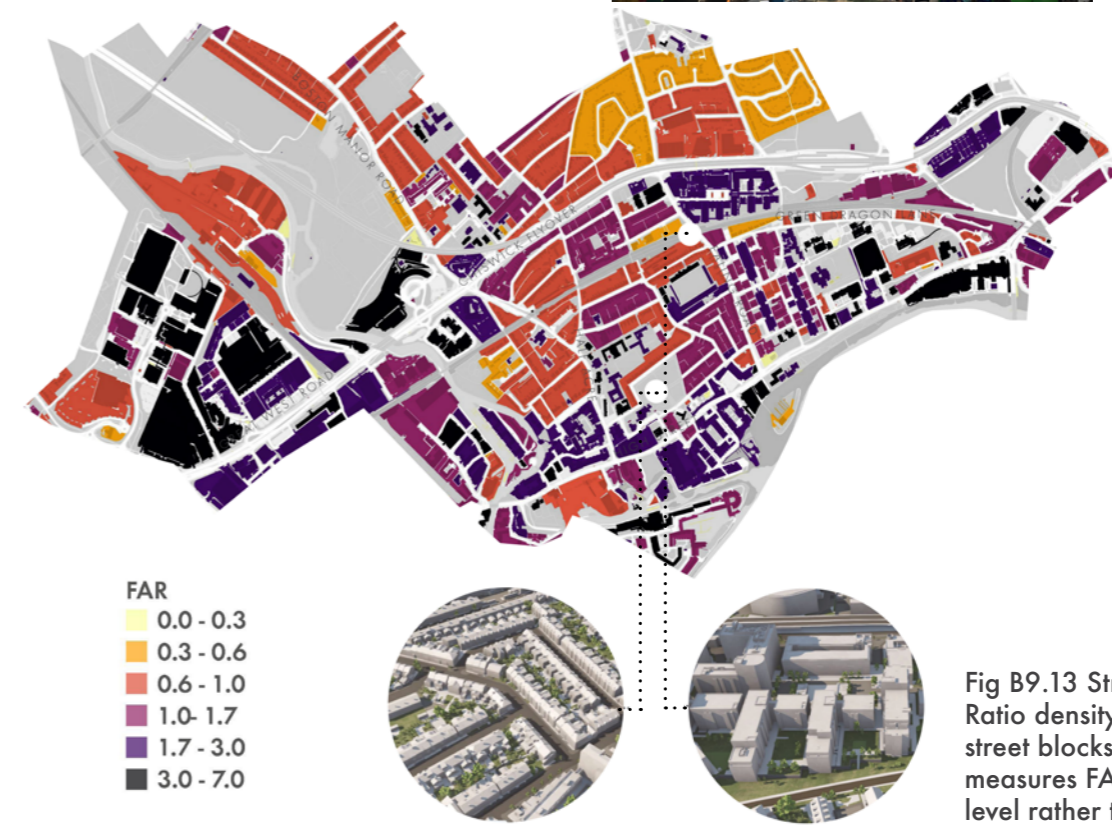
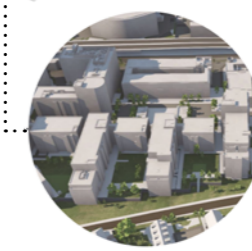


Fig B9.11 Strategic Urban Greening Factor analysis of existing street blocks - this measurement includes the contribution of street trees and planting in the public realm as well as greenery within plots. The adopted London Plan suggests a minimum score 0.3 to support climate adaptation.



Parade plots along Brook Road South have an existing FAR density of 1.5



Residential plots along the Great West Road have an existing FAR density of 2.0

Fig B9.13 Strategic Floor Area Ratio density analysis of existing street blocks - this analysis measures FAR at the street block level rather than plot level and so gives an average estimate of FAR on plots.

B9.19 For a greener, healthier, cleaner, safer, thriving and liveable Borough, Brentford should transition towards:

1. **Supporting Brentford High Street as a vibrant hub for the local community.**

The centre is an important asset that needs support to evolve and meet the needs of its local communities into the future. Brentford High Street is undergoing significant change and further interventions will be needed to knit the old, the new and the in-between together into a strong centre with a mix of daytime and evening uses, high quality public realm, a range of workspaces for creative businesses and freelancers, and greening that relates really positively to the waterfront. The emerging town centre masterplan work

is establishing the framework for bringing forward transformation in the town centre. Cycleway route C40 and the ongoing delivery of C9 improve cycle access to the town centre.

2. **Supporting the Great West Corridor as a major urban hub with an intensive mix of uses,**

where transformation through high quality development design, introduction of greater permeability / active travel, and measures to improve air quality, will enable this corridor to thrive and be positively embraced by adjacent neighbourhoods.

3. **Enabling new connections to be established to overcome existing barriers.**

The M4, Great West Road and South Western railway currently bisect this area into a series of east-west oriented tranches.

Opportunities to create better pedestrian, cycle and public transport links north-south across these areas should be maximised and integrate well with planned infrastructure provision like the West London orbital link and the Brentford to Southall rail link.

4. **Helping nature take its course through the enhancement of existing green and blue corridors.**

The area already supports varied habitats through its existing green and blue infrastructure, like at Boston Manor Park. Future growth should encourage continuous and generous wildlife habitats along corridors such as the River Brent and Grand Union Canal walk towards the Thames, with such routes enhanced as attractive, safe riverside walking and cycling routes that are well signposted from the town centre and stations. To reduce future flood risk in this area in a way that creates a more biodiverse and accessible riverside, action needs to be taken to maintain flood defences at their current level, accepting that flood risk will increase, and creating climate resilient communities that are prepared for this. Future development should introduce greenery to contribute to the enhancement of corridors, helping to soften the landscape and contributing toward biodiversity net gain in this area. Enhancements to open spaces should introduce opportunities for play space and food growing. The SINC to the west of Commerce Road presents an opportunity for a new nature reserve with learning and volunteering opportunities.

5. **Respecting and reinforcing the wealth of historic fabric and environments.**

The well celebrated and the more hidden. New development must respect the extensive historic environment throughout Brentford. This includes its Georgian remnants, the cohesive character of existing neighbourhoods, for example around Whitestile Road, the historic Brentford Dock environments, the Grand Union Canal and River Brent, Brentford's industrial heritage like The Gillette Factory (Grade II listed) on the Great West Road, as well as the more unassuming heritage like

its Victorian terraces. Brentford's layers of development and variety of architectural styles contribute positively to the character of the area. New developments must also be sensitive to the setting of assets outside the Borough boundary such as Royal Botanical Gardens, Kew World Heritage Site. Opportunities to integrate archeological remains in new development to add archaeological interest and contribute to placeshaping should be explored.

6. **Contributing to the waterfront.**

In this location, waterfront buildings tend to have a more low-rise, residential and intimate relationship with the River Thames and are often framed by trees. The area also has a mixed-use character, including the operational boatyards, which lend a unique character to Brentford. This character should be maintained whilst considering new opportunities for mixed-use context-led development along the River Brent and Grand Union Canal with an animated and publicly accessible riverfront environment, which builds on the unique active travel, ecological and cultural opportunities that these historic waterways provide.

7. **Embedding sustainability and climate resilience in new development.**

This area is a focus for new development and as such there is an opportunity that exists to embed high sustainability standards in the design of all new development. Further, there exists an opportunity to investigate the feasibility of delivery of a heat network and alternative energy sources to ensure all new development is powered by renewable sources. Existing development should be a focus for retrofit to improve environmental performance.

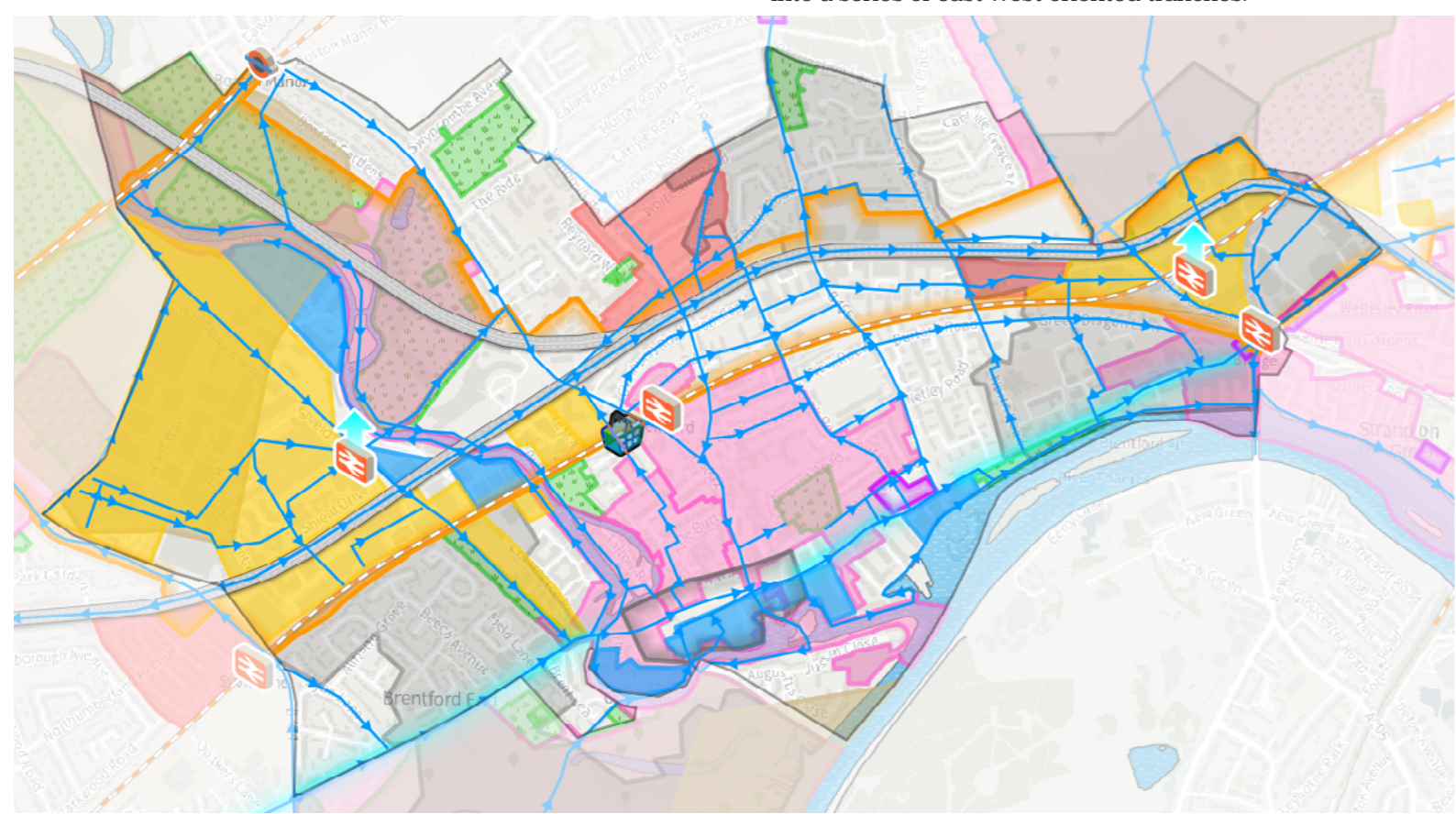


Fig B9.15 Area transitioning vision diagram *Diagram to be amended to align with the single Local Plan, once adopted



Boston Manor



“Quite an open feel with local allotments, and some small green spaces, quiet, fairly good transport connections and local shops”

- Resident's view on what's special

Plan of neighbourhood *Diagram to be amended to align with the single Local Plan, once adopted

What's special about Boston Manor?

