

May 2024

HOUNSLOW CHARACTER, SUSTAINABILITY AND DESIGN CODES SPD

PART B10 CHISWICK



London Borough
of Hounslow

Allies and Morrison
Urban Practitioners

PLACES

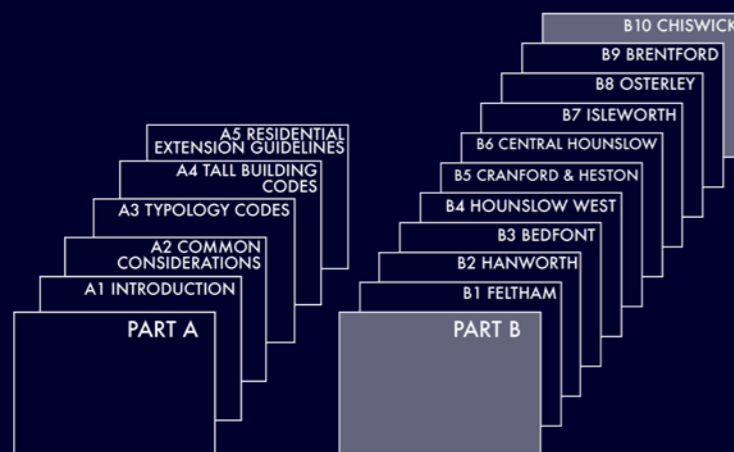
Document structure

This Character, Sustainability and Design Codes SPD is comprised of the following documents;

Part A: Introduction & Design Codes

Part B: Places

These are individual files that must be opened separately to view. Use this diagram to help navigate the structure of the Character, Sustainability and Design Codes SPD.



FELTHAM

HANWORTH

BEDFONT

HOUNSLOW WEST

CRANFORD AND HESTON

CENTRAL HOUNSLOW

ISLEWORTH

OSTERLEY

BRENTFORD

CHISWICK

* This document is **Chiswick**
To view other places please open the relevant documents as named above.

Introducing Hounslow's places

Neighbourhoods and Places

- B10.1** This section, Part B, explores character at a scale best understood by local people - at the neighbourhood and place level. The Borough is comprised of a series of places and neighbourhoods which each have a subtle, or sometimes distinct, character of their own. The map overleaf shows an interpretation of Hounslow's neighbourhoods. It has been developed using the previous study areas set out in the 2014 characterisation, together with local residents.
- B10.2** Identifying where neighbourhoods begin and end is inherently subjective - there will be overlaps and blurred edges between boundaries, even disagreements, as places in the Borough mean different things to different people. Influencing factors like physical infrastructure (roads, railway lines), natural assets (waterways, open spaces), local parades and centres, ward boundaries, can all help to inform the drawing of neighbourhoods that make sense. The degree of consistency or diversity of different aspects, like a particular typology or ethnic group, is also key to how neighbourhoods are perceived.
- B10.3** The purpose of dividing the Borough into these smaller areas is to provide detail on the special qualities and specific challenges for local people in each neighbourhood. This will help to ensure future growth is fairer, greener and more inclusive, and will help shape policies to ensure that the existing identity and character of neighbourhoods is a key consideration. The 37 neighbourhoods have been grouped into ten larger places. The neighbourhoods have been grouped by similarities in character and sense of place informed by: discussions at community

workshops to understand Hounslow's diverse communities; the mapping of physical assets such as topography, landscape and urban morphology; the historic evolution of each area; analysis of land use and housing typologies and their future growth direction.

Structure of Part B - Places

B10.4 Part B describes the ten places in the Borough. This chapter has four sections:

- 1. Place-level analysis** - this section sets out the baseline analysis for the place and the vision for the place, which has been shaped by the contribution of local people through an online survey and workshops, and have been developed by considering:
 - What are the area's key assets that people love and that should be celebrated and conserved?
 - What are the current social, economic, health and environmental issues of a particular area that we want to address?
 - What opportunities for growth exist in this area and where? This includes thinking about planned investment in infrastructure, Council aspirations for growth and wider trends e.g. changing role of high streets, shift to active travel informed by existing or proposed transport hubs/links/interchanges.
- 2. Neighbourhoods** - this is design guidance that sets out the special qualities for each neighbourhood in turn, and what that means for development in the area. Each neighbourhood page includes photos, key materials and features, the distribution of typologies and where possible, quotes from

local people to help convey what makes each neighbourhood unique.

- 3. Sites** - this section draws on the design codes set out in Part A and for the Neighbourhoods to provide design codes at a site-specific scale on key sites identified by the Council.
- 4. Town Centre** - this section sets out specific design guidance for the town centre, drawing on the town centre visions and masterplans, to deliver design improvements in the town centre.

