

May 2024

HOUNSLOW CHARACTER, SUSTAINABILITY AND DESIGN CODES SPD

PART B6 CENTRAL HOUNSLOW

 London Borough
of Hounslow

Allies and Morrison
Urban Practitioners



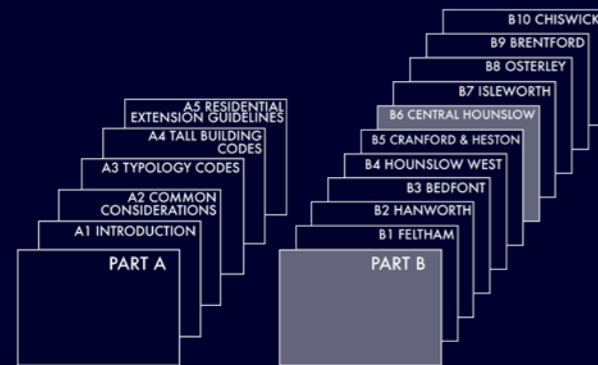
PLACES

Document structure

This Character, Sustainability and Design Codes SPD is comprised of the following documents;

- Part A: Introduction & Design Codes
- Part B: Places

These are individual files that must be opened separately to view. Use this diagram to help navigate the structure of the Character, Sustainability and Design Codes SPD.



FELTHAM

HANWORTH

BEDFONT

HOUNSLOW WEST

HESTON AND CRANFORD

CENTRAL HOUNSLOW

ISLEWORTH

OSTERLEY

BRENTFORD

CHISWICK

* This document is **Central Hounslow**
To view other places please open the relevant documents as named above.

Introducing Hounslow's places

Neighbourhoods and Places

- B6.1 This section, Part B, explores character at a scale best understood by local people - at the neighbourhood and place level. The Borough is comprised of a series of places and neighbourhoods which each have a subtle, or sometimes distinct, character of their own. The map overleaf shows an interpretation of Hounslow's neighbourhoods. It has been developed using the previous study areas set out in the 2014 characterisation, together with local residents.
- B6.2 Identifying where neighbourhoods begin and end is inherently subjective - there will be overlaps and blurred edges between boundaries, even disagreements, as places in the Borough mean different things to different people. Influencing factors like physical infrastructure (roads, railway lines), natural assets (waterways, open spaces), local parades and centres, ward boundaries, can all help to inform the drawing of neighbourhoods that make sense. The degree of consistency or diversity of different aspects, like a particular typology or ethnic group, is also key to how neighbourhoods are perceived.
- B6.3 The purpose of dividing the Borough into these smaller areas is to provide detail on the special qualities and specific challenges for local people in each neighbourhood. This will help to ensure future growth is fairer, greener and more inclusive, and will help shape policies to ensure that the existing identity and character of neighbourhoods is a key consideration. The 37 neighbourhoods have been grouped into ten larger places. The neighbourhoods have been grouped by similarities in character and sense of place informed by: discussions at community

workshops to understand Hounslow's diverse communities; the mapping of physical assets such as topography, landscape and urban morphology; the historic evolution of each area; analysis of land use and housing typologies and their future growth direction.

Structure of Part B - Places

- B6.4 Part B describes the ten places in the Borough. This chapter has four sections:

1. **Place-level analysis** - this section sets out the baseline analysis for the place and the vision for the place, which has been shaped by the contribution of local people through an online survey and workshops, and have been developed by considering:
 - What are the area's key assets that people love and that should be celebrated and conserved?
 - What are the current social, economic, health and environmental issues of a particular area that we want to address?
 - What opportunities for growth exist in this area and where? This includes thinking about planned investment in infrastructure, Council aspirations for growth and wider trends e.g. changing role of high streets, shift to active travel informed by existing or proposed transport hubs/links/interchanges.
2. **Neighbourhoods** - this is design guidance that sets out the special qualities for each neighbourhood in turn, and what that means for development in the area. Each neighbourhood page includes photos, key materials and features, the distribution of typologies and where possible, quotes from

local people to help convey what makes each neighbourhood unique.

3. **Sites** - this section draws on the design codes set out in Part A and for the Neighbourhoods to provide design codes at a site-specific scale on key sites identified by the Council.
4. **Town Centre** - this section sets out specific design guidance for the town centre, drawing on the town centre visions and masterplans, to deliver design improvements in the town centre.

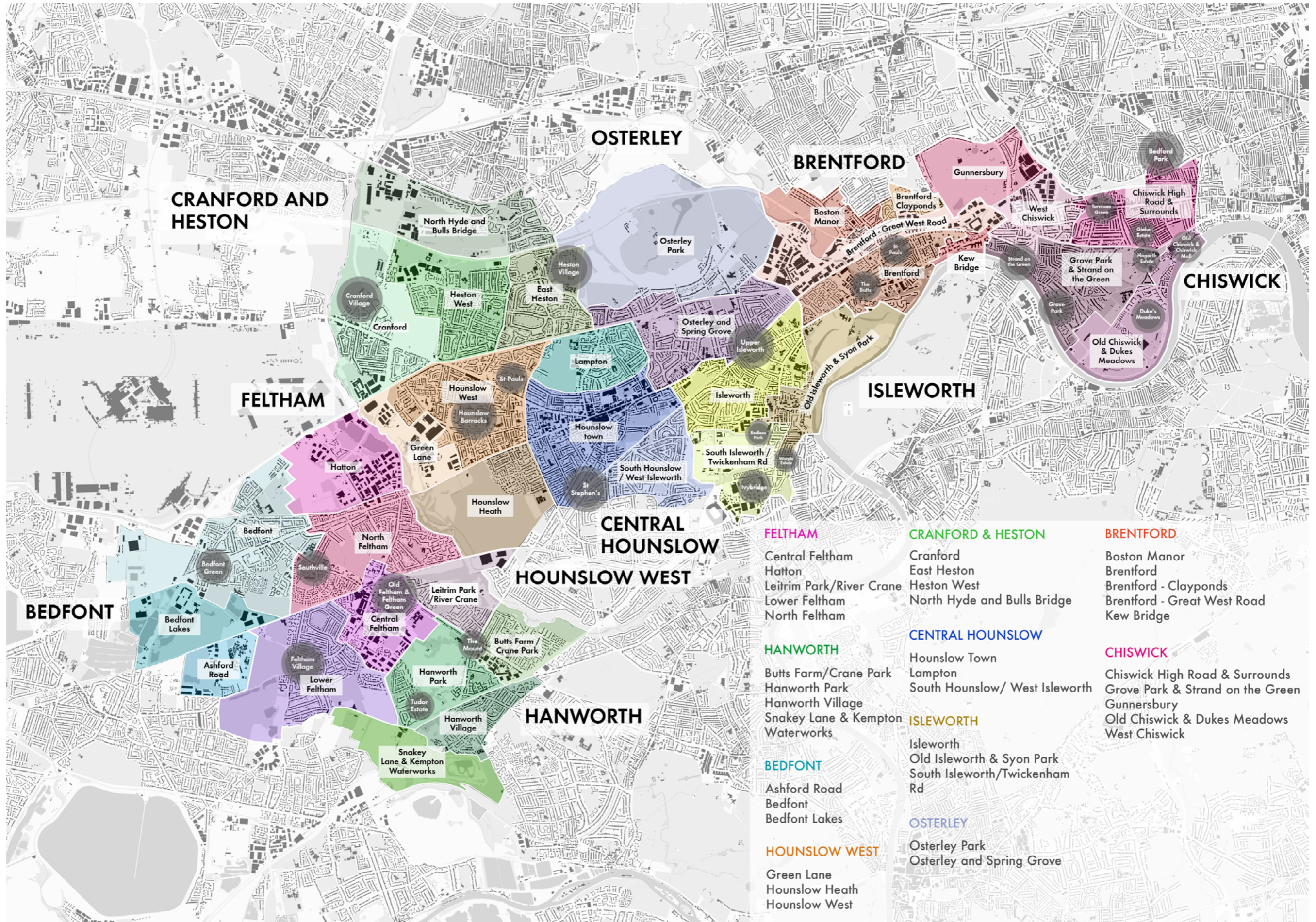
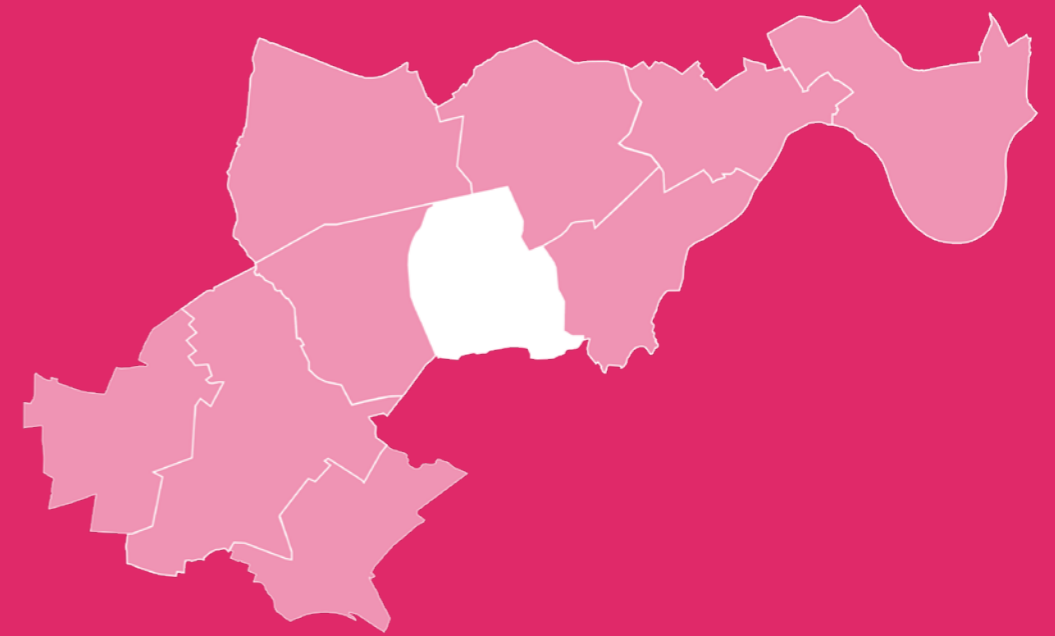