B9.20 Boston Manor Park is an important large open green space for local residents. The park is bisected by the M4 flyover and has a very different character on either side, the woodland forming the edge to the River Brent/ Grand Union Canal and the more formal open space with large mature trees to the east providing the setting for Boston Manor House. The unique character of this area is protected by the Grand Union Canal and Boston Manor conservation area. The park includes tennis courts, a basketball court and children's play area. Other well-loved spaces include Blondin Park and allotments, just outside the borough, and Swancombe and Boston Manor playing fields which give the area a green open feel, a characteristic locals really value.

B9.21 Boston Manor sits within Brentford ward. The average age of residents is 31 making it one of the youngest areas in the Borough (GLA, 2015). Boston Manor has a predominately white population with a significant number of Black African residents living in the residential area to the east of Windmill Lane (2011 Census).

B9.22 Housing is mostly early two-storey inter-war semis, and short-medium terraces with rear access lanes. Mature tree-lined streets are found throughout. The area is home to architecturally interesting three-storey inter-war linear apartment blocks to the south, where there is generally a greater mix of house types, including 18th century houses on the southern end of Boston Manor Road and the

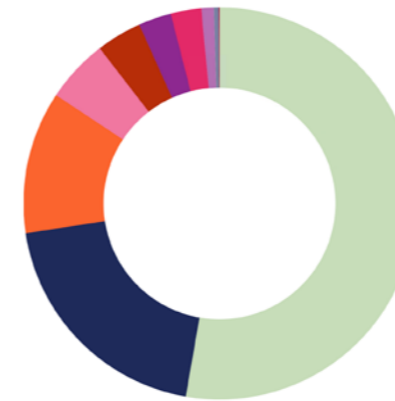
recent Reynard Mills development.

B9.23 The area is served by the listed underground station, Boston Manor, and a shopping parade to the north of Boston Manor Road. Boston Manor Road merges gradually with Hanwell, part of Ealing, to the north. The area to the east has strong associations with the Clayponds neighbourhood too and buses link the area to Brentford town centre, Hanwell and Ealing.

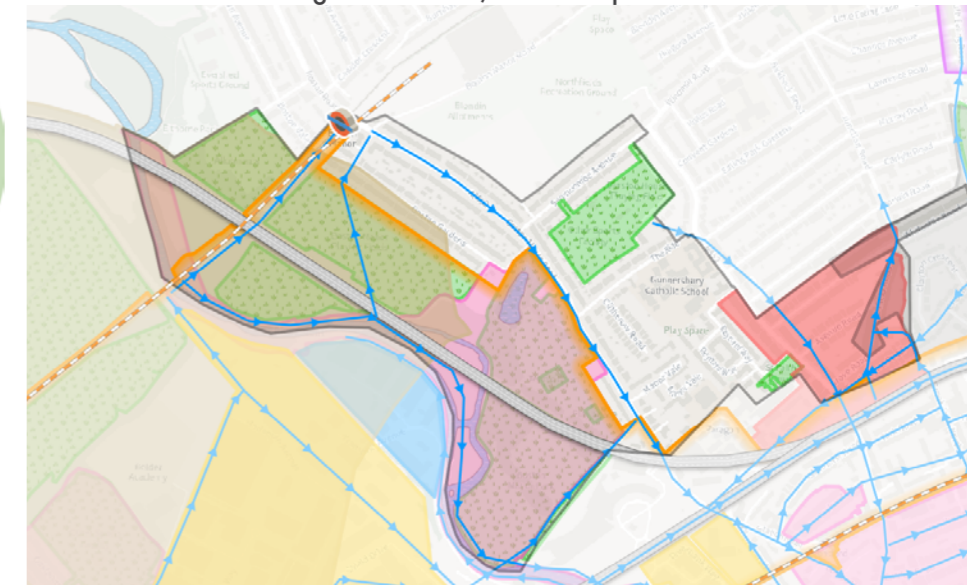
B9.24 Local assets include Gunnersbury School, Our Lady & St John's RC Primary School, St Faith's Church and The Globe pub on Windmill Road.

Prompts for character-led development

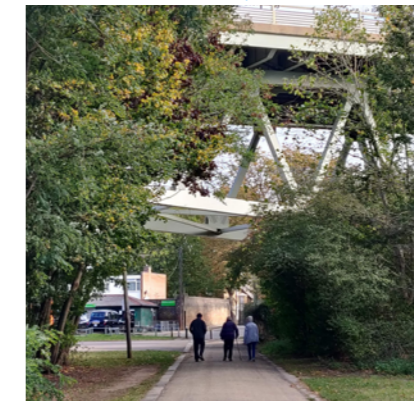
- Development should be respectful to the area's existing character. The existing building height range in this area is between 2-5 storeys approximately. Interventions could successfully respect existing character with a height range of between 2-5 storeys. New additions must be generous in their design quality and contribution to streetscape, seeking to support further enhancements to the public realm and experience of the neighbourhood. Any intervention should recognise the importance of historic buildings and their settings, including Boston Manor and views from Boston Manor Park out to the wider area, as well as support enhanced north-south links.
- Proposals should seek to address deprivation in parts of Boston Manor through addressing barriers to housing, education and employment, and improving the quality of the local environment.



Green open space	52.8%
Suburban	19.8%
Urban terrace	11.7%
Institutions headquarters	5.2%
Modern urban	3.8%
Cul-de-sac	2.7%
Industry	2.5%
Mansion blocks	1%
Villa typology	0.4%



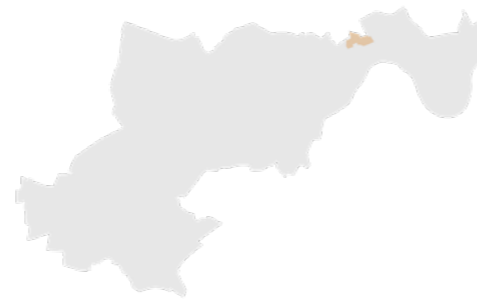
Green Open Space	Opportunity Area Boundary	Train line	Conservation Areas
Areas of Special Character	Major Road	20% most deprived IMD decile nationally	Tube Station
Waterbodies	Enhance Greening and Active Travel		



Selection of locally distinctive architectural details and materials which may inform new developments



Brentford - Clayponds



What's special about Brentford - Clayponds?

B9.25 Clayponds sits within Brentford ward. The average age of residents in the ward is 31, making it one of the youngest areas in the Borough (GLA, 2015). Clayponds has an ethnically diverse population with a significant concentration of residents who describe themselves as Black or Mixed living in the neighbourhood (2011 Census).

B9.26 The Clayponds area of Brentford is predominantly housing, bounded to the south by the M4 flyover and to the north by South Ealing Cemetery. Beneath the cemetery is Clayponds Gardens, a post-war estate comprised of free-standing blocks arranged around green space, with a community centre and play area. To the south is the Clayponds housing estate, an early inter-war estate of two storey semi-detached houses and short terraces, arranged to maximise greenery and landscaping. The layout and design reflect the ideas of housing reformers that homes for the working class should be built to high standards.

B9.27 A shopping parade on South Ealing Road includes a couple of grocery stores and a newsagents. The 65 bus route connects the area with larger centres at Ealing, Brentford and Kingston for locals to access a greater variety of shops and leisure activities.

B9.28 Lionel Road Primary School and Clayponds Hospital just outside the neighbourhood are important community facilities for residents. Carville Hall Park to the south is a local green space with an outdoor gym and play area, just north of Great West Road. Across Lionel Road is the much larger Gunnersbury Park, a short walk away for locals and a much-loved green space. The south-western corner of the Gunnersbury Park conservation area falls into Brentford Clayponds.

B9.29 The area relates closely with the Boston Manor area, specifically Whitestile Road.

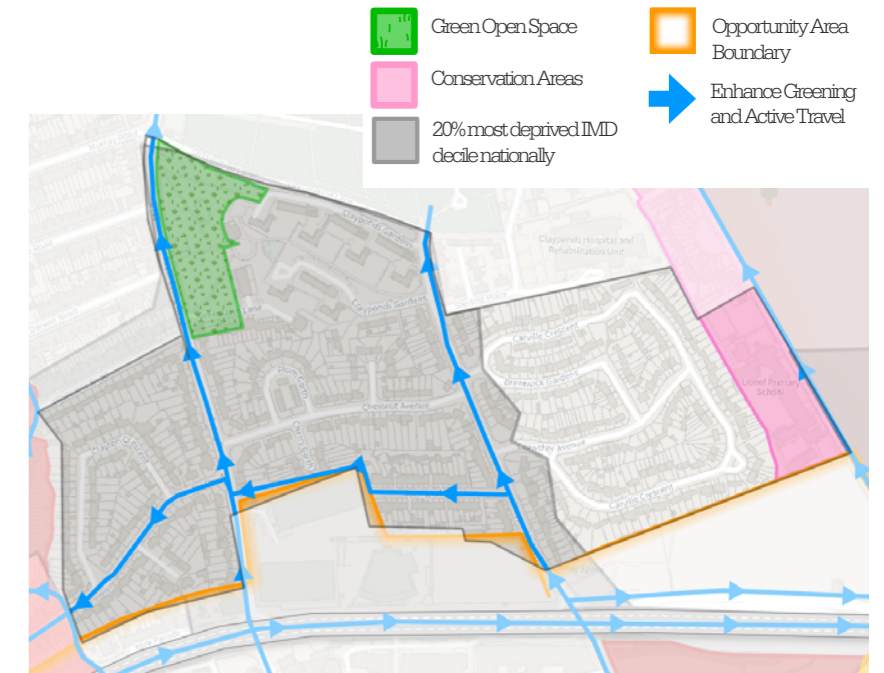
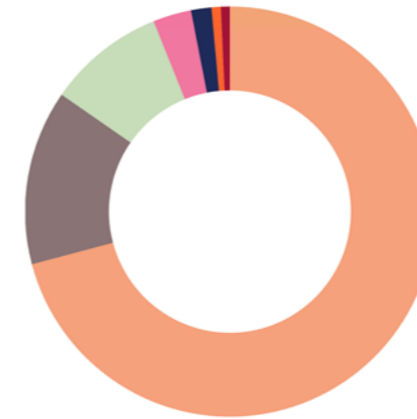
Prompts for character-led development

- There are opportunities for public realm improvements along the local shopping parade on South Ealing Road.
- The area serves as a gateway into London from Heathrow Airport, and the location of the new Lionel Road station on the West London Orbital Rail, in Brentford, to the west of the neighbourhood means any new additions should be generous in their design quality to accommodate the area's changing urban fabric.
- Proposals should seek to address deprivation in Clayponds through addressing barriers to housing, education and employment, and improving the quality of the local environment.
- The existing building height range in this neighbourhood is 2-6 storeys approximately. New buildings should be between 2-5 storeys.