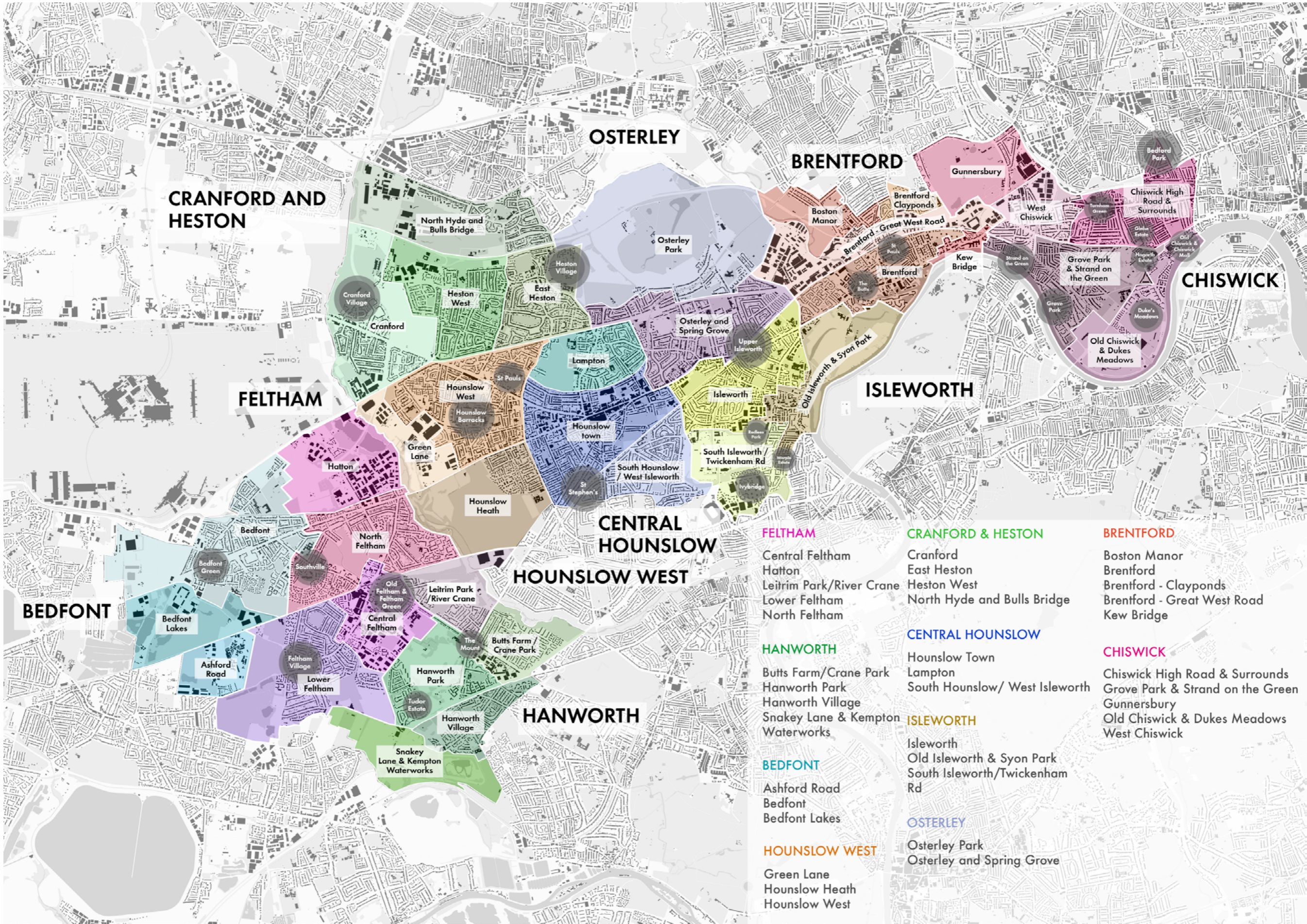
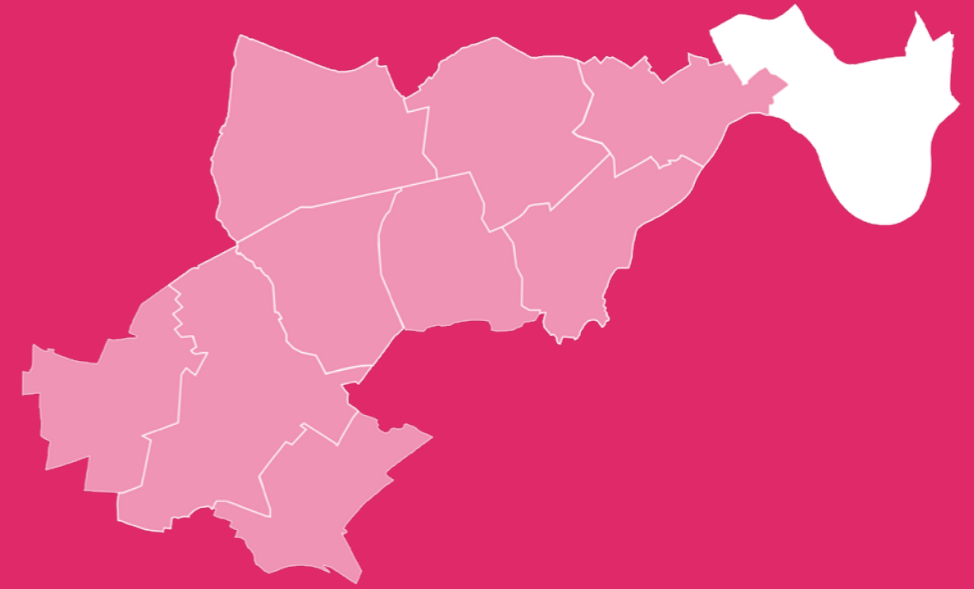