Fig B6.1 Hounslow's neighbourhoods

CENTRAL HOUNSLOW



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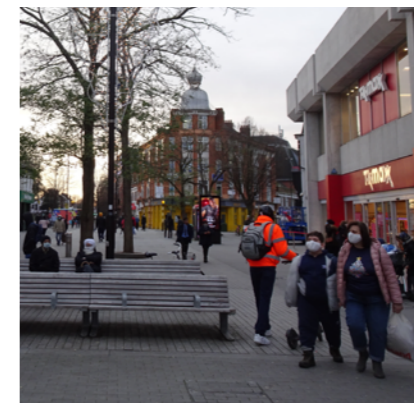
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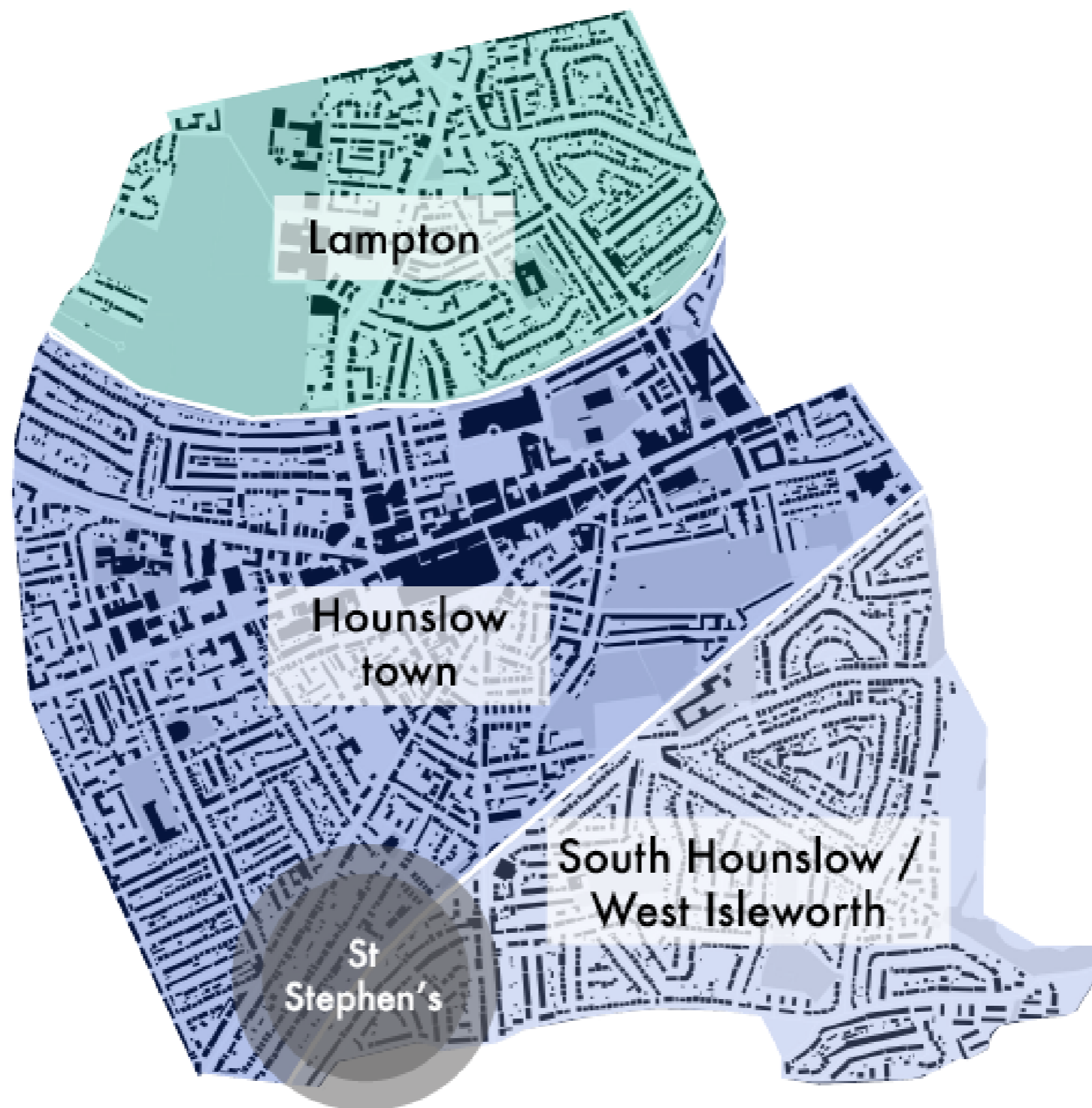
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Towards a greener, healthier, cleaner, safer, thriving and liveable... Central Hounslow

B6.5 The area is centred on the ancient Staines Road; Hounslow became an important stopping point for travellers heading into and out of London, with regular markets and facilities. It continues to have a buzz and lively atmosphere today. By the 1860s, the core High Street was very much established along this route, as well as buildings on larger plots along Bath Road, and terraced streets to the south that came along with Hounslow Station. Lampton village grew up to the north west, now marked by the Great West Road and Lampton Road junction. The District Railway arrived in the 1880s which led to rapid development, and other Victorian quarters were developed in a piecemeal fashion around this time to the south. The Great West Road cut through Lampton village in 1925 which dramatically altered its character but the area still retains some old buildings of character like The Lawn which was built in 1810. More recently the town centre has seen waves of post war change.

B6.6 Central Hounslow is focused on its large, vibrant and bustling town centre, the 'capital for Hounslow'. Accessibility within the town centre is excellent, within the town centre and it's immediate vicinity having high PTAL levels (6a). The south eastern portion of this area suffers from poor transport accessibility. Similarly to other parts of the borough, air quality is poorest along major routes. Air quality within the town centre is improved, possibly as a result of the partial pedestrianisation of the high street.



Fig B6.2 Heritage. Blue areas show conservation areas. Red hatch shows archaeological priority area.



Fig B6.3 Proximity to centres (15 min walk) Light blue shows areas within a 15 min walk from a centre (dark blue)



Fig B6.4 Air Quality Poorer air quality is found along the major routes

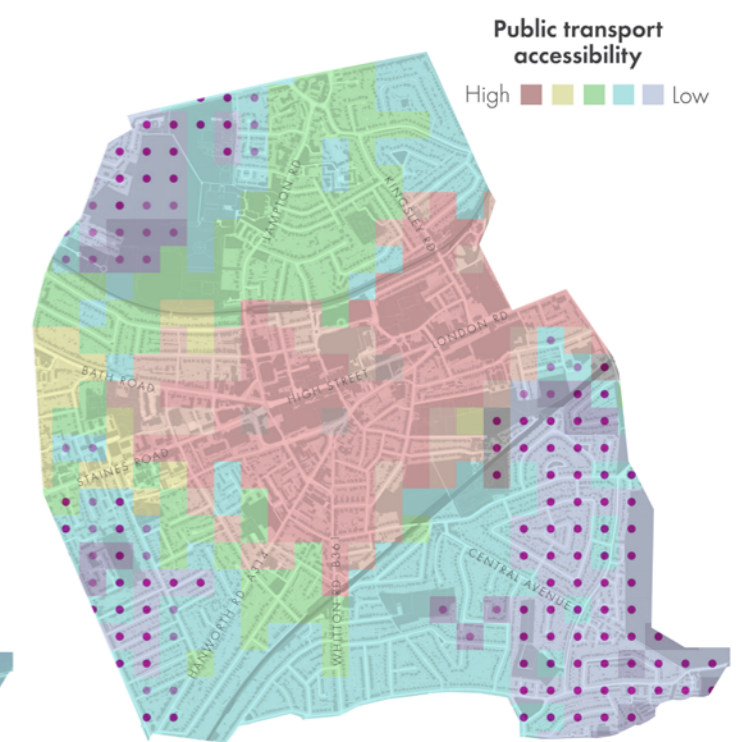


Fig B6.5 Public Transport Accessibility Levels (PTAL)

B6.7 The area's residents are incredibly diverse, although there are major nuances within the area - over 70% of Hounslow Town and Lampton residents are from BAME backgrounds, compared to 45% south of the railway line in South Hounslow/West Isleworth. Above the railway line the median age of residents is 31 making it one of the youngest places in the Borough. This increases to 37 on average in South Hounslow/West Isleworth neighbourhood (2011 Census).

B6.8 The majority of residents in Hounslow Town and Lampton are Indian. The areas also have large Pakistani and Polish communities too - Polish residents also being a notable group in South Hounslow/West Isleworth. There are a number of Filipino and Burmese residents living in Hounslow Central ward (2011 Census). The area is highly religious - Hounslow Central ward has the greatest proportion of Hindus and Buddhists in Hounslow, and Heath ward has the highest proportion of Muslims. There is a greater proportion of residents who are Christian or who have no religion living in Hounslow South ward (2011 Census), compared with the rest of the Central Hounslow area.