Selection of locally distinctive architectural details and materials which may inform new developments



B9-25



Plan of neighbourhood *Diagram to be amended to align with the single Local Plan, once adopted



Brentford - Great West Road



What's special about Brentford - Great West Road?

- B9.30 The Great West Road area of Brentford serves an important role as a major employment hub for the wider area. Sky, Allianz and University West London are all based here alongside an assortment of smaller companies, enterprises and micro-businesses too. The area is almost exclusively industrial and commercial use, tucked in north of the railway line and fronting both sides of the Great West Road.
- B9.31 At the junction with Boston Manor Road is University West London Brentford's Paragon Campus alongside smaller scale office buildings hosting a mix of uses including a specialist furniture showroom.
- B9.32 Further to the east is some mixed use development on previously commercial sites between Great West Road and the railway line - including residential, hotel, retail, community and leisure uses in four to 10 storey buildings. The taller elements contrast with the low-rise warehouse units with large building footprints alongside some big box retail to the southwest.
- B9.33 Notable buildings include the surviving examples of fine Art Deco architecture fronting Great West Road, including the Gillette clock tower, a local landmark for the area. The well-preserved nineteenth and twentieth-century houses of the St Paul's conservation area also contribute to the area's history and identity. Shops and local cafes support both residents and workers, and are dispersed throughout the area.

Selection of locally distinctive architectural details and materials which may inform new developments



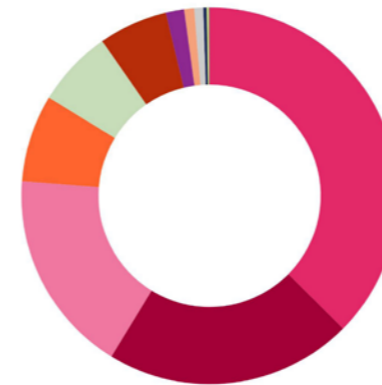
B9.34 The area's industrial heritage, fragments of old and new housing and green space, as well as the thread of the River Brent add to its distinctiveness whilst drawing it into the heart of Brentford and contributing to an overall eclectic 'Brentford' character too.

Prompts for character-led development

- The neighbourhood of Great West Road is part of the existing enterprise zone, the Great West Road Opportunity Area, and will sit adjacent to the planned new overground extension and future station. This means any new development should set a high bar in terms of design aspiration to support transformation, whilst being respectful to surrounding character adjacent to the neighbourhood.
- The existing building height range in this neighbourhood is 2-26 storeys approximately. The height of any new interventions should fall within this existing range (subject to review), and any proposals for tall buildings should refer to the Local Plan.
- Throughout the Great West Corridor, there is a need to improve active and sustainable travel links, in addition to public realm improvements, greening and reducing severance. A key priority for new developments is to support linkages into the neighbouring areas and sensitively manage the transition in scale.
- New development should improve access to and along the Grand Union Canal / River Brent towpath to increase the accessibility of this local asset and promote further connections to other green spaces along its course.

- The frequency of street trees and planting in the west of the area is generally low and needs to be increased. All development sites should seek to make a contribution towards addressing the urban greening priority area including greening key streets through increasing street trees and planting. Development should seek to achieve the minimum Urban Greening Factor target on site and then contribute towards greening in the public realm.

- This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment such as green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.

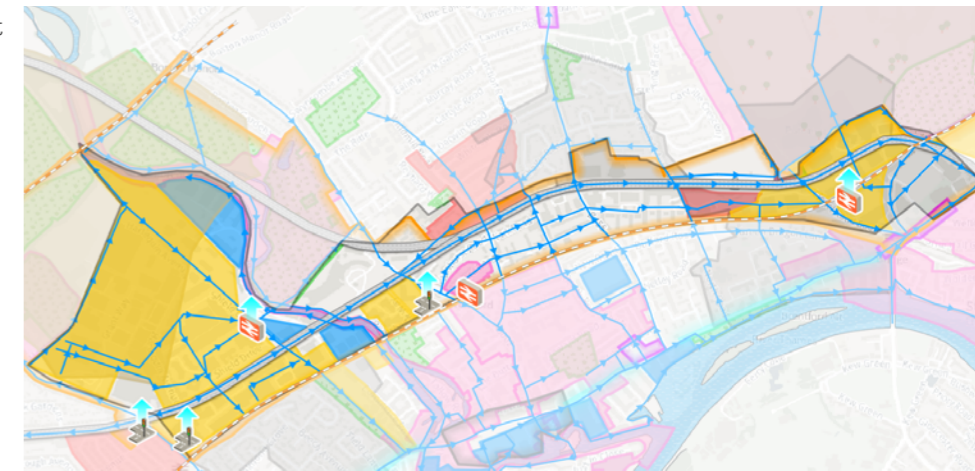


Neighbourhood typologies

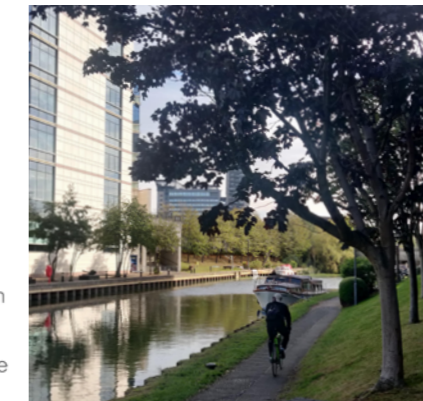
- 38% Industry
- 21% Big box
- 18% Institutions
- 8% Urban terrace
- 7% Green space
- 6% Modern urban
- 2% Cul-de-sac
- 2% Cottage estate
- 1% Infrastructure



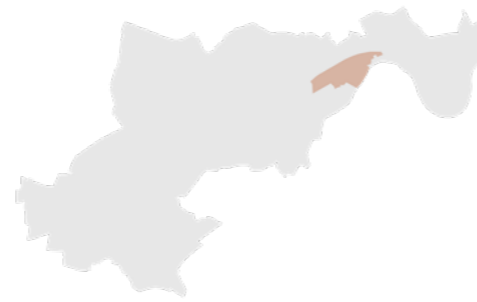
Plan of neighbourhood *Diagram to be amended to align with the single Local Plan, once adopted



	Train Station		Opportunity Area Boundary		Enhance Greening and Active Travel
	Key Transformation area		20% most deprived IMD decile nationally		Train station improvements
	Conservation Areas		Local Centres		Major Road
	Areas of Special Character		Train line		Junction Improvements
	Water bodies		Sites with Character Enhancing Potential		



Brentford



WHAT IS BRENTFORDS'S CHARACTER?

What's special about Brentford?

- B9.35 Brentford neighbourhood includes the historic town centre, waterfront and nearby residential areas. The area has a rich heritage with an eclectic mix of Georgian, Victorian, Art Deco, Mid Century and industrial buildings. The town's waterways of the Thames, the Brent and the Grand Union Canal are highly valued by locals, and have been integral to the area's evolution. Formerly attracting canal-side industrial activity, they continue to play an important role, supporting new and existing activities and uses and helping to forge a strong identity for the neighbourhood. This includes Brentford Marina, the boatyard and moorings at Lots Ait and recreational facilities such as the Team Keane Watersports School.
- B9.36 Its people also contribute to the identity and diversity of Brentford. The average age of residents in Brentford ward is 31, making it one of the youngest areas in the Borough (GLA, 2015). It also has the second highest percentage of people living in one-person households - 35.8% of Brentford ward residents (2011 Census). Residents are majority white British and 33.9% of the ward are from BAME backgrounds. There are notable numbers of Black African, Polish, Irish, Black Caribbean and Chinese residents. Locals value the sense of community in Brentford and there is a strong community sector across the ward as a whole.
- B9.37 Brentford's diversity of people is reflected along the High Street, home to a mix of shops, cafes, restaurants and services. Further west on

London Road is Brentford Cafe, a popular dining spot. Local schools include St Paul's Primary and Brentford School for Girls. Brentford Library is a key local resource and Grade II listed historic gem.

B9.38 The Butts predominantly Georgian historic neighbourhood, central Market Place and Waterman's Park are three important historic assets. Other historic landmarks include the former church of St Lawrence (Grade II* listed), the Magistrates Court and the Beehive pub. Much of the High Street is within the Butts and St Paul's conservation areas, which underwent appraisals in January 2020 and January 2021, respectively, and includes a number of 18th and 19th century buildings. To the east sits a complete Victorian New Town of terraces arranged around open space including St Paul's recreation ground. Robin Grove Park and playground, and the large Syon Park to the west, are valued green spaces for Brentford's residents. Nearby is Griffin Park stadium which is to be redeveloped.

B9.39 Brentford has seen continued renewal, with mid-century homes, shops and public spaces and more recent apartments fronting onto Brentford Lock. These sit side by side with Victorian heritage, as well as existing industrial activities including two working boatyards.