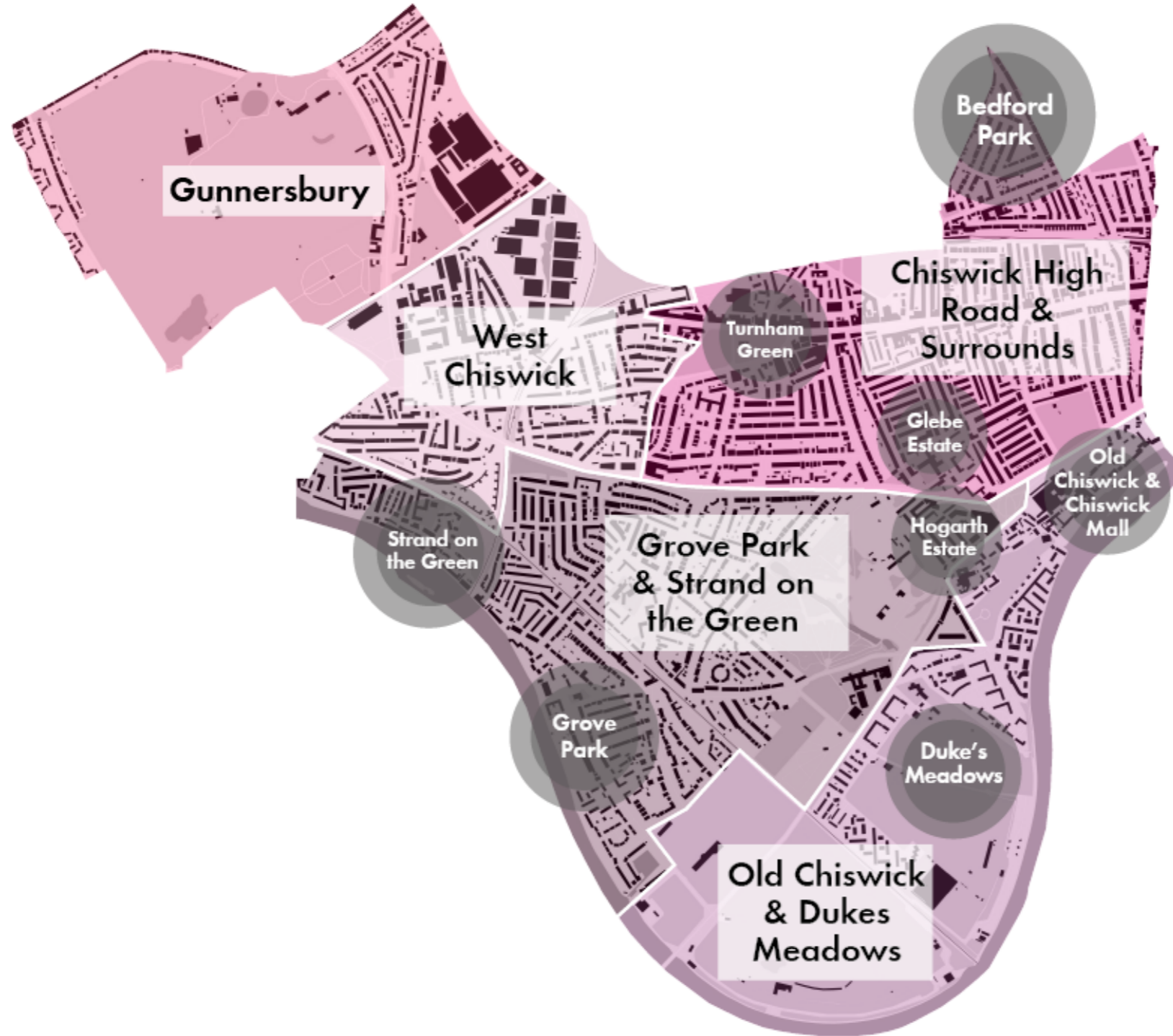
Fig B10.1 Hounslow's neighbourhoods

CHISWICK



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Towards a greener, healthier, cleaner, safer, thriving and liveable... Chiswick

B10.5 Chiswick is a historic, attractive and vibrant part of the Borough. It's well connected to Central London, has a wealth of heritage assets and high quality green spaces, such as Dukes Meadow, Turnham Green, Chiswick Back Common and others, and sits proudly on the river Thames. It's a desirable place to live, work and visit. There are many wonderful areas in Chiswick - from Strand on the Green to Gunnersbury, which all make an important contribution to Chiswick as a whole. Any future change should help to strengthen, support and bring together local communities and protect the quality of the Thames riverside character.