B6.9 The area has the highest proportion of people living in multi-generational households in the entire borough, many of whom are from Indian ethnic backgrounds. Hounslow Central ward also has the highest proportion of people living in overcrowded conditions of any ward in the borough (35.6% of residents). Crucially, new development must seek to respond to the specific housing needs of locals in this part of the Borough.

B6.10 Whilst Central Hounslow has a number of important parks and green spaces, most of them dating back centuries, there is an issue of open space deficiency and a lack of street trees, it does not have any strong green corridors as a backbone to its green infrastructure and environmental network. Rail corridors provide wildlife links across the area, but it falls to the key routes like Staines Road, Lampton Road and Inwood Road to perform both the movement and green infrastructure links between the area's green assets.

B6.11 An important part of the vision is to improve walking and cycling connections to existing green spaces like Lampton Park and Hounslow Heath, and to introduce new pocket parks, play areas and street greening to help support a healthy and happy local population.



Fig B6.6 The number of street trees on Hounslow High Street has been increased over recent years, but there remain many other streets and spaces that need new and well managed trees.

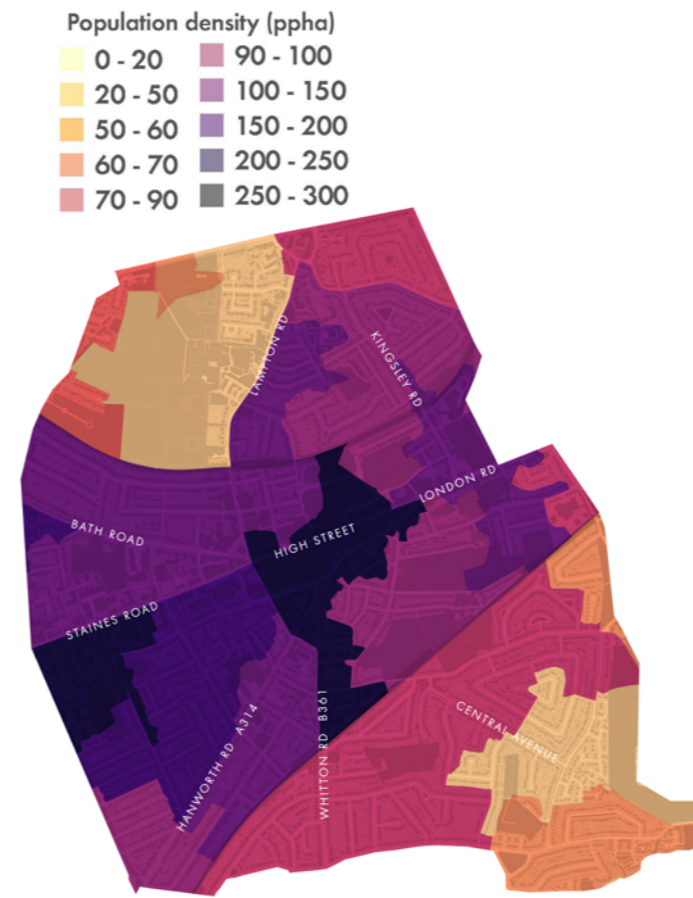


Fig B6.7 Population Density. Darker shades indicate a higher density.

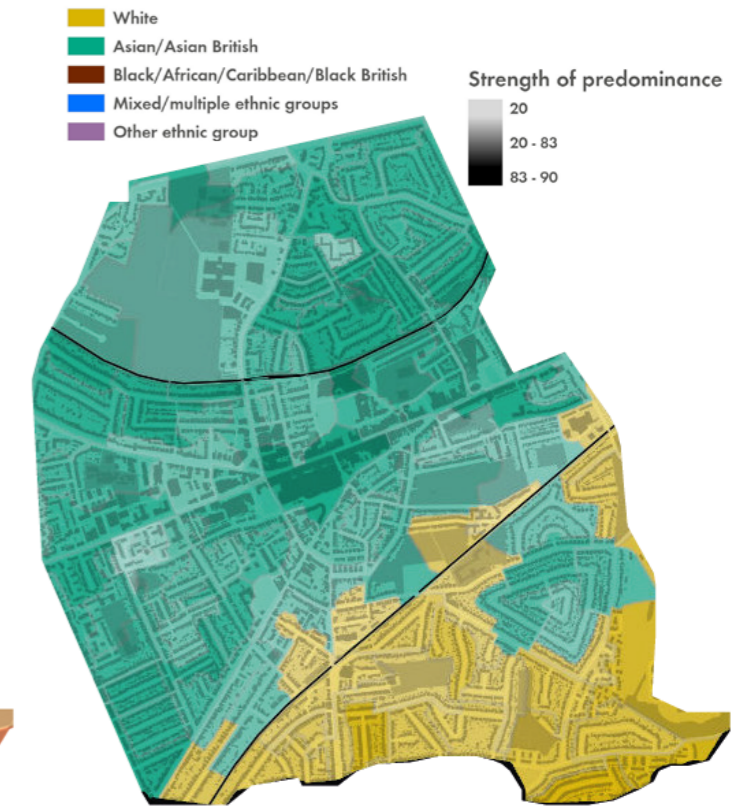


Fig B6.8 Ethnic groups predominance.

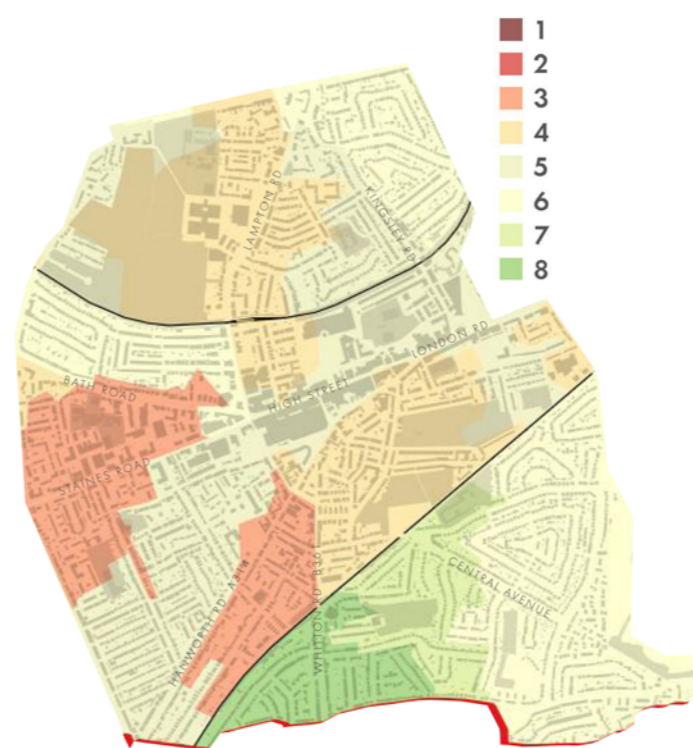


Fig B6.9 Index of Multiple Deprivation Decile (where 1 is most deprived 10%). Red shows more relatively deprived areas, dark green shows least

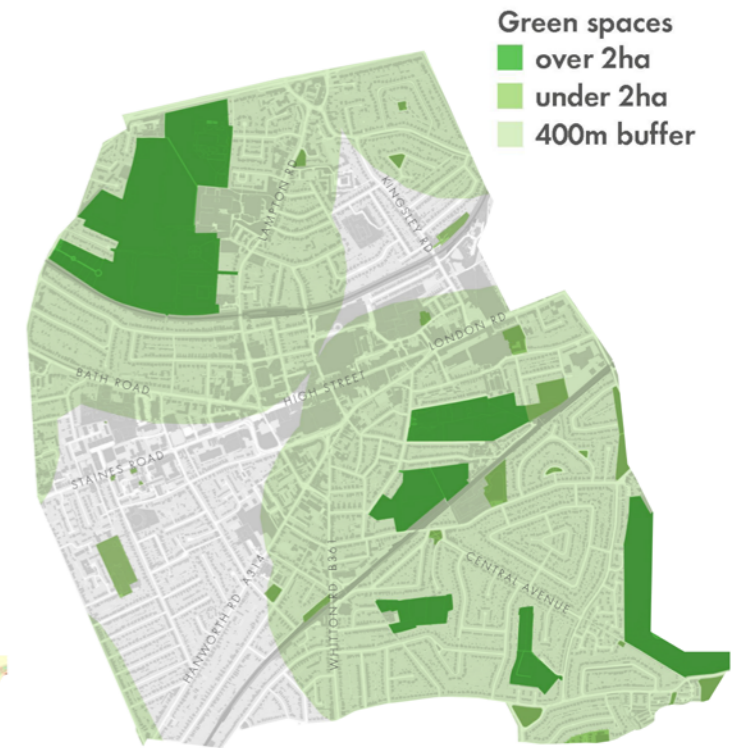


Fig B6.10 Access to green spaces. Dark green show open space, light green shows 400m radius from green space.

B6.12 As Figure B6.11 illustrates, the residential parts of this area have an existing estimated Urban Greening Factor of around 0.5, however the street blocks within the town centre are considerably lower with an average score of 0.1. It is imperative that the town centre is significantly greened to ensure it can adapt to future climates and be a comfortable and attractive place to spend time in the future.

B6.13 Most of the Central Hounslow area is identified as an urban greening priority area in the Green Infrastructure Strategy, and therefore all developments must make an additional contribution to help improve urban greening overall in this area. It is suggested therefore that in this area, sites must achieve the minimum Urban Greening Factor target (see Urban Greening Factor code in Part A2) on site and then contribute towards greening in the public realm.