Prompts for character-led development

- There are a few remaining Art Deco industrial buildings in the town centre including on Commerce Road. These are to be retained and refurbished.
- The existing building height range in this neighbourhood is 2-13 storeys approximately. New buildings should be between 2-10 storeys in height, subject to review. Any proposals for tall buildings should refer to the Local Plan.
- Opportunities to enhance some of the post-war housing to address current issues around structure and visible backs should be encouraged.
- To reinforce the use of Brentford as a town centre, leisure uses and retail should be actively encouraged and should cater for a diverse community and a range of ages. There

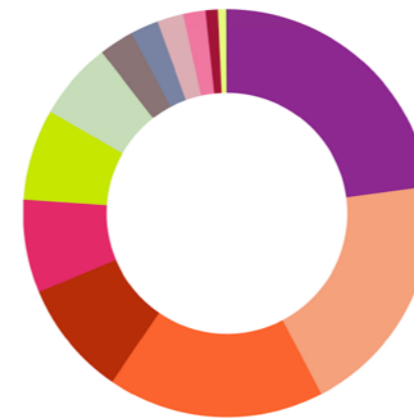
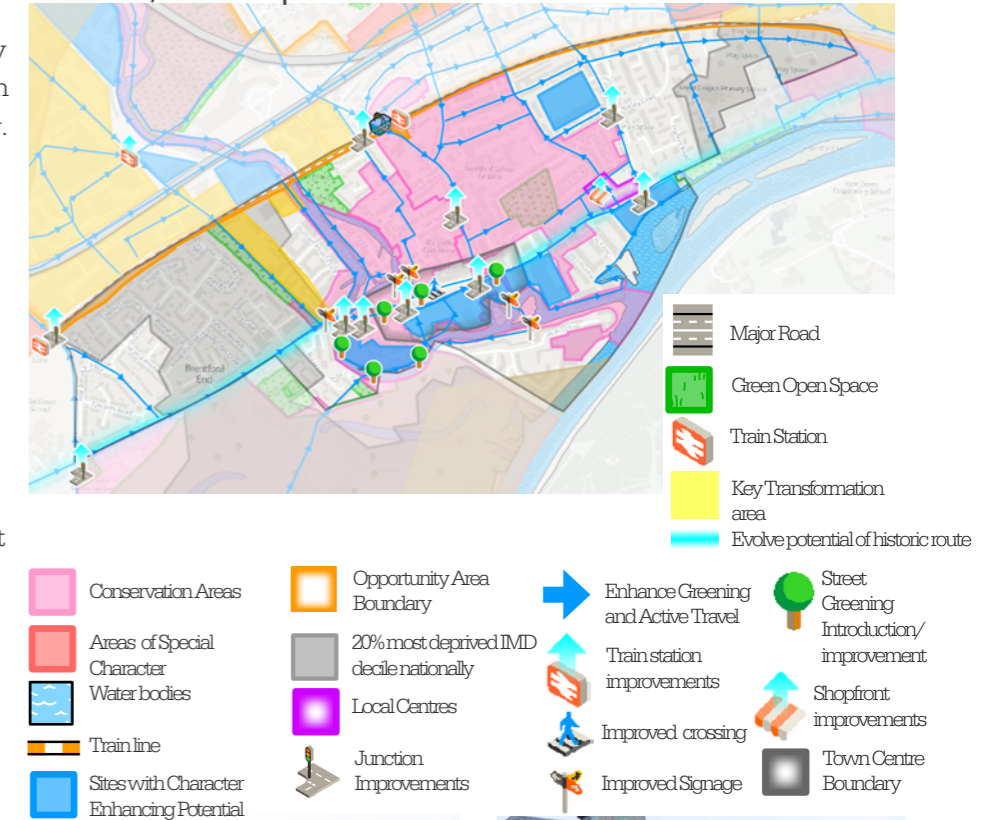
is an opportunity to enhance connectivity to the town centre with active and sustainable travel, in addition to public realm improvements.

- All development sites must make a contribution towards addressing the Brentford urban greening priority area including greening key streets and the town centre through increasing street trees and planting. Development must achieve the minimum Urban Greening Factor target on site and then contribute towards greening in the public realm.
- This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and

contribution with particular attention to sustainable drainage and biodiversity.

- Proposals should be informed by the emerging town centre masterplan and the town centre design codes overleaf.

Plan of neighbourhood *Diagram to be amended to align with the single Local Plan, once adopted



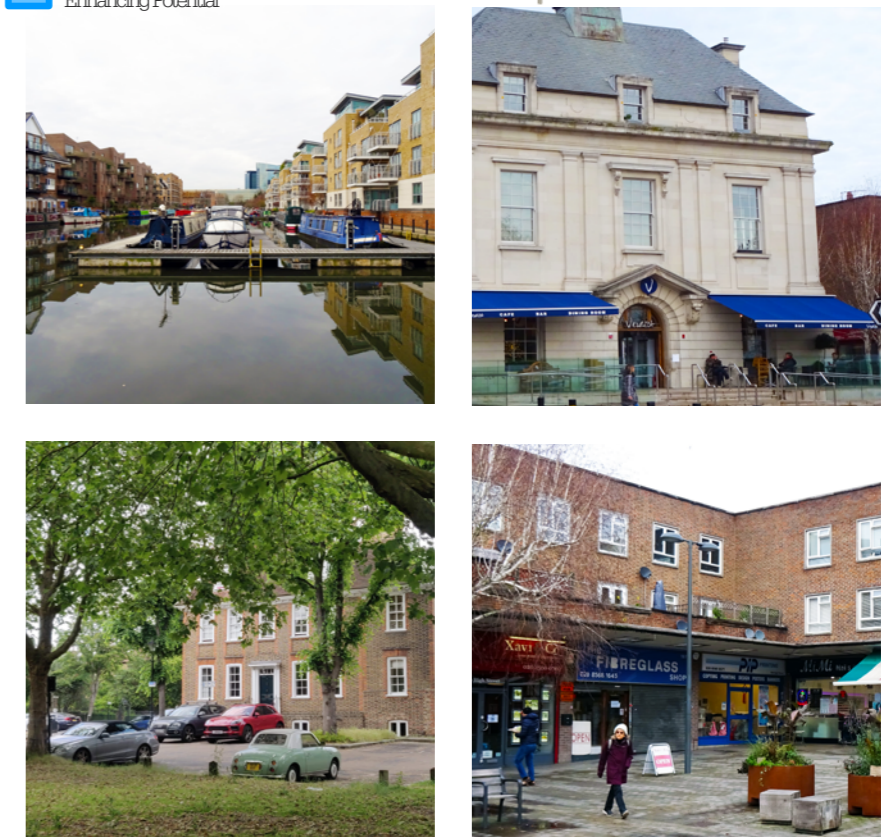
Selection of locally distinctive architectural details and materials which may inform new developments



Roof rhythm and terracotta tiling

Timber and masonry detailing

Industrial waterside environment



Brentford Town Centre Design Codes

Town Centre Masterplan themes: Character and Townscape and Development

The design codes in this section seek to deliver these key masterplan principals in bold.

Built Form

Creating a more coherent townscape and reinforcing local character areas and key holding points...

B9.40 Developments should be accompanied by a robust study of the existing roofscape to ensure that all proposals contribute towards the development of an attractive townscape. Developments can take cues from Brentford's industrial waterside architecture when proposing roof forms.

B9.41 Developments should be accompanied by a robust study of nearby buildings, taking into account their materiality, architectural detailing, scale, grain and effect on the townscape. This should be used to inform sensitive contextual responses that work to enhance the cultural significance of the area.

B9.42 A local palate of materials includes cast-iron and concrete details, brick, arches and industrial fenestration design. Texture in materiality will be encouraged. Any advertising or signage should be integrated into the design early on rather than an afterthought.

Mediating scale...

B9.43 While buildings on the High Street may be of a slightly different scale, the town centre is largely surrounded by residential low to mid rise buildings and the transition between the high street and the surrounding area needs to be carefully considered.

B9.44 Additional storeys above neighbouring properties can be accommodated through set backs that preserve the street profile and reduce visual bulk.

Returning to a town centre grain...

B9.45 New development should recognise the strength of the finer grain historic structure of the centre. New development should incorporate finer grain, street-focused buildings that can be easily adapted between uses, particularly at ground floor to build in resilience. Applicants should consider the integration of community facilities as part of their proposals.

Defining fronts, backs and edges...

B9.46 Fronts and backs should be clearly defined with active frontages along primary routes with non-residential uses at ground floor.

B9.47 End of parade sites should provide frontage and/or fenestration to the flank wall, to provide natural surveillance and security.

B9.48 Access to non-residential uses at ground floor should be from the primary route, with separate entrances to each retail and/or commercial unit as required.

B9.49 Residential entrances at ground floor should be welcoming and clearly visible if they are interspersed with non-residential uses.

B9.50 Multiple entrances (front doors and lobbies) may be required on larger sites and denser developments, distinguishing these between residential and non-residential uses at ground or upper floors.

A3.51 Balconies and private amenity spaces facing high streets need to be set in (not projecting) and carefully design to protect privacy.

Riverfront views, access and setting...

B9.52 New development should ensure that the waterside character, heritage and uses are taken into consideration at an early stage of the design process so that they are reflected in building design.

B9.53 Every effort should be made to retain existing industrial waterside architecture as part of new developments. Where this is not possible, developments should strive to reuse existing materials or other architectural elements.

B9.54 Waterside developments should make every effort to incorporate active ground floor frontages (commercial, leisure, culture and community uses) facing the waterfront. Residential and other publicly inaccessible use is not appropriate for the ground floor of waterside developments.

B9.55 Careful consideration of existing riverside uses such as boatyards and residential mooring should be made early on of the design of any proposed development.

B9.56 Waterfront development building lines should be positioned to allow for the provision of a generous, continuous public realm along the water.

B9.57 Where possible, building lines should be positioned a minimum of two meters away from the outer edge of any riverside/canal paths. Where a riverside path is not defined, building lines should be positioned a minimum of five

meters away from the water edge or from the inner face of waterside fall protection (whichever one is further away from the water).

B9.58 Newly established water-facing building lines should be accompanied by a robust study of existing building lines to ensure that development proposals will create a coherent and continuous waterside frontage.

Heritage & Context

Reinforce local character areas and key holding points and a retrofit-first approach...

B9.60 While surrounded by a heritage context, much of the high street has lost many of its historic buildings. The ones that do survive should be retained, celebrated and highlighted in any new development. On sites with no historic context, new high quality innovative architecture is welcomed which will add a new element of character to the town centre, while being mindful of the heritage which surrounds the town centre. Regeneration should be heritage led.

Celebrate local landmarks...

B9.61 New developments must also be sensitive to the setting of heritage assets outside the town centre including but not limited to Syon Park, Royal Botanic Gardens, Kew World Heritage Site and surrounding and adjacent conservation areas.

B9.62 Any development proposals will need to be accompanied by a full views, heritage and townscape assessment. Proposals that might impact the Outstanding Universal Value or significance of the Kew WHS should prepare heritage assessments in line with the UNESCO Guidance and Toolkit for Impact Assessments in a World Heritage Context (2022) publication. Additionally, development proposals will need to be mindful of the setting of historic buildings within and adjacent to the town centre. Opportunities to enhance the setting of local landmarks should be explored.