B10.6 Chiswick suffers from poor air quality issues along major routes, and has a number of Air Quality Focus Areas. The Chiswick Gunnersbury ward has the 2nd worst outdoor environment in the borough in terms of air quality pollution. It is particularly crucial for interventions to help improve air quality along major routes.

B10.7 Public Transport Accessibility Levels are high in the north of Chiswick around Chiswick High

Road and West Chiswick. PTAL levels decline towards the south of the area with the poorest levels towards the riverside. There is a need to reduce the severing impact of the A4 and better links to Dukes Meadow and the Thames Path. Although PTAL levels are high in the north of the area it is recognised that there are some challenges regarding transport provision, such as lack of step free access at stations and Turnham Green Station not being serviced by Piccadilly line trains for the majority of the day. Improving connectivity through walking and cycling is crucial to support connectivity between Chiswick Town Centre, surrounding residential areas and Chiswick Station as it evolves in order to continue to provide for user needs and preferences, and remain viable.

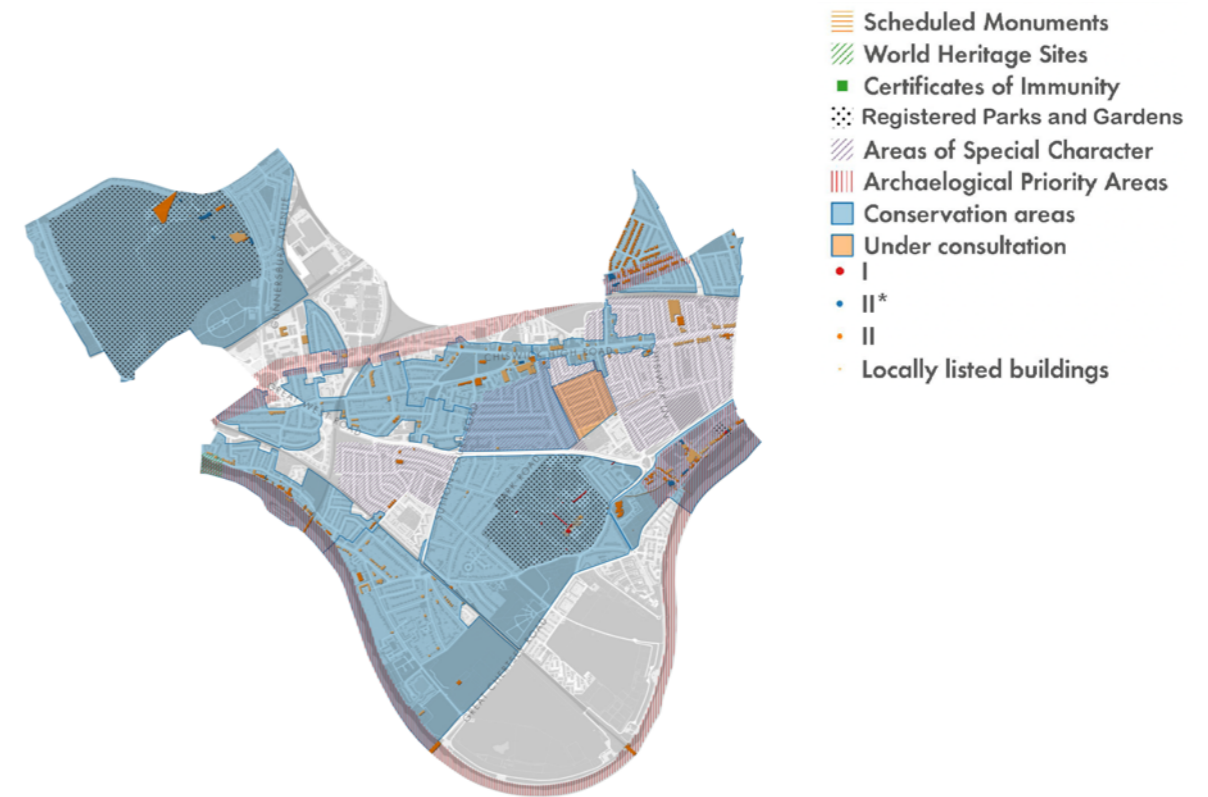
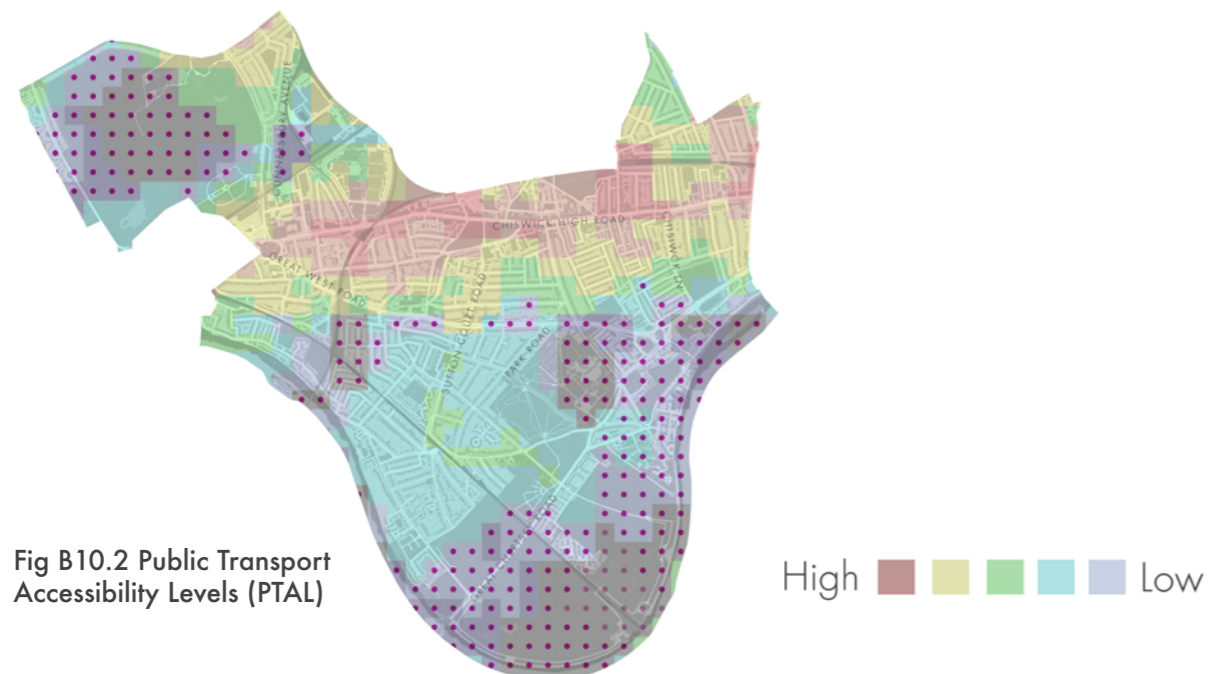