B6.14 The mix of old and new within Hounslow Town Centre contributes to its character. Some parts are quite fragmented and there are significant opportunities for future growth to address this and help to shape and redefine the character of the area in a way that promotes exemplary contemporary development whilst also staying true to its historic character and distinctiveness. In other more intact neighbourhoods, the vision for change is focused on smaller, more subtle interventions like sensitive infill opportunities and public realm enhancements that help enhance local character.

B6.15 The existing buildings across the area are predominantly two to four storeys. In Hounslow Town Centre building heights are higher with largely five to eight storey developments as well as taller buildings over 10 storeys.

B6.16 Whilst much of the recent development has been in the form of modern apartment blocks, the dominant residential typologies throughout this area include urban terrace and mansion blocks. There is much that can be drawn from this existing character - reinventing these typologies to achieve denser and more resilient intensification could be very beneficial for Central Hounslow. New development should aim to achieve a Floor Area Ratio of at least 1.5 or 2.0. This may require a 50% uplift in building heights in some locations.

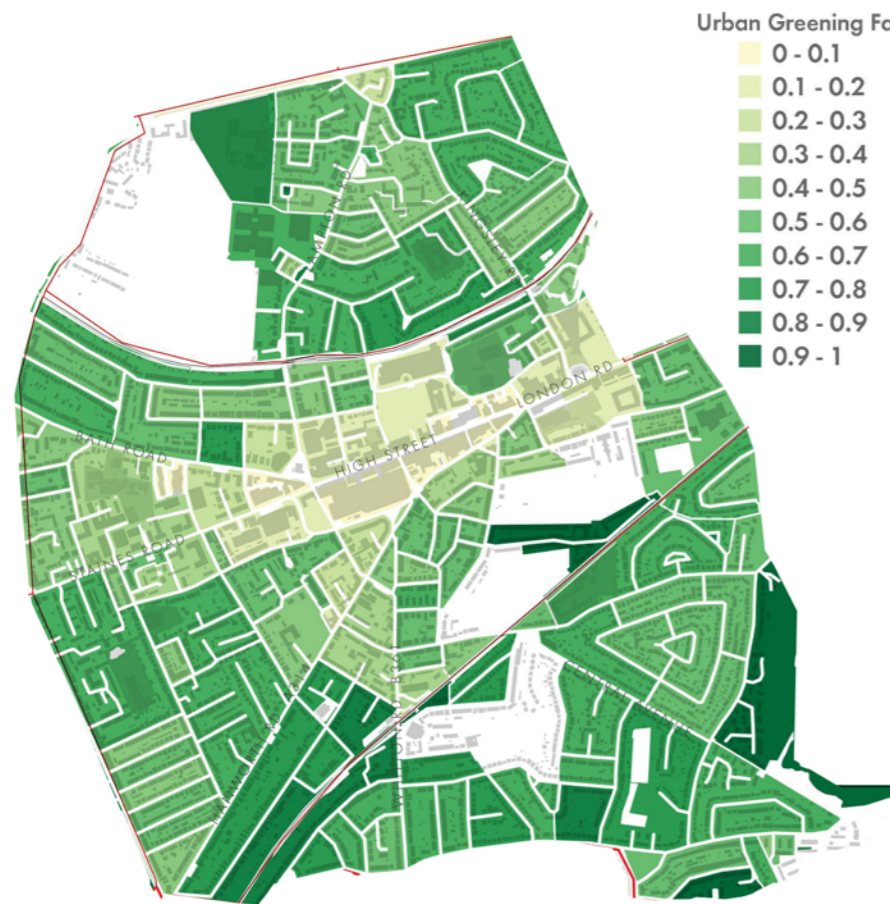


Fig B6.12 Higher density mid rise forms of development which support family units and good outdoor space are appropriate in this area. Precedent example: Mansion blocks in South Kilburn Estate © Alison Brooks Architects

Fig B6.11 Strategic Urban Greening Factor analysis of existing street blocks - this measurement includes the contribution of street trees and planting in the public realm as well as greenery within plots. The adopted London Plan suggests a minimum score 0.3 to support climate adaptation.

Fig B6.13 Approximate existing building heights 2020.

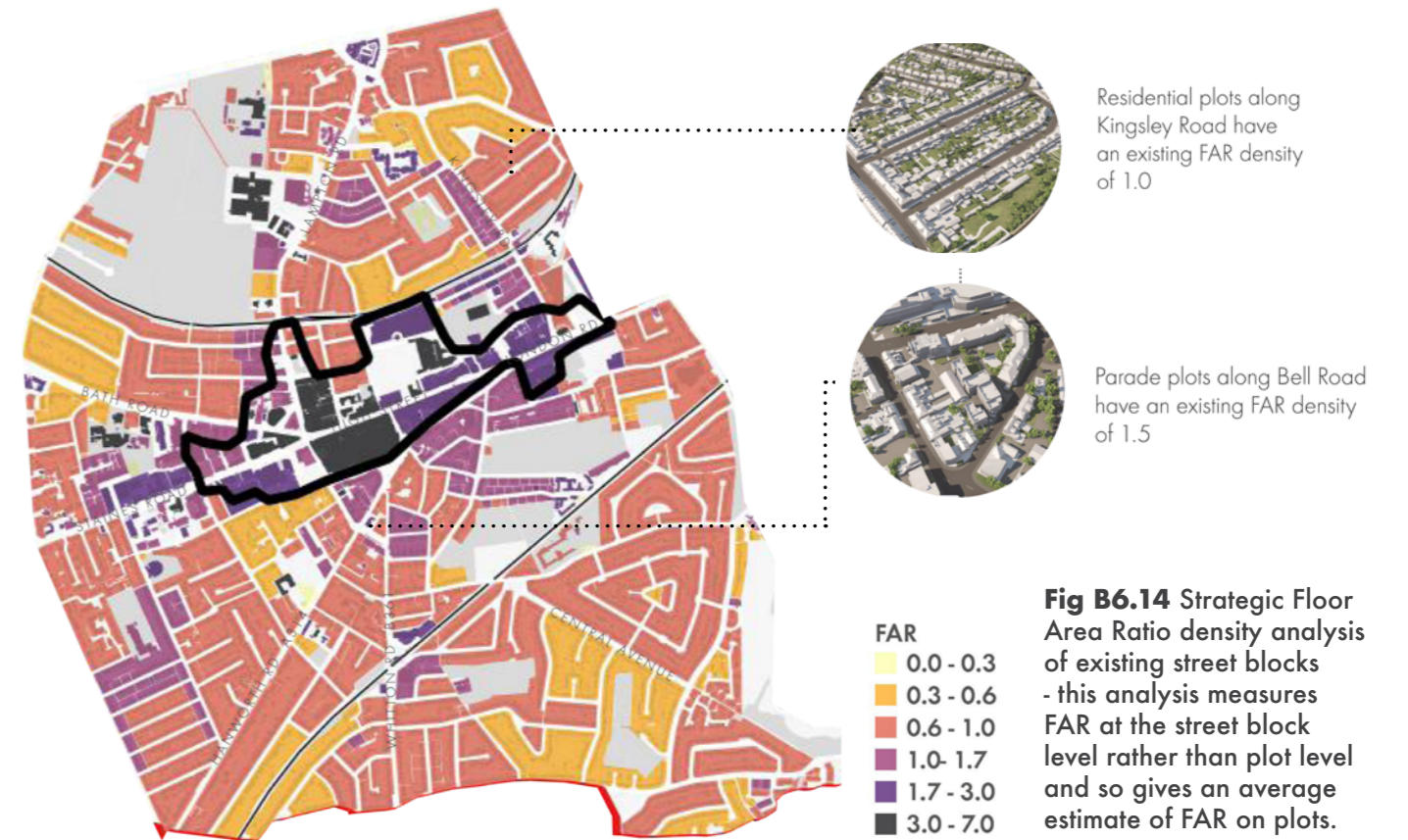
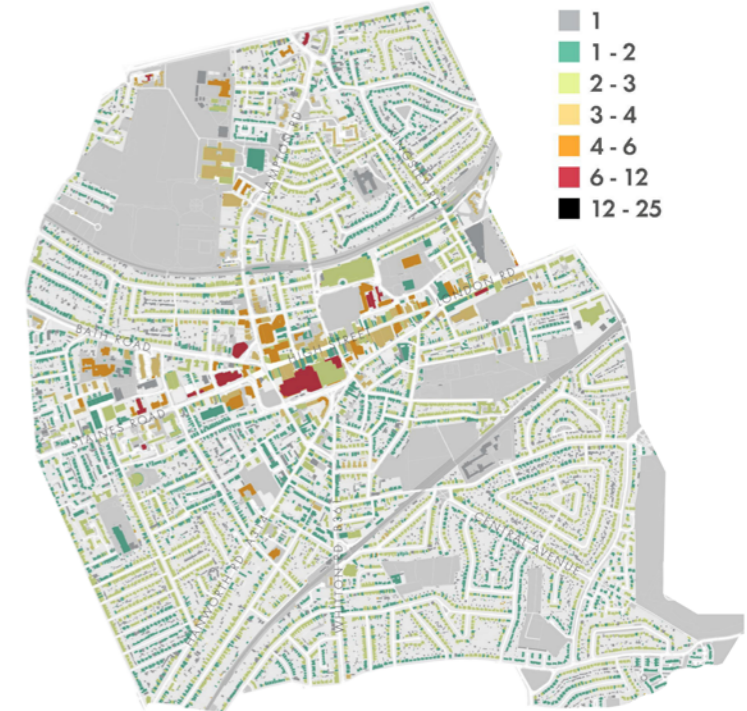


Fig B6.14 Strategic Floor Area Ratio density analysis of existing street blocks - this analysis measures FAR at the street block level rather than plot level and so gives an average estimate of FAR on plots.