Fig B9.16 Waterside developments with industrial inspiration. Left: AHMM © Hawley Wharf Right: Brentford Lock West

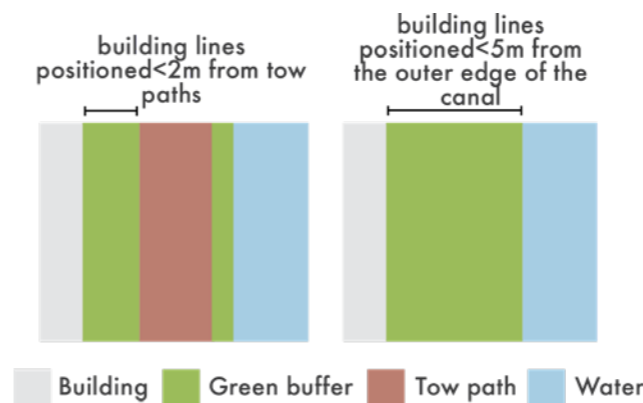


Fig B9.17 Appropriate building lines adjacent to the waterfront

Town Centre Masterplan themes: Movement and Public Realm.

The design codes in this section seek to deliver these key masterplan principals in bold.

Movement

The High Street: a place for people and vehicular movements...

- B9.63 Improvements are required along the length of the High Street, including:
 - segregated cycle lane facilities along the High Street,
 - new signalised junctions with pedestrian crossings,
 - prioritisation of pedestrians and cyclists along the High Street and at junctions,
 - removal of railings,
 - wider pavements along the High Street and at appropriate junctions,
 - traffic calming measures along the High Street.

B9.64 The proposed C9 cycleway scheme should include the installation of street trees along the High Street and pedestrian crossings to improve greening.

B9.65 Existing access and movement arrangements around and into the site need to be factored into proposals.

B9.66 Commercial servicing arrangements must be integrated into the design at an early stage.

Connectivity and wayfinding...

B9.67 Braemar Road, Netley Road and Albany Road are frequently used by children and their families as key routes to and from schools. On these streets pedestrian and cycle movements should be prioritised through greening, integration of cycle facilities, inclusion of places to rest and improvement of the crossing facilities on Ealing Road between Braemar and Netley Road to facilitate active travel to and from local schools.

B9.68 Brentford Station is a key access point to the town centre. Development should improve routes to the station to strengthen its connectivity.

B9.69 This could include the establishment of a quiet pedestrian and cycling route from the High Street to Brentford Station and the A4 via Market Place/Upper Butts/ Church Walk through use of signage, and creation of a step free & cycling friendly crossing over the railway line.

B9.70 Improvements should also be made to Half Acre/Boston Manor Road to improve the pedestrian experience between the station and the town centre, including pavement resurfacing where necessary, pavement widening, removal of barriers and bollards, greening and junction and crossing improvements at Brentford Station and The Butts. The public realm outside Brentford Station should also be improved to facilitate better a pedestrian way finding experience and safety.



Fig B9.18 Shared space, Orford Road, Walthamstow. An example of prioritising pedestrian and cycle movements.

Public realm

Continuous riverside walk...

B9.71 The Grand Union Canal/River Brent facilitates largely off-road pedestrian movement throughout the town centre. Its potential to facilitate active travel could be enhanced as attractive, safe and continuous river routes that are well signposted from the town centre and stations. The varied character of Brentford's waterways, including along the Grand Union Canal itself, should be celebrated and protected.

B9.72 All waterside developments should contribute towards the establishment of a publicly accessible, continuous waterside walkway, ensuring that the historic importance of the Grand Union Canal towpath is preserved, care should be taken that materials used are appropriate for this context.. Proposals that seek to restrict access to parts of the waterside will be resisted. It is encouraged that all proposed waterfront developments have early input from the Canal and River Trust.

B9.73 Newly established publicly accessible waterside walkways should connect to publicly accessible waterside routes on adjacent sites. Where this is not possible due to conditions on neighbouring sites, the waterside walkway should be designed in a manner that facilitates future connection of waterside routes.

B9.74 To further enhance the accessibility, continuity and safety of this route all waterside public realm should be designed in a secure manner that discourages antisocial activity and follows secure by design principles. This should include enhancing access from key bridges, path widening to 2m+, barriers to cycling such as stairs removed or adapted to accommodate

cycling, resurfacing where needed with appropriate materials, route signage introduced, under bridge lighting introduced and waterside markers or lighting explored where appropriate, and the introduction of overlooking by occupied buildings.

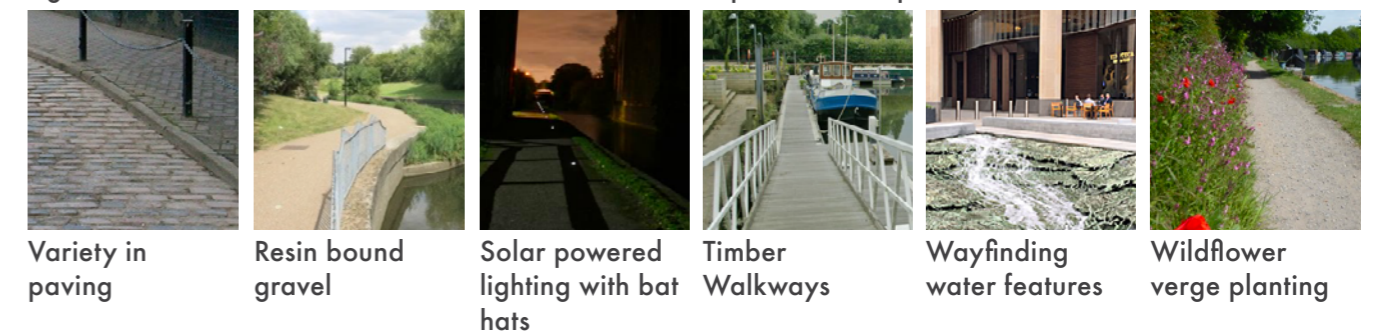
B9.75 Changes in paving texture, pattern and scale will be welcome. Where a continuous path is unlikely to be achieved in the short & medium term, developments should propose creative wayfinding strategies that allow users to seamlessly navigate along the waterfront and within Brentford town centre., e.g. painted water crossings/streets, running water features.

B9.76 Overhead architectural elements such as balconies, overhangs and other projecting structures should be kept a minimum of 1m away in plan from the nearest edge of any riverside/canal paths.

B9.77 Developments located south of the High Street should aim to incorporate yards running north-south towards the waterfront as a means of improving pedestrian connectivity, framing views to the waterside and reinforcing Brentford's identity as a waterside town.

B9.78 The identity of the waterside public realm is heavily influenced by the winding nature of the rivers and the stepping down of the public realm towards the water, a result of the industrial heritage of the area. Waterside developments that reinforce this identity will be encouraged.

Fig B9.19 Selection of materials which could be used to improve the towpath



B9.79 Waterside public realm should be designed to encourage activity. Dedicated areas for public activity such as performances, talks, exhibitions, market stalls and other uses should be incorporated into public realm designs as much as possible. Public art proposals will be encouraged.

A distinctive natural landscape...

B9.80 Opportunities for greening in street furniture should be taken, including opportunities for flower boxes and community planting. Planting should be maximised, particularly species of trees appropriate for a waterside setting. Developments should strive to improve urban greening and biodiversity.

B9.81 The frequency of street trees and planting on the high street is generally low and needs to be increased.

B9.82 Where appropriate along the riverside and within the town centre, well-designed, coordinated street furniture such as benches should be introduced to provide places to rest, in addition to wildflower verges to improve biodiversity along the route. (precedent for that-reflecting character).

B9.83 Waterside developments should introduce elements of urban greening along the waterfront and town centre sites should explore the opportunity for increased greening, including street trees and green walls and roofs. Opportunities for the introduction of pocket parks along the waterfront should be explored where possible

Brentford's characteristic yards...

B9.84 Yards running north-south should be paved differently to the public realm directly adjacent to the water, in order to reinforce the distinct character of the yards.

Reinforce the town centre core as a place for people and civic life...

B9.85 Hardscape designs should take cues from existing waterside materials. Texture and variety in laying patterns will be encouraged. Large-format paving will be resisted. High quality stone finishes such as granite will be encouraged. A coordinated approach to any new street furniture is needed in the town centre. Queues should be taken from the existing public realm around the marketplace to ensure a high quality and cohesive look. Any new street furniture should be constructed of robust high quality materials. Street furniture should be consistent and also take cues from the industrial waterside heritage. Iron or metal street furniture painted in a black white palette and of designs that evoke 19th century industrial London will be encouraged.

B9.86 Marine timber seating benches could be specified to further reinforce the waterside character of the area and to create continuity with the public realm currently being constructed as part of The Brentford Project.



Black and white industrial infrastructure Bridge painting Consistent traditional black signage



Traditional signage Hanging baskets Traditional post and rail balustrades

Fig B9.20 Selection of materials which could be used in the public realm

B9.87 Waterway infrastructure such as locks should be celebrated and become a visual focal point of the public realm design.