Fig B10.3 Heritage

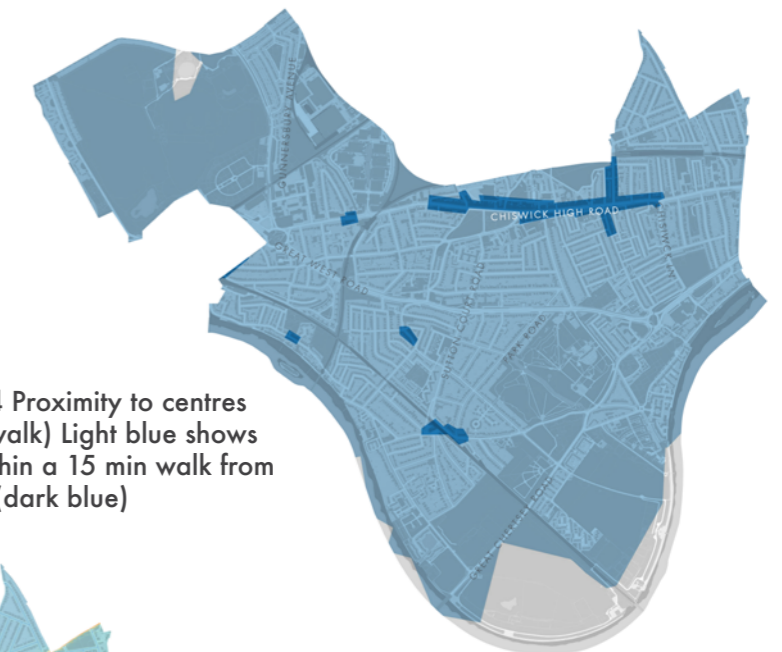


Fig B10.4 Proximity to centres (15 min walk) Light blue shows areas within a 15 min walk from a centre (dark blue)