B6.17 For a greener, healthier, cleaner, safer, thriving and liveable Borough, Central Hounslow should transition towards:

1. **Transforming the character of Hounslow Town Centre.**

There are significant opportunities to help shape and redefine character of the town centre, in a way that is contemporary, protects existing historic elements, and that is distinct to Hounslow. This can be achieved through exemplary new development, re-purposing existing buildings, and investment in the town centre's public realm. An uplift in density will be expected, generally in the form of mid-rise development but there may be opportunities for an increase in height in certain parts of the town centre, for example concentrated along main routes and close to public transport. The town centre supports a form of new development that is

finer grain than in other parts of the Borough, and the emphasis should be on this and maintaining and creating routes through to achieve better links. The town centre will be diversified and welcoming for all, with a mix of good quality shops, cafés, restaurants, leisure activities and cultural venues that enhance Hounslow as a vibrant evening destination, in addition to schools, clinics, civic and faith buildings, homes and workspaces to establish the centre as a leading location for business.

2. **Significant greening of Central Hounslow through establishing green network of routes and spaces.**

There are opportunities to provide new pocket parks, play space, exercise zones, planting and trees in the town centre. Public realm enhancements will help to improve the quality and legibility

of connections to green spaces beyond the town centre at Lampton Park, Hounslow Heath and other smaller green spaces. Improvements to existing green spaces, including Inwood Park, Kingsley Road open space, Gainsborough Gardens and Hounslow allotments, will support greening measures, as well as interventions to re-green streetscapes and existing residential developments where possible. Maximising and integrating urban greening into new development is also encouraged. Where hard surfacing is used, it should ideally be permeable, or seek to integrate SUDS.

3. **Improving wayfinding, and walking and cycling connections into the town centre from nearby neighbourhoods and train stations.**

Public realm enhancements and creative wayfinding and signage will help improve the quality and legibility of routes into the town centre on foot and bike e.g. Hanworth Road / Grove Road. Suitable wheelchair access should be provided on pedestrian routes, including pavements, where possible. Better connections are needed particularly to the south east where the railway line acts as a barrier to movement, and between train stations/stops and the town centre e.g. along Whitton Road.

4. **Establishing a new local hub at Hounslow Train Station.**

Hounslow train station is the proposed terminal for the West London Orbital. There are opportunities to transform the character and role of the area around Hounslow Train Station, by providing new and intensified uses and activities to serve South Hounslow's neighbourhoods. Public realm enhancements will help establish a new local hub, including tree planting, facilities for cyclists and ensuring easy access for pedestrians. An uplift in density will be expected, generally in the form of low to mid-

rise development that sits well with its suburban character. The station is a listed building, the setting of which needs to be considered in any proposal.

5. **Evolving the potential of London Road and Bath Road and enhancing the quality of these old routes.**

Sensitive infill development will help to improve the character and quality of these key routes. Strengthening edges by repairing gaps, providing a consistent building line and improving the quality of the public realm will help to contribute to the overall cohesiveness of the street. Historic buildings and features will be protected and restored, and public realm enhancements will help support active travel. An uplift in density will be expected along these key routes, including opportunities for an increase in height in appropriate locations. New housing should respond to needs and wants of local people, exploring new ways of delivering multi generational housing, embedding community facilities into new developments as well as private and communal outdoor spaces.

6. **Continuing to invest in Lampton Road's local parades.**

There are opportunities for shop-front improvements along Lampton Road including around Hounslow Central and at the junction with Great West Road. This should be supported by public realm enhancements to make these local parades safer and more attractive, including introducing greening, removal of street clutter and improved crossings.

7. **Enhancing energy performance and moving toward cleaner energy solutions.**

The overall energy

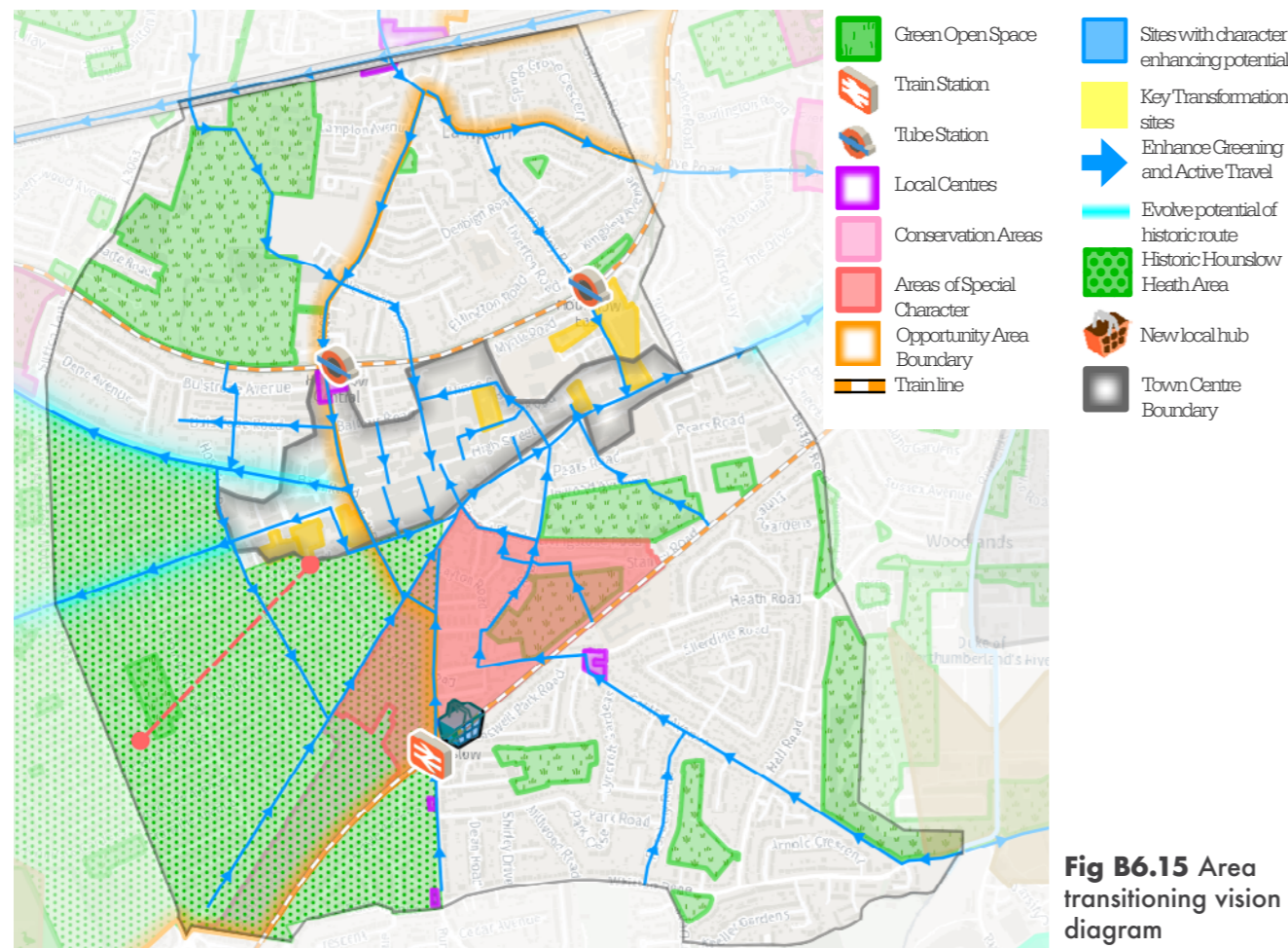
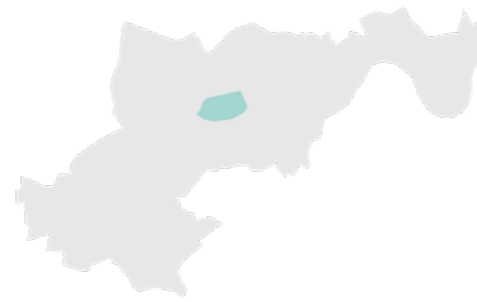


Fig B6.15 Area transitioning vision diagram

performance of the area will be improved through encouraging the retrofitting of existing properties with insulation and by advocating the use of schemes offered by the UK government which residents may be eligible for. Furthermore, in areas which will see notable new development such as Hounslow Town Centre, District Heat Networks should be introduced, and new development should be connected to these in order to receive clean and efficient energy, contributing to London's net zero carbon target.



Lampton



What's special about Lampton?

- B6.18** Lampton is a residential area focused on Lampton Road and Kingsley Road with Lampton Park at its heart. The neighbourhood grew up around Lampton village, where today's local centre exists around Jersey Parade at the junction with Great West Road, Spring Grove Road and Lampton Road. The area developed rapidly in the first half of 20th Century with terraced and semi-detached residential streets. South Asian communities migrated to the area in the 1960s and 1970s, and today the Indian community make up the majority population in Lampton (2011 Census). There is also a significant concentration of Pakistani, Black African and Polish residents. The area falls within the Heston East ward and Hounslow Central ward. Heston Central has the highest proportion of multigenerational households.
- B6.19** Local community and faith facilities include Lampton Children's Centre, Lakshmi Narayan Temple, Lampton School and Sports Centre. Alexandra Primary School is located off Tiverton Road serving local residents. The area is in close walking distance to Hounslow town centre and Asda Hounslow is a popular local trip for a food shop. Corner shops on Kingsley Road and Great West Road are also important for meeting local shopping needs.
- B6.20** Lampton Park is an important local natural asset and includes tennis courts, playing fields and a play area. Many surviving 19th Century buildings are found around its edge. Lampton

village has lost much of its historic fabric, but there are opportunities to protect remaining assets and also enhance the shopping parade on Great West Road / Lampton Road junction.