Fig B9.22 Plan showing public realm improvements within Brentford Town Centre

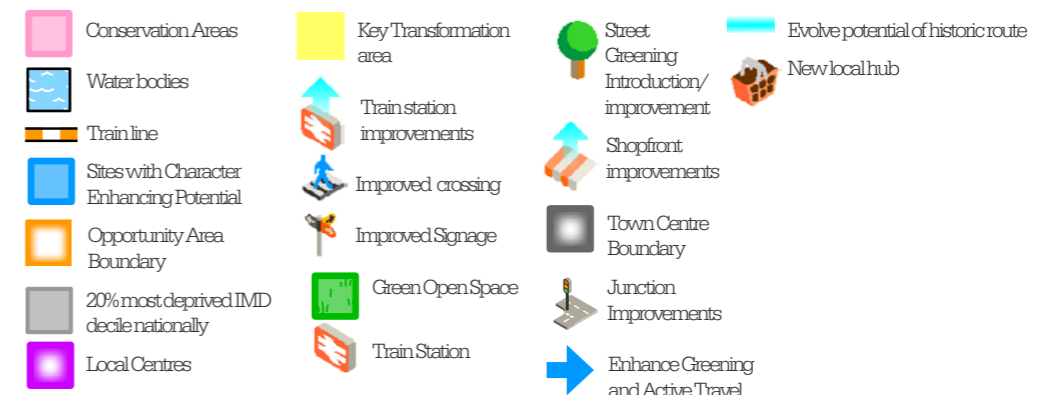
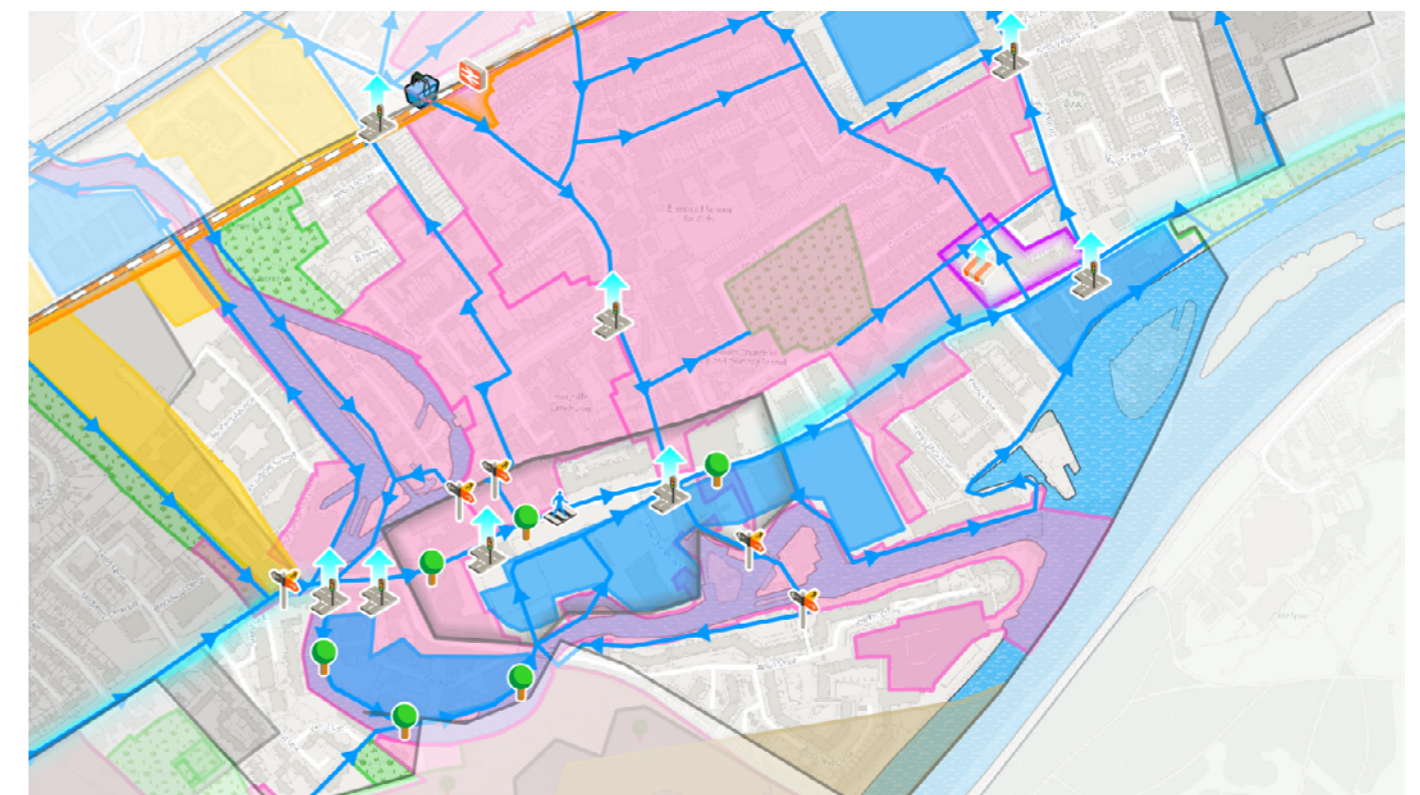
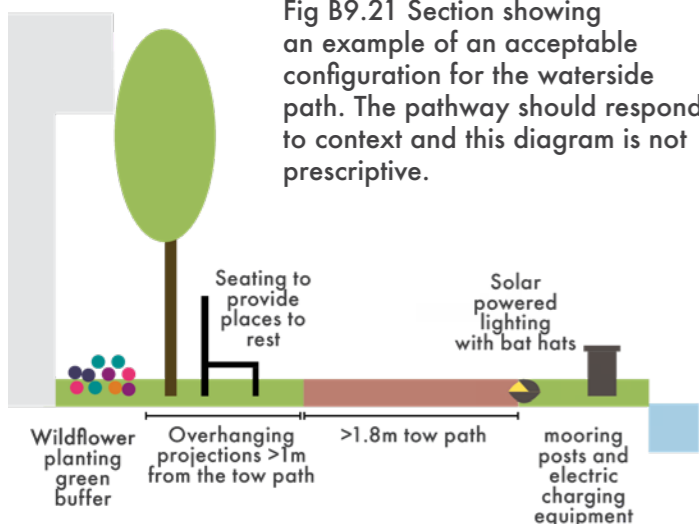


Fig B9.21 Section showing an example of an acceptable configuration for the waterside path. The pathway should respond to context and this diagram is not prescriptive.



Kew Bridge



What's special about Kew Bridge?

B9.88 Kew Bridge is a mix of old and new Victorian houses, large industrial scale buildings, mid-rise, as well as high-rise office spaces. To the east of the neighbourhood are two storey houses ranging from late 19th to late 20th century, surrounded by some civic use - including museums, and an emerging local centre to the south by the Grade II listed bridge and river, opposite Kew Bridge Station (Grade II listed) and the Express Tavern. The London Museum of Water & Steam is a Grade I listed local landmark and popular attraction. The variety of the area's historical features are protected by the Kew Bridge conservation area.

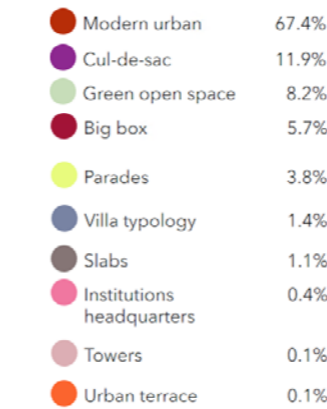
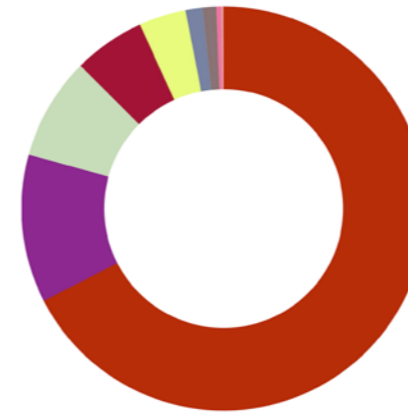
B9.89 The main residential area is at Brentford Towers. The six 23-storey blocks were constructed in the 1970s. The Green Dragon Primary School sits in the heart of the estate, with green spaces and car parking around. The towers serve as a local landmark for this part of the borough, although the area can feel quite isolated with no direct access to the High Street. Residents are majority white in Kew Bridge neighbourhood, although significant ethnic minority groups include Black African and Caribbean, and white Other.

B9.90 Kew Bridge benefits from significant blue infrastructure, which includes the River Thames, as well as large open spaces - including parks, such as the Watermans Park nearby, which has a strong riverside character and will be reinforced by the new boat moorings. The relationship to Kew on the south

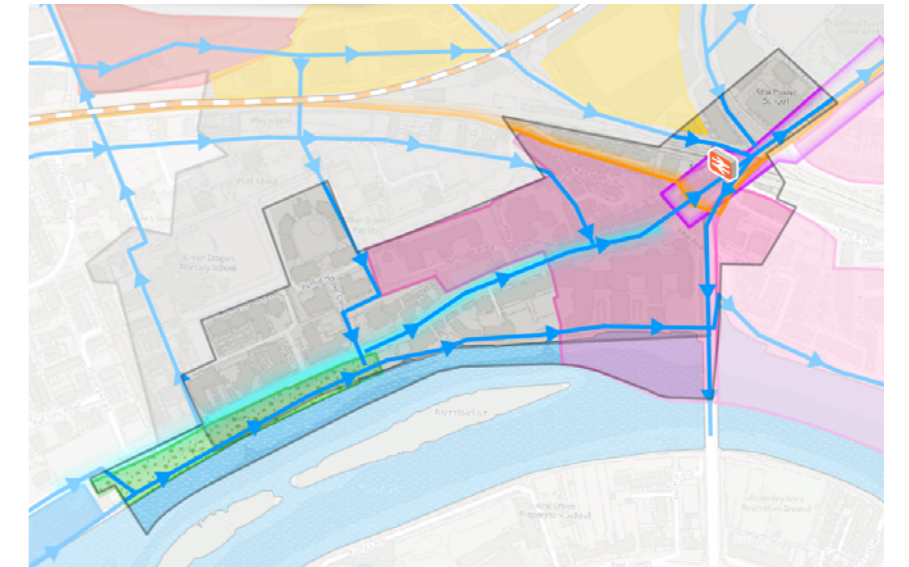
side of the river is strong and views across the river are an important part of the character of the area.

Prompts for character-led development

- Any development in this area must be mindful of the many layers of heritage and constraints that exist, not least the sensitivity of the Royal Botanic Gardens, Kew World Heritage Site. This neighbourhood is also known as Brentford East and the connection to Brentford is important.
- The existing building height range in this neighbourhood is 2-23 storeys approximately. New buildings should be between 2-8 storeys in height, subject to review. Any proposals for tall buildings should refer to the Local Plan.
- New development must support and enhance Brentford High Street and respect the waterfront context. Improving quality and access along the waterfront in Kew Bridge should also be a priority, including the provision of appropriate riparian life-saving equipment. There are opportunities for public realm improvements particularly around the Brentford Towers area which would include enhanced connections to the High Street.
- Proposals should seek to address deprivation in parts of Kew Bridge through addressing barriers to housing, education and employment, and improving the quality of the local environment.
- Proposals in this area should seek to address high levels of traffic, through improving the pedestrian and cycling environment through improved active travel and sustainable connections and an improved pedestrian focused public realm, in addition to traffic calming measures.



Plan of neighbourhood *Diagram to be amended to align with the single Local Plan, once adopted



Selection of locally distinctive architectural details and materials which may inform new developments





SITE DESIGN CODES BRENTFORD

<u>Brentford Fountain Leisure Park</u>	<u>41</u>
<u>Phoenix Business Park</u>	<u>47</u>

Brentford Fountain Leisure Park

Site overview

B9.91 The site is occupied by the Brentford Fountain Leisure Centre and its associated surface car park. The low-rise building is in poor condition and fronts the Chiswick High Road to the south. The site is surrounded by new /under construction developments which will be predominantly mid to high-rise.