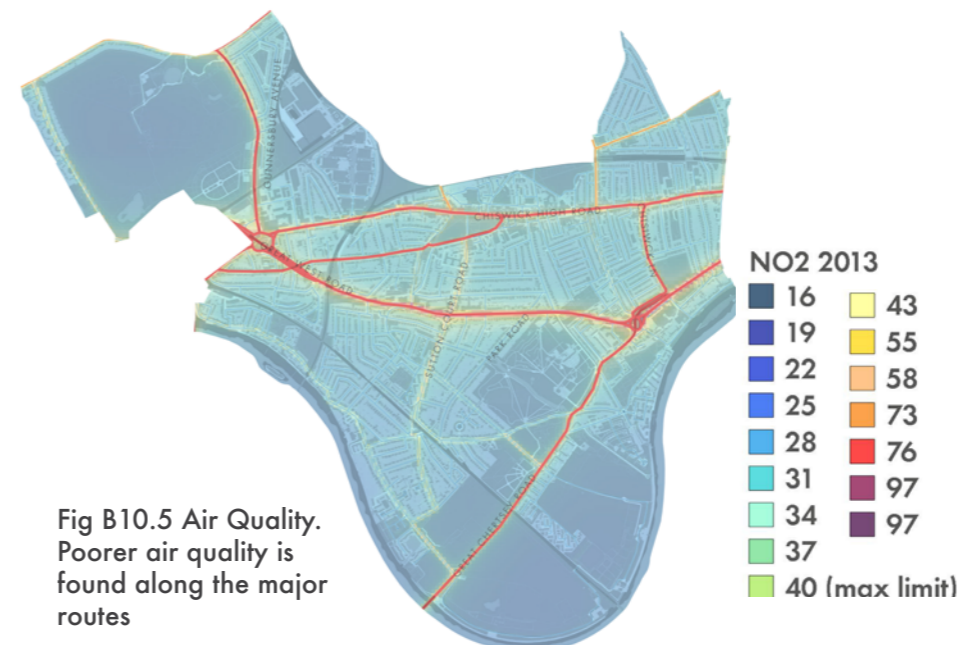


Fig B10.5 Air Quality. Poorer air quality is found along the major routes

B10.8 Chiswick has the highest number of community and civic assets in the Borough, it has an active civil society and attracts a significant amount of third sector funding.

B10.9 Chiswick is the least deprived part of the borough, meaning that residents who live in this area tend to be in employment, have access to adequate community facilities. The area is safe and educational outcomes are good. There are, however, pockets of deprivation around Duke's Meadows in the South. Household living arrangements are an important measure for quality of living: levels of overcrowding are low in the area overall, however levels of overcrowding in the Chiswick Gunnersbury ward are average. All three of the Chiswick wards have high levels of single person households, the majority of whom are occupied by white British people.

B10.10 Its residents are predominantly white British, with notable Western European and Irish communities (2011 Census). They are also on average older too - the median age across Chiswick's wards ranges from 37-39, compared to the Borough average of 34 (GLA, 2015).

B10.11 The level of digital inequality across Chiswick is low, and there are no high-risk postcodes for e-withdrawn or passive and uncommitted users. There is a good level of connectivity across the area.

B10.12 Chiswick is blessed with extensive green infrastructure assets. Access to green space is good across the majority of the area, however those in the south west of the area have to travel further to green space.



Fig B10.6 Chiswick High Road is within one of the priority urban greening areas in Chiswick and is also identified as an urban boulevard opportunity.