Prompts for character-led development

- New development must maintain and further support the emphasis on Lampton Road and the local centre, enhancing frontage and delivering sensitive intensification. Where applicable, the consistent set back building line and large green frontages should be maintained along Lampton Road to retain the area's green character. Development in the west of the area should seek to improve connections into Lampton Park and enhance the edges of the park.
- The existing building height range in this neighbourhood is 2-9 storeys approximately. New buildings should be between 2-9 storeys in height, subject to review. Any proposals for tall buildings should refer to the Local Plan.
- This is a highly accessible neighbourhood and adjacent to Hounslow Town Centre. Future development needs to ensure these benefits do not lead to over-development or insensitive intensification. Sustainable travel enhancements are needed alongside development. Many properties have already seen rear and roof extensions and further alterations should make a positive contribution to front / back garden green infrastructure.
- Positively encourage retention of landscaped front gardens including existing brick wall or hedge boundary treatments, where these form

Selection of locally distinctive architectural details and materials which may inform new developments



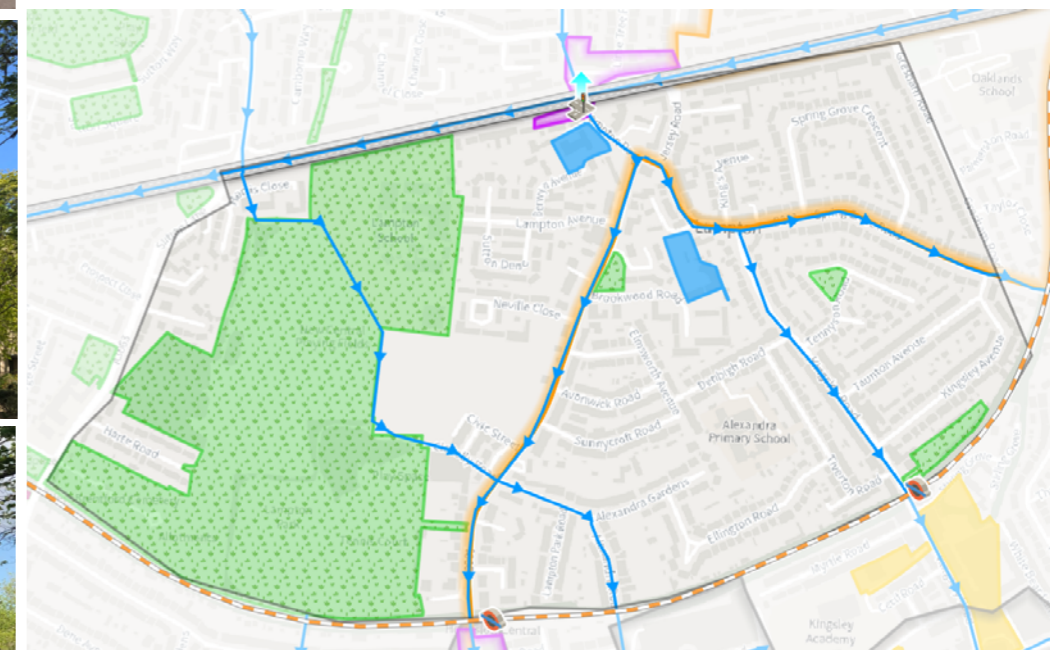
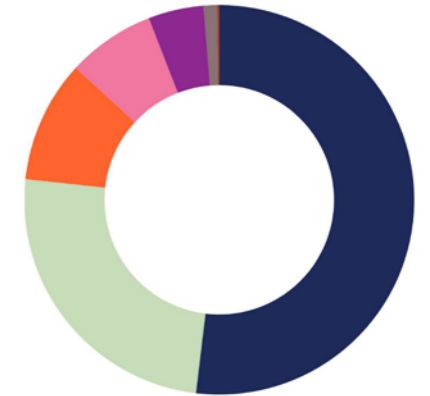
Red brick and white frames Terracotta tiling and roof rhythm London stock brick Deep window reveals Inset balconies New communal green spaces



WHAT IS CENTRAL HOUNSLOW'S CHARACTER?

part of the prevailing character of the streetscape. Where hardstanding or driveways are proposed, permeable surfacing is promoted and retention of some soft landscaping within the wider front garden is encouraged.

- All development sites must also make a contribution towards greening in the public realm on key streets to address the urban greening priority area identified in the Green Infrastructure Strategy. This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.



Plan of neighbourhood

Hounslow Town

What's special about Hounslow Town?

- B6.21** Hounslow Town takes in the Borough's largest town centre as well as residential areas on either side, including St Stephen's Conservation area to the south. Over the years the High Street has evolved from a through-route along the historic Staines Road to a partly pedestrianised area whilst still retaining many old buildings. Today the bustling and multicultural centre is a focal point for the communities surrounding it.
- B6.22** The centre caters for a really diverse mix of uses - an assortment of national retailers, local stores and businesses, cafes and food outlets, including a popular food court. Office spaces sit above retail units. The variety of shops and services reflect the diversity of Hounslow Town's residents. 76.6% of Hounslow Central ward residents are from a BAME background. The majority of locals in the area are Indian, followed by White British and Pakistani. There are also significant Polish, Nepalese and Burmese communities who live in the area (2011 Census). The area has the highest proportion of multi-generational households in the Borough and the highest proportion of Hindus living in the borough (26.4% compared with the Borough average of 10.3%).
- B6.23** Hounslow Town, Grove Road, and The Orchard are the local primary schools. Kingsley Academy serves the wider Hounslow community. Other facilities include the Civic Centre, Hounslow Muslim Centre and Gurdwara Sri Guru Singh Sabha tucked within a business park off Alice Way, and which is a key focal point for Hounslow's Sikh community.
- B6.24** Residential areas are found to the north and south of the high street providing a mix of homes,

Selection of locally distinctive architectural details and materials which may inform new developments



Shop awnings B6-27 Red brick Adapted old building



from Victorian and Edwardian terraces, to inter-war suburban housing. Some recent developments, such as the High Street Quarter and Pears Road/Selbourne Avenue have added further modern typologies into the mix. Nearby green spaces include Inwood Park, with a play area and cycling hub and Lampton Park on the northside of town. Inwood Road allotments provides growing space for local residents.

- B6.25** Hounslow Town is close to three stations - Hounslow mainline, Hounslow Central and Hounslow East, providing good public transport accessibility. It is also the hub for a network of buses helping local people to get around the Borough and further afield.

Prompts for character-led development

- The existing building height range in this neighbourhood is 2-24 storeys approximately. Further guidance on density and height for new development within the town centre is given in the Hounslow town centre design codes. Any proposals for tall buildings should refer to the Local Plan.
- New development should respect the historic building line and grain of the neighbourhood.
- Positively encourage retention of landscaped front gardens including existing brick wall or hedge boundary treatments, where these form part of the prevailing character of the streetscape. Where hardstanding or driveways are proposed, permeable surfacing is promoted and retention of some soft landscaping within the wider front garden is encouraged.
- Proposals should ensure provision of adequate lighting to enable suitable night time visibility to enhance perception of safety in the neighbourhood.
- Interventions should enhance signage to improve wayfinding from the town centre and stations and improve the quality of shopfronts along the high street and, if a change on level is present, provide a non-slip ramp to access shops or other services, where feasible.
- All development sites must also make a contribution towards greening to address the urban greening priority area identified in the Green Infrastructure Strategy. This will require a considered analysis of existing green infrastructure and a concerted effort



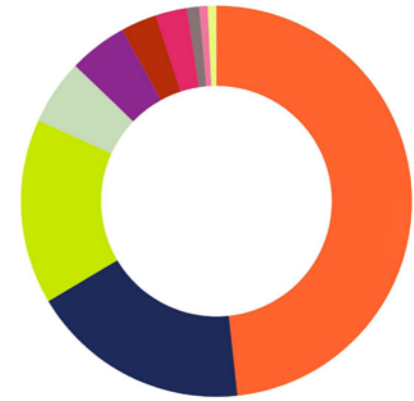
WHAT IS CENTRAL HOUNSLOW'S CHARACTER?

to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.

- The Treaty Centre plays an important role for residents, though its blank frontage and the dominating traffic environment present an opportunity for enhancements, including for public and highway safety. Redevelopment of the Treaty Centre should consider onward connections both during the day and evening, including bus stops, stations, and parking. Along the high street some underused sites represent opportunities for intensification to enhance the shopping, food and drink, community, leisure and workspace offer.
- Contemporary, high quality design is to be encouraged. Proposals should respond to the needs of the local community regarding the types of homes provided and the provision of green and community space and ensure they are wheelchair accessible. Proposals should be informed by the emerging town centre masterplan.