Stakeholders

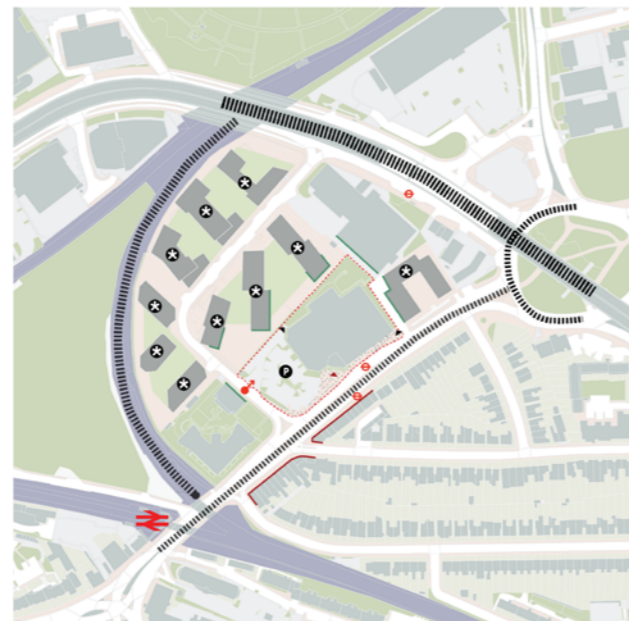
- Citygate Skoda West London
- Brentford Fountain Leisure Park
- The Gateway W5

Suitability and sensitivity issues identified in Part C Types identified in Part C Types

- Within 400m from a green open space of high quality
- Within the Great West Corridor Opportunity Area
- High levels of public transport accessibility (PTAL score 3 - 6b)

Constraints and opportunities

- The site is within close proximity to a railway station, as well as bus stops, making it well served by public transport.
- The area's close proximity to the M4, Great West Road, as well as the Chiswick High Road can make it feel like a place to pass through, rather than to stop.
- The site is located opposite the Wellesley Road Conservation Area and in close proximity to the Kew Bridge Conservation Area.
- Air and noise pollution from Chiswick Roundabout, M4, Great West Road, as well as Chiswick High Road provides an opportunity for improvement.
- Brentford FC stadium sits close to the area along the western edge of the site.



- Location plan**
- Site boundary
 - Major route
 - Railway line
 - Commercial frontage
 - Sensitive frontage
 - Commercial access
 - Servicing access
 - Car parking
 - Existing tall building
 - Consented scheme

Key information from Site Allocation	
Existing Site:	
Source	Emerging single Local Plan Site Allocations
Ownership	Public
Existing use	Leisure centre
Size	0.90 ha

- The site re-development presents an opportunity to re-locate the Grade II listed Brentford Market fountain to the site.
- Development has the potential to impact on a number of heritage assets, including the Royal Botanic Gardens Kew World Heritage Site, an Archaeological Priority Area and a number of conservation areas that are within proximity of the site, namely Wellesley Road, Kew Bridge, Gunnersbury Park & Thorney Hedge.



Aerial view facing north-west showing the scale of existing buildings and nearby developments

- Consented scheme
- Scheme under construction
- Site boundary



Key view of the site facing north-east from Chiswick High Road



Design guidance

Objectives

- Redevelopment of the Brentford Fountain Leisure Centre and car park to provide a new, multi-storey leisure centre alongside high quality housing to maximise the potential of the site.
- Transforming what is currently a 'black box' facility into an outward-facing and state-of-the-art leisure and community complex that serves its neighbourhood well.
- The redevelopment of the site should be coordinated with neighbouring schemes that are under construction or have been recently completed so that new and existing buildings and streets are well-integrated.
- Better integration of leisure uses with active travel networks and clean air routes.

Access

- Vehicular access should be retained along Capital Interchange Way.
- The private servicing road for Wheatstone House could be transformed into a two sided street subject to coordination and agreement across neighbouring land holdings, leading to mutual benefits.

Form of development

- A more permeable, street-based form of development is encouraged to improve pedestrian movement across the site and away from Chiswick High Road.
- Mixed-use perimeter blocks with mid-rise buildings and some tall elements are appropriate in this location. A taller focal element as part of a perimeter block, should be situated towards the rear of the site away from Chiswick High Road
- Free-form towers which do not form part of perimeter blocks are discouraged.
- Basements may be required to provide the requisite servicing and parking space for the leisure centre.
- Parking should be reduced as part of the redevelopment, conforming to the maximum

parking standards in the London Plan.

Relationship to surroundings

- The new leisure centre should be regarded as an exceptional, 'feature' building with good visibility from Kew Bridge Station and Chiswick Roundabout.
- The height of new buildings should mediate between recent development to the north-west of the site and existing buildings on the southern side of Chiswick High Road.
- The Grade II Brentford Market fountain should be relocated to a publicly accessible position within the site to better reflect this important statue's historic context and to allow it to be enjoyed in its historic setting.

Street level experience

- Active frontages should be provided along the perimeter of the site and within new internal streets.
- Residential entrances are better suited along the northern and eastern edges of the site with non-residential frontages elsewhere.
- Commercial (retail, food and beverage) uses are suitable along Chiswick High Road and facing the new leisure centre.
- New streets should have a minimum carriageway width of 5.5m and 2m footways on either side excluding private defensible space.

Wider contribution

- Improvements to the quality of the public realm along Chiswick High Road will be required as part of the development.
- Building lines should be set back to create wider pavements with better landscaping.
- Improvements to pedestrian and cycling active travel routes and infrastructure including better crossing points, enhanced public realm and high-quality cycle parking.
- Consider whether existing leisure uses can be retained during construction.

Climate mitigation and adaptation

- Soft landscaping and tree planting along the perimeter of the site can help to mitigate surface water flooding. It is a priority that mature trees are retained.
- Setbacks and lower building heights along the northern edges of the site can help to

mitigate the downdraught and the urban heat island effect.



Spatial framework

- Retail
- Residential
- Community / leisure
- Private amenity
- Improved public realm
- P Parking
- ✿ Potential for taller building
- Special civic interest
- Sensitive frontages
- Commercial frontages
- Secondary route
- Primary route
- Pedestrianised route
- Busy road
- ▶ Parking access
- ▶ Key access
- ▶ Services access

*For more information on site capacity and building heights refer to the Local Plan and Great West Corridor Masterplan

Good practice precedents

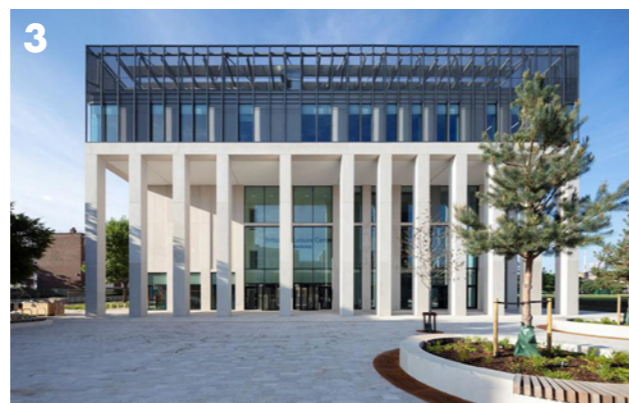
1 Saxon Court © Maccreehor Lavington - Residential perimeter block where the scale of massing is varied and steps down towards the pavement. Balconies made to face away from busy roads and inner courtyards are separated physically and visually.



2 Sir Ludwig Guttmann Health & Wellbeing Centre, LB Newham, Penoyre & Prasad - Wider, double height spaces create a sense of openness and more inviting street level experience to the Wellbeing Centre while a more compact rhythm of windows maintains privacy for the health facilities at upper levels.



3 Britannia Leisure Centre by Faulkner Browns © Diane Auckland - A triple height colonnade create a welcoming arrival experience and a sense of civic grandeur. Upper levels of the sports centre are differentiated by their materials and structure with sports uses visible from the street.



4 Swiss Cottage Leisure Centre © Farrells - The use of a glass facade allows the climbing wall within the centre become a central feature. This allows passers-by or the users to get a catch a glimpse of the activities happening inside, creating a vibrant and characterful atmosphere. Climbing wall animates what would otherwise be a large, blank facade creating a visual connection between the leisure uses



5 Down Leisure Centre by Kennedy Fitzgerald Architects © Donal McCann Photography - A setback at ground floor and overhang at first floor creates a sense of shelter at the main entrance.



6 Sportzentrum Schulcampus Überlingen © Wulf Architekten - The centre is enveloped in white expanded metal facade, giving the building an elegant appearance. The saw-tooth roof design allows the building to blend into its low-rise residential context yet allowing it to stand out and forge its own identity.



7 Tewkesbury Leisure Centre © Pozzoni Architecture - The scheme is located adjacent to a local council office, and responds sensitively to the local character by using subtle brickwork, timber and glass, yet uses texture and scale to stand out.



8 Alderley Park Sports Centre © Pozzoni Architecture Ltd - Recycled plastic and timber are used to mimic the surrounding woodland, while the Gabion wall utilises stone from across the site to create a strong yet humble and economical aesthetic.



9 Camp del Ferro Sports Center © José Hevia, AIA, Barceló Balanzó Arquitectes, Gustau Gili Galfetti - The centre is setback from the main road with generous levels of investment in public realm through high quality street furniture, lighting and tree planting.



Phoenix Business Park

Site overview

B9.92 The site is occupied by two low rise retail buildings bounded south by M4 and Great West Road, and north by residential units mostly postwar housing estates. To the east the site sits within close proximity to 'West London Audi' a five storey building, and 'Mercedes-Benz retail group fleet sales' both offering car dealerships

Stakeholders

- Screwfix
- Topps Files
- AT&T GB LTD

Suitability and sensitivity issues identified in Part C Types

- Within 400m from a green open space of high quality
- Poor levels of public transport accessibility (PTAL score lower bracket 1a/2/3)
- Within the Great West Road Opportunity Area
- High levels of cycling accessibility

Constraints and opportunities

- The Great West Road, and M4 create poor public realm environment, which provides an opportunity for improved public realm
- The site lies next to the Great West Road (A4); a TFL Red Route where development should not impact the transport network.
- World Heritage Site: Royal Botanic Gardens: Kew, Conservation Area: St Paul's Brentford, Conservation Area: Gunnersbury
- The area's close proximity to the M4, and the Great West Road, can make it feel like a place to pass through, rather than to stop.
- The site is a Locally Significant Industrial Site; proposals within LSIS should intensify uses



Location plan

- ▭ Site boundary
- ▬ Railway line
- Commercial frontage
- Sensitive frontage
- ▲ Commercial access
- ▲ Servicing access
- Car parking
- ⊠ Existing tall building