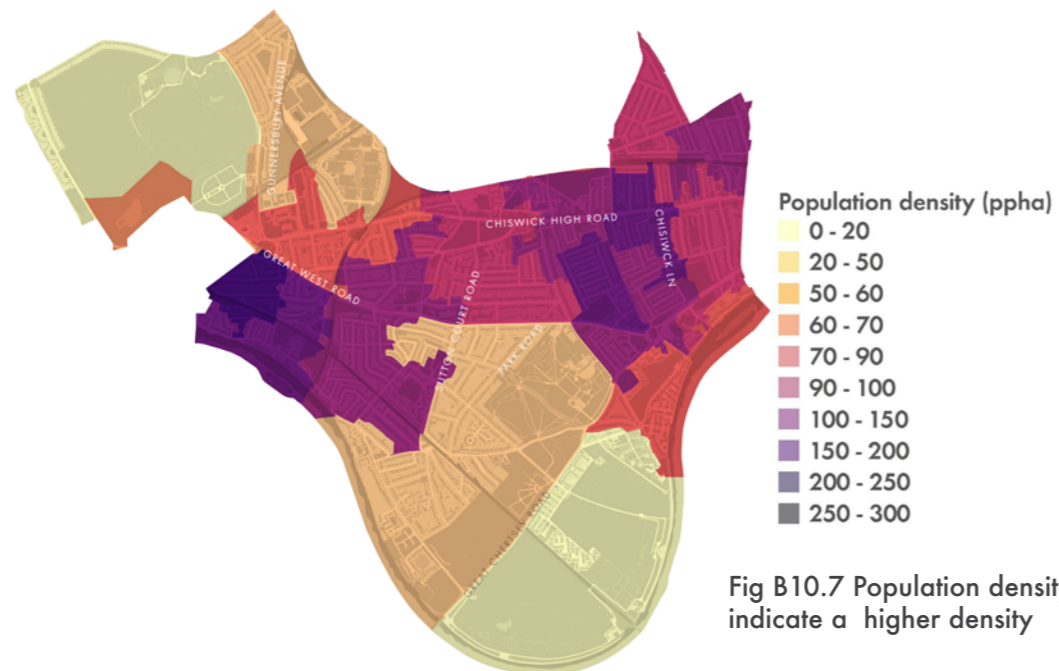


Fig B10.7 Population density darker shades indicate a higher density

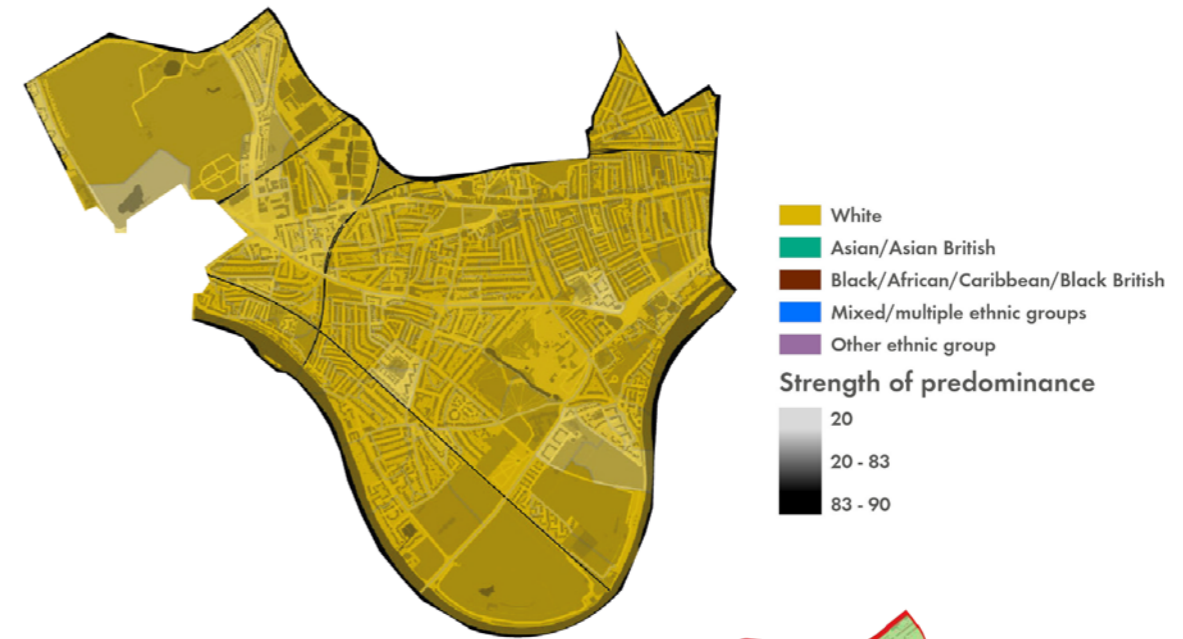


Fig B10.8 Ethnic Groups

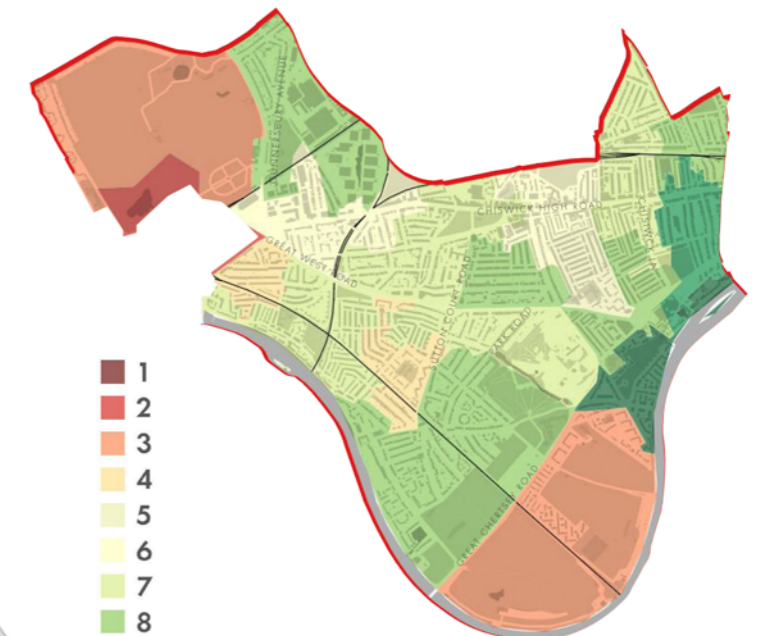


Fig B10.9 Index of Multiple Deprivation Decile (where 1 is most deprived 10%). Red shows more relatively deprived areas, dark green shows least