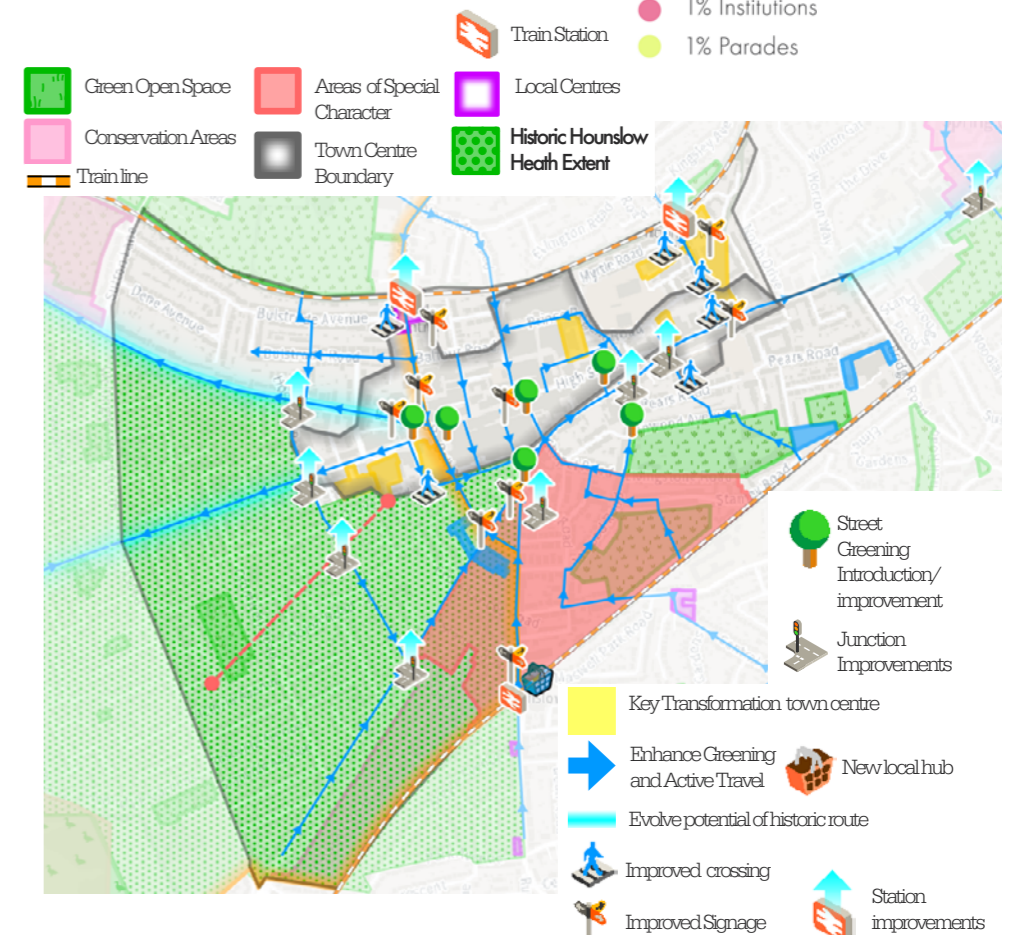
"The variety of food shops is the best thing about Hounslow High Street"

- Resident's view on what's special



Neighbourhood typologies

- 48% Urban terrace
- 18% Suburban
- 15% Centres
- 5% Green space
- 5% Cul-de-sac
- 3% Modern urban
- 3% Industry
- 1% Slabs
- 1% Institutions
- 1% Parades



Plan of neighbourhood

Hounslow Town Centre Design Codes

Built Form

Shaping and redefining a distinct character & enhancing the existing townscape and adding further variety and interest through new markers and architectural quality...

B6.26 There is less of a consistent and coherent material palette in the town centre, as there is a wide variety of materials and finishes used. Any materials should be robust and innovation is welcome, there are opportunities to explore the use of more unique materials such as glazed brick. Developments should take inspiration from the proportion, scale and roof form of the existing buildings but be innovative with materials.

B6.27 High quality detailing is expected, and the creation of character with the use of projecting features, banding and careful detailing around window reveals, entrances parapets and roof lines. Any advertising or signage should be integrated into the design early on rather than an afterthought.

Active and welcoming frontages...

B6.28 Clearly distinguishable residential and non-residential entrances should be provided on the high street frontage if possible, and celebrated with a welcoming design. If residential entrances are located to the rear or side of plots, these must also be visible, safe create a good sense of arrival and provide natural surveillance and security.

B6.29 Developments that include new shop frontages should be high quality, have character and aim to provide some cohesiveness to the High Street. Applicants should consider the integration of community facilities as part of proposals. Applicants should demonstrate how proposals can be easily adapted between uses, particularly at ground floor to build in resilience.

B6.30 Development should consider how it responds to the front, sides and back of the site as frontages facing onto the High Street will require a different response to those facing the roads or alleyways running parallel to the High Street such as Hanworth Road, Prince Regent Road or Matisse Road. End of parade sites should provide frontage

and/or fenestration to the flank wall, to provide natural surveillance and security.

B6.31 Projecting balconies on the High Street frontage or to side streets are not permitted and all building elements should be within the ownership red line boundary. Inset balconies are preferred. Balustrade designs should incorporate privacy measures and take opportunities for innovative, characterful design.

Improve fringe and backland areas and returning to a finer urban grain in the town centre...

B6.32 Development should maximise opportunities for active frontage on all sides of the building, particularly if it faces onto one of the small alleyways/ routes that run perpendicular to the high street. Servicing requirements must be integrated at an early stage and doors should be kept to a minimum and not compromise the openness of frontages or the quality of residential entrances.

B6.33 Development should carefully consider how residential elements are incorporated into mixed use schemes, as the narrow nature of plots facing onto the high street present challenge for amenity requirements, outlook and overlooking, overshadowing. We encourage adjacent plot owners to come together with land assembly proposals to produce more workable development sites. It will be important to maintain permeability and a fine grain in any land assemble schemes.

B6.34 The transformation should recognise the strength of the finer grain historic structure of the centre. A move away from the larger footprint, big pieces of development, towards street-focused buildings will underpin the evolution of the town centre. The Treaty



Fig B6.16 17/18 Britton Street. (Left) Example of a new facade that works within a street frontage context. (Right) Example of a residential entrance integrated into the street frontage. ORMS.

Centre is a key development site to improve the character of the town centre. Development here should include N-S routes to connect the High Street with Grove/Hanworth Road.

Density and Height

Make the high street centre stage and maintain a continuous High Street frontage celebrating the Roman Road and human scale of this route...

B6.35 Where individual development sites come forward the prevailing heights along the High Street of 3-4 storeys should be maintained. Such developments can propose additional height sitting behind the high street buildings, but these taller elements should be limited to a modest increase above the high street frontage, subject to achieving appropriate separation distances. The High Street buildings should be maintained as the primary building frontage. Some additional height (one storey) on the High Street could come in the form of mansards or appropriate roof forms, provided they are sufficiently set back from visible frontages.

B6.36 Larger development sites on the High Street and Staines Road provide opportunities for more comprehensive higher density development. Such sites could accommodate tall building clusters. Development on such sites should ensure that, where possible, there is a congruous transition between proposed higher density development and lower rise neighbours in adjacent streets.

B6.37 Larger scale development can be proposed in what are currently service yards in plots behind the high street, provided these developments can provide high quality accommodation.

B6.38 Historic buildings along the High Street should be retained, and new development should compliment these.

B6.39 Development proposals in the High Street area will need to be mindful of creating a 'canyon' effect with large development being overbearing on smaller routes. Heights should not be focused around these small routes and set-backs should be used to help avoid this effect. Buildings should be of a human scale along these smaller routes and should incorporate natural surveillance.

WHAT IS CENTRAL HOUNSLOW'S CHARACTER?

Town Centre Masterplan themes: Character and Townscape and Development

The design codes in this section seek to deliver these key masterplan principals in bold.

Heritage

Valuing the existing 'everyday' heritage...

B6.40 There are some existing architectural landmarks and notable buildings along the High Street, including Holy Trinity Church, Neal's Corner and Hounslow House. Opportunities should be taken to enhance the setting of these local landmarks with public realm improvements. Particularly around the church, which is a popular place for people to gather.

B6.41 The redevelopment of the Bus Garage site will change the character of this part of the town centre. The existing bus garage has good examples of early 20th century architecture, particularly the front building with a handsome curved frontage. This building should act as an anchor to any new development, informing the design of the new and helping to knit together old and new. As the climate emergency becomes more acute, opportunities to retain and reuse existing high quality buildings should be taken. Strong justification will need to be provided for demolition of existing buildings and structures and a comparison of embodied carbon for demolition vs. rebuild should be provided. Integrating this development with the surrounding lower scale buildings will be a key element of this site.



Fig B6.17 (Left) South Molton Street, example of new development integrating with historic shop frontages with innovative materials. DSDHA. (Right) Wellington House, Wimbledon. Example of a building next to a bus garage. MATT Architects

Town Centre Masterplan themes: **Movement and Public Realm.**

The design codes in this section seek to deliver these key masterplan principals in bold.

Public Realm

A simple, inclusive stage for activity...

B6.42 To enhance the accessibility and appearance of the High Street some street furniture could be consolidated/ removed. There are several small concrete stools along the high street, which are troublesome for some users who have to navigate these barriers in wheelchairs or who are partially sighted; these should be removed. Consolidating some of the street furniture would also reduce conflicts between pedestrians, cyclists and scooter users.

B6.43 Many of the shops along the High Street lack step free access. It is a key priority that wheelchair accessible ramps are installed on all new shops and retrofitted to existing shops to enhance the accessibility of the High Street for all users. All ramps need to be on private land and not obstruct public highway.

New and existing destinations...

B6.44 There is also an opportunity to bring back an open market, which would add to the vibrancy and variety of the high street. This could be an occasional market, similar to those in Chiswick.

More pleasant routes...