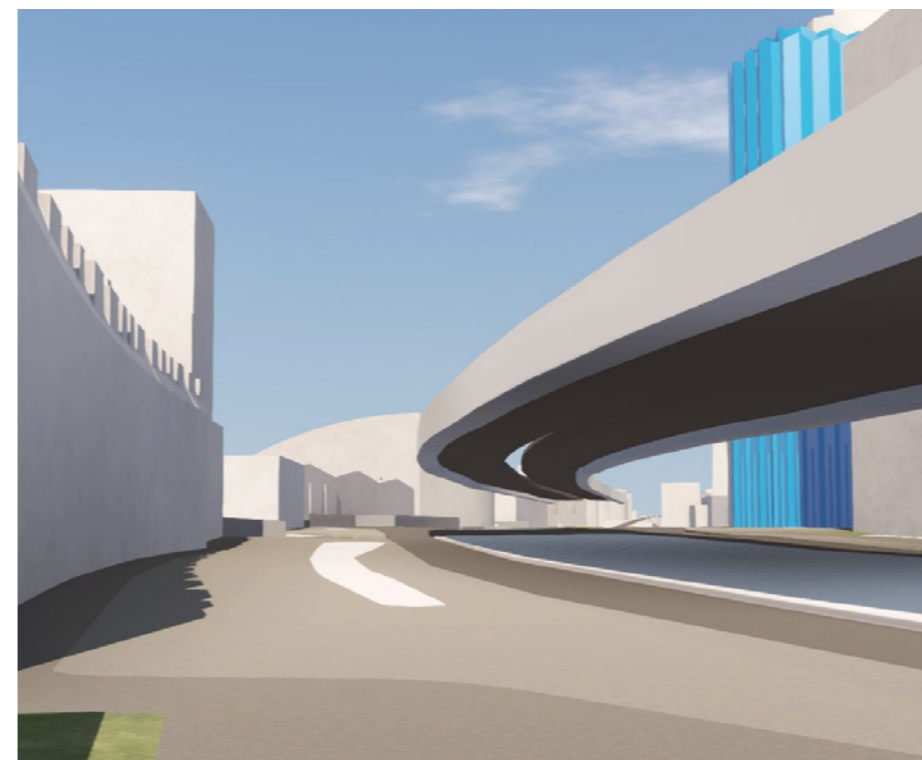
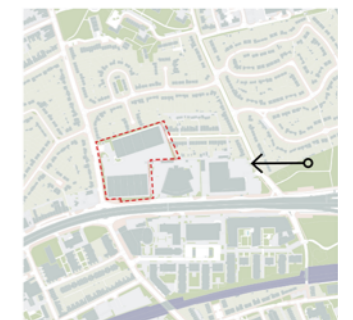
Key information from Site Allocation

Existing Site:	
Source	Emerging single Local Plan Site Allocations
Ownership	Private
Existing use	Research and Product Development, Light industrial, General Industrial and Warehousing
Size	1.6 ha
PTAL	1a/2/3
Prevailing height	2-28 storeys

suitable for broad industrial type activities and make consideration of impact of development on existing occupiers in accordance with the agent of change principle.



Aerial view facing west showing the scale of nearby buildings



Key street level view of the site facing east along the Great West Road



Design guidance

Objectives

- Transformation of the Phoenix Business Park into a mixed-use development which intensifies and diversifies industrial and employment uses.
- Better integration of the site with its surroundings.
- Provision of a range of different, high quality housing types with associated amenity space.

Access

- Existing vehicular access from Ealing Road should be retained and adapted for servicing and new resident-only vehicular access from Springvale Road provided.
- There is potential for a new pedestrian route leading to Chestnut Avenue via Cherry Garth.
- Access for servicing should continue via Ealing Road with a separate egress onto Ealing Road

Form of development

- Linear residential buildings are better suited to the north of the site to repair the incomplete suburban block between Ealing Road and Springvale Avenue.
- Due to the technical requirements and larger format of industrial facilities, a more idiosyncratic form of development is needed.
- This could take the form of a multi-story industrial podium facility with mixed-use buildings and private courtyards above.
- Taller buildings would be suitable in this location but these should be carefully distributed towards the edge of the podium to maximise the efficiency of the structure and industrial floor plate.
- The scale of buildings should step down towards the new inner street to minimise overshadowing and ensure that BRE standards are met.
- Free-form towers which do not form part of perimeter blocks are discouraged.

Relationship to surroundings

- The scale of buildings should respond to transition in character from the low-rise suburban,

residential setting to the north and mid to high-rise setting along Great West Road.

- Residential buildings should be set back or positioned away from the Great West Road to minimise the impact of noise and air pollution.
- Existing mechanical equipment near the perimeter of the site on neighbouring plots should be screened to minimise disturbance to new residents.

Street level experience

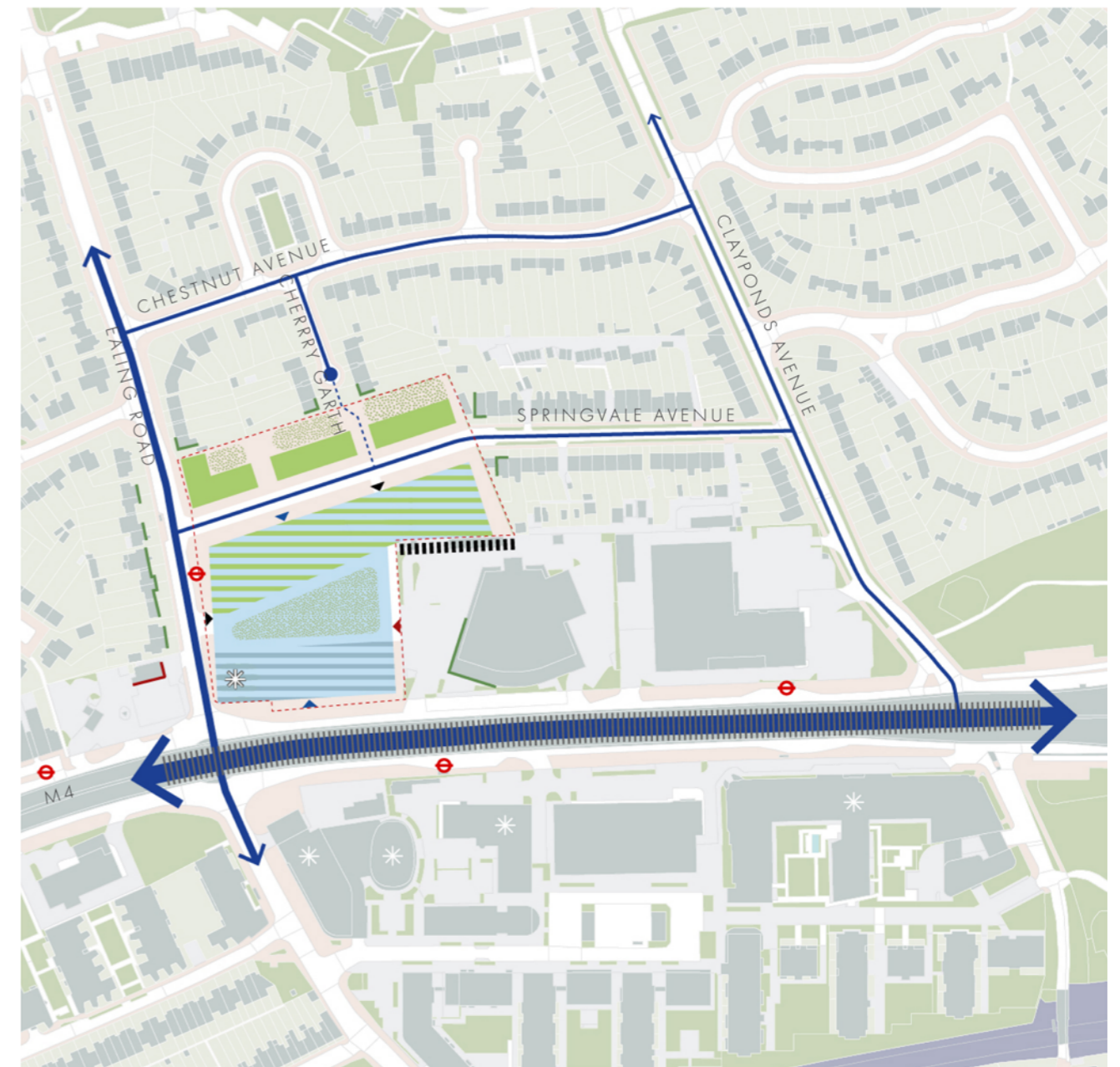
- Missing frontages along Ealing Road should be replaced with active commercial uses and homes with front doors.
- The extension of Springvale Avenue should retain a predominantly residential character and should not allow through vehicular movement.
- Consistent, active frontages should be provided along the Great West Road with entrances and display areas making the internal employment uses visible from outside.
- Proposed developments should avoid overshadowing the new street.
- New streets should have a minimum carriageway width of 5.5m and 2m footways on either side excluding private defensible space.

Wider contribution

- A direct walking and cycling route between Springvale Avenue and Ealing Road should be integrated with the proposed clean air route.
- Improvements to the public realm along Ealing Road and Great West Road should be provided.

Climate mitigation and adaptation

- Setbacks setbacks in the massing of taller elements can help to mitigate the downdraught and the urban heat island effects.
- Tree planting and soft landscaping along Ealing Road and extended Springvale Avenue can mitigate surface water flooding.



Spatial framework

- | | |
|--------------------------------|----------------------|
| Resi/business above industrial | Secondary route |
| Industrial | Primary route |
| Residential | Pedestrianised route |
| Business | Sensitivity to noise |
| Private amenity | Parking access |
| Potential for taller building | Key access |
| Sensitive frontages | Services access |

*For more information on site capacity and building heights refer to the Local Plan and Great West Corridor Masterplan

Good practice precedents

Applicants should refer to the Greater London Authority's Industrial Intensification and Co-location Study (2018) for more detailed design guidance.

1 West Hendon © Allies and Morrison - Private courtyard within a residential perimeter block with high quality landscaping, outdoor seating and pocket play areas for residents. Balconies are oriented inwards to create a sense of intimacy and passive surveillance over the space.



2 Caxton Works © Studio Egret West, SEW and Ben Luxmore - Mixed-use industrial development where a warm toned weathered steel is further softened by moments of glazing and timber joinery for doors and windows.



3 Caxton Works © Studio Egret West, SEW and Luke Hayes - Residential units sit above the employment units, with the changes in levels help the massing to blend in with its surrounding context.



4 Kaap Nord, Amsterdam © VSAP Architects - The scheme offers commercial floorspace with low-rise buildings at the forefront acting as a mediators between the foreground and taller buildings to the rear.



5 Unite and Travis Perkins © Cooley Architects - The scheme offers shops at ground floor facing the street and larger industrial floorspace towards the rear with residential units above. The use of a setback upward extension allows the building to benefit from more floor space, whilst still blending in with its low-rise surrounding context.



6 Minnesota Street Warehouse © Mariko Reed courtesy of Jensen Architects - Supergraphics and can animate and amplify the industrial character of buildings, particularly in or near servicing areas and entrances which can have extensive blank, or inactive frontages.