B6.45 To create pleasant and legible routes, signage from the train and tube stations to the town centre is key. Existing signage includes Legible London panels. Improvements to the way finding experience could be made such as directional pavement plaques/paving to signal towards the high street and train stations.

Yards and alleyways...

B6.46 Staines Road provides a potential opportunity to improve the public realm and provide new development opportunities. The Council should seek to commission a masterplan for this road to maximise development opportunities and improve active travel connections to the rest of the town centre.

B6.47 The public realm to the east and west of the High street is poor, characterised by narrow, uneven pavements and cluttered frontages. Opportunities should be taken to set back development from the red line to widen the pedestrian footpath.

B6.48 There are a number of service routes in the proximity of the high street, which require improvement, such as Matisse Road, which is a key route from Hounslow East Station to the High Street,. This could take the form of development with shops/ improved servicing below them. Any development should incorporate an active frontage and seek to improve the experience for pedestrians, whilst considering servicing requirements.

A shift from grey to green...

B6.49 Development in the town centre should explore opportunities to incorporate green roofs and walls, planting and street trees.



Fig B6.19 Example of town centre signage: Stainless steel plaques embedded into the pavement to indicate the shortest distance between the two train stations in Bradford.



Fig B6.18 New Inn Yard, Shoreditch - good example of public realm improvements to alleyways and yards

B6.50 Introduce green active travel connections between the town centre and Inwood Park, through introducing street trees. Connection to the town centre could be improved through connecting Selborne Avenue to London Road.

Movement

East-west walk and cycle links and Hanworth-Grove Road connections....

B6.51 Hanworth Road, Grove Road, London Road, Staines Road & Bath Road are a key focus for route improvements within Hounslow Town Centre. To improve active and sustainable travel, and reduce severance in these locations, and on other roads within the town centre, interventions should:

- Integrate segregated cycle lanes;
- Rationalise and rebalance space allocation on the kerbside;
- Widen pavements, and where new development comes forward set back building lines to accommodate wider pavements where appropriate;
- Utilise high quality pavement material, such as Yorkstone, that clearly indicate crossing points with tactile paving;
- Include greening such as planting and street trees;
- Remove central reservation barriers and railings, where safe to do so;
- Include additional pedestrian crossing points;
- Signal entry to the town centre using street furniture, utilising lampposts and benches to

provide places to rest,

- Explore how the green triangle at the Hanworth/Grove Road Junction could facilitate more planting and benches to provide a high-quality waiting area at the busy bus stop,
- Improve the connections to and from bicycle parking and stations.

B6.52 Improvements should be made to the junction at High Street/School Road to facilitate active travel to and from school.

B6.53 The connection from Asda to the town centre requires improvement to encourage active travel, through greening and improvement of cycling parking at Asda and along Prince Regent Road. Accessibility for wheelchair users is particularly poor along this route due to the narrow width of the pavement; this should be widened to improve access for all pavement users.

B6.54 The Council supports step-free access at all stations.



Fig B6.20 Frideswide Square, Oxford (the Urbanists) Before and after. The improvements included removing the multiple lanes of traffic and introducing low curbs, high quality paving, crossing points, a wide central reservation and greenery as traffic calming measures. It signals a welcoming entry to the city's busy station.

South Hounslow / West Isleworth



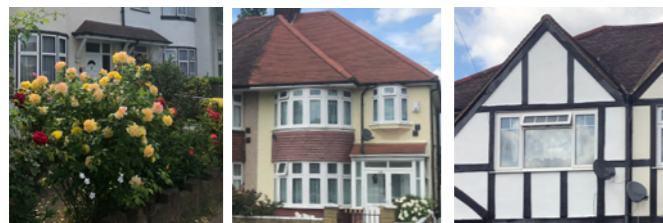
What's special about South Hounslow / West Isleworth?

B6.55 This neighbourhood is predominantly residential, bounded to the north by the railway line serving Hounslow Station. The majority of residents describe themselves as White British, although there are significant Indian and Polish communities living in the area. 45.1% of residents are non-white (2011 Census). The area has the highest level of volunteering in the Borough (Community Life Survey, 2016/17) with 70% of survey participants in the area volunteering in the last 12 months. The median age is 37 for Hounslow South ward, compared to the younger borough average of 34 (GLA, 2015).

B6.56 There are a mix of homes, but are mainly low density inter-war properties with generous front and back gardens. Pockets of Victorian and Edwardian terraced housing can be found along and off Whitton Road served by two local shopping parades, acting as local centres for the neighbourhood. Industrial activities including an MOT centre are also located along Whitton Road.

B6.57 Another cluster of activity is located at the junction of Central Avenue and Heath Road. The shopping parade fronting Lyncroft Gardens includes a post office, pharmacy, newsagents and other food stores. Chatsworth Primary School is just around the corner. The area is served by the H20 bus linking the neighbourhood with Twickenham and Hounslow Town Centre.

Selection of locally distinctive architectural details and materials which may inform new developments



Front gardens contributing to streetscape | Generous bay windows and reveals | Facade detailing

B6.58 The area benefits from some other small open spaces including Gainsborough Gardens and Murray Park (just outside of the borough boundary). Hounslow Road and Gainsborough allotments offer food growing space for residents and a peaceful haven away from the surrounding urban streets. Local people also enjoy walking a little further to the River Crane and Duke of Northumberland's River for quieter walks, and Hounslow Heath has been a new and welcomed discovery for some locals exploring their area during lockdown.

B6.59 South Hounslow / West Isleworth has a strong relationship south with Whitton, Twickenham and also east to Isleworth. Locals travel south to do bigger shops or to socialise. Prince Albert pub on Whitton Road is popular place for sports watchers.

Prompts for character-led development

- The existing building height range in this neighbourhood is 2-3 storeys approximately. Interventions could successfully conserve the modest scale of this neighbourhood with a height range of between 2-3 storeys.
- The area benefits from being in close proximity to Hounslow Station, though the railway line acts as a barrier to get into Hounslow Town Centre, creating a poor pedestrian experience.
- The existing local parades should be supported and enhanced and a new local centre around Hounslow station created by intensifying and adding new uses.
- Some front gardens in the area contribute to streetscape, however, there is an opportunity for greening on residential streets. Replacing street trees as part of interventions is encouraged.
- Positively encourage retention of landscaped front gardens including existing brick wall or hedge boundary treatments, where these form part of the prevailing character of the streetscape. Where hardstanding or driveways



WHAT IS CENTRAL HOUNSLOW'S CHARACTER?

“Our neighbours are special! The range of housing is interesting when going on Covid-era residential walks”

- Local resident

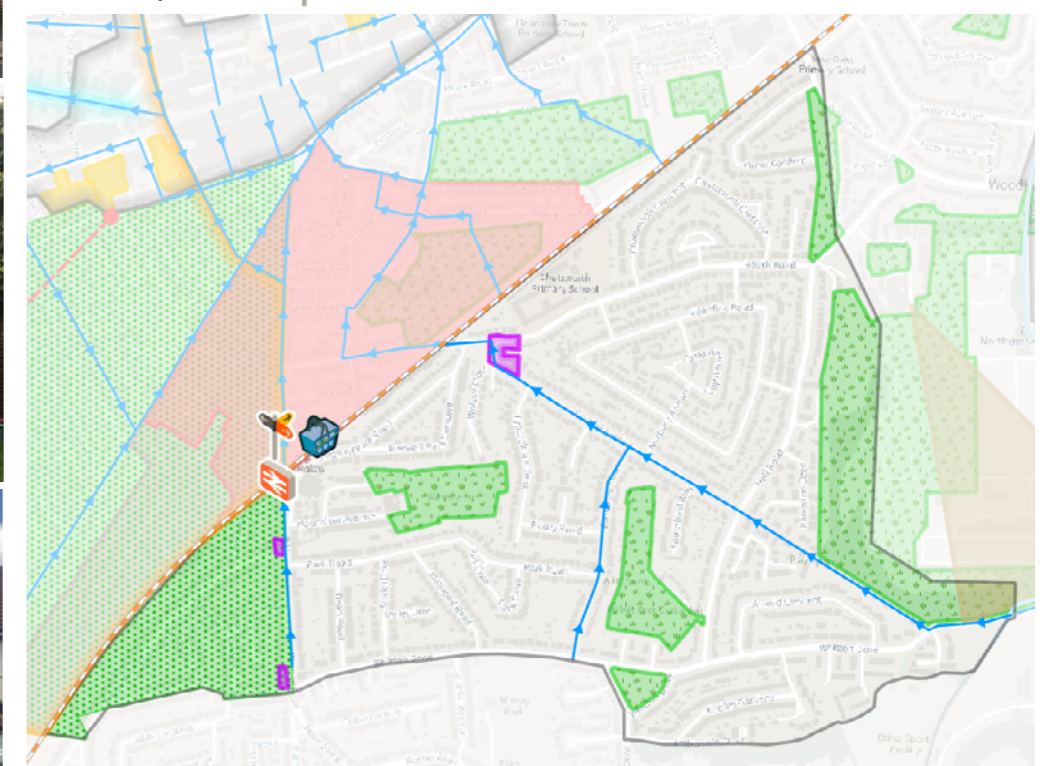
are proposed, permeable surfacing is promoted and retention of some soft landscaping within the wider front garden is encouraged.

- All development sites must also make a contribution towards greening in the public realm on key streets to address the urban greening priority area identified in the Green Infrastructure Strategy. This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.



Neighbourhood typologies

- 68% Suburban
- 12% Green space
- 12% Urban terrace
- 8% Cul-de-sac
- 1% Parades



Plan of neighbourhood