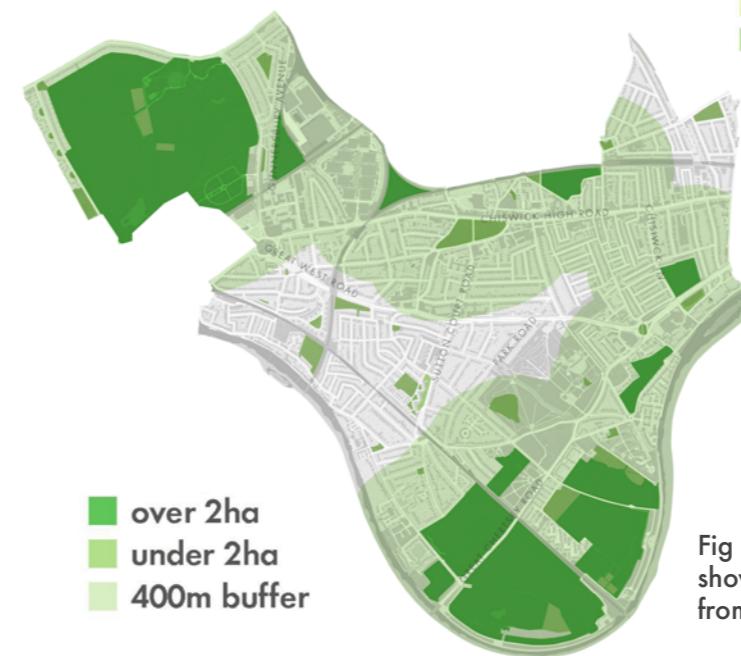


Fig B10.10 Access to open space. Dark greens show open space, light green shows 400m radius from space

B10.13 As Figure B10.11 illustrates, the majority of street blocks in the area contribute well to the existing amount of greening. Most residential blocks generally achieve a score of at least 0.6, with many blocks around Dukes Avenue and Park Road well exceeding this level. In contrast it is the non-residential parts of the area that score least well. Industrial and office locations in the west of the area have the least urban greenery, but low scores are also found on some sites along Chiswick High Road. Both areas have been identified as urban greening priority areas in the Green Infrastructure Strategy. Interventions in these locations will need to be sensitive to enhance their resilience.

B10.14 The prevailing height of existing buildings in residential areas is two to three storeys. Along the Chiswick High Road most buildings are between two and four storeys. In the non-residential areas within Gunnersbury and West Chiswick some taller elements up to 11 storeys are found. A Floor Area Ratio (FAR) of 1.5 is achieved by most existing buildings in

the area; new development must respect this FAR and existing prevailing heights.

B10.15 Much of this area is protected by Conservation Area status and future change will need to be sensitively accommodated. Of particular importance in this area is avoiding the over intensification of plots. Any extensions must be proportional to the existing building and must avoid the loss of important green infrastructure in front and back gardens.



Fig B10.12 The industrial areas to the west of Chiswick High Road are also identified as a priority urban greening area where additional greening is needed.

B10.16 There are fewer opportunities for significant development in this area, but that does not mean no change at all. New development in this area will range from the re-use of historic buildings for new uses through to new high quality developments that support mixed communities within the Great West Corridor opportunity area. New developments must have particular regard to the priorities in this area which include the upgrading and sustainable, sensitive retrofit of existing building stock, protecting the character of historic buildings and the setting of historic assets, the enhancement of public realm and connectivity and the balancing of enhancing sustainable travel modes with the need for access and strategic movement.

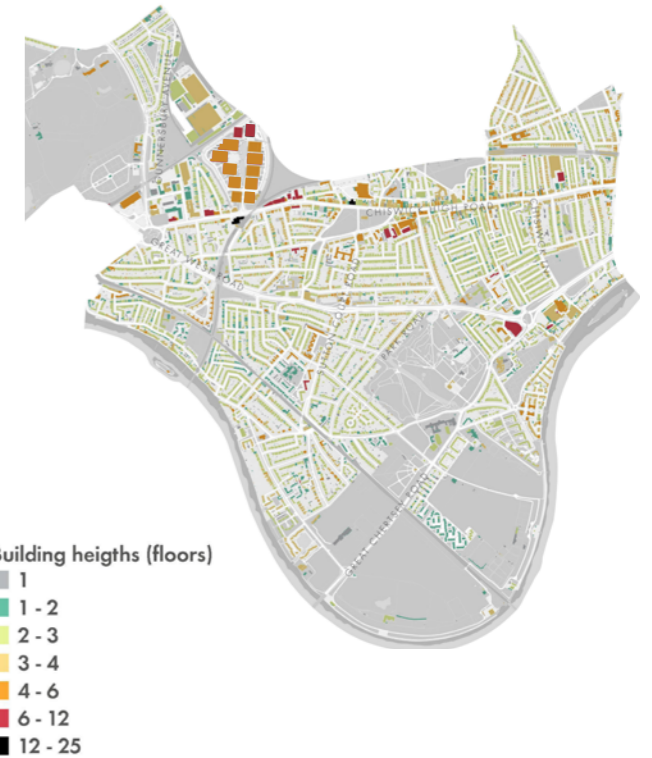


Fig B10.13 Approximate Existing Building Heights (2020)

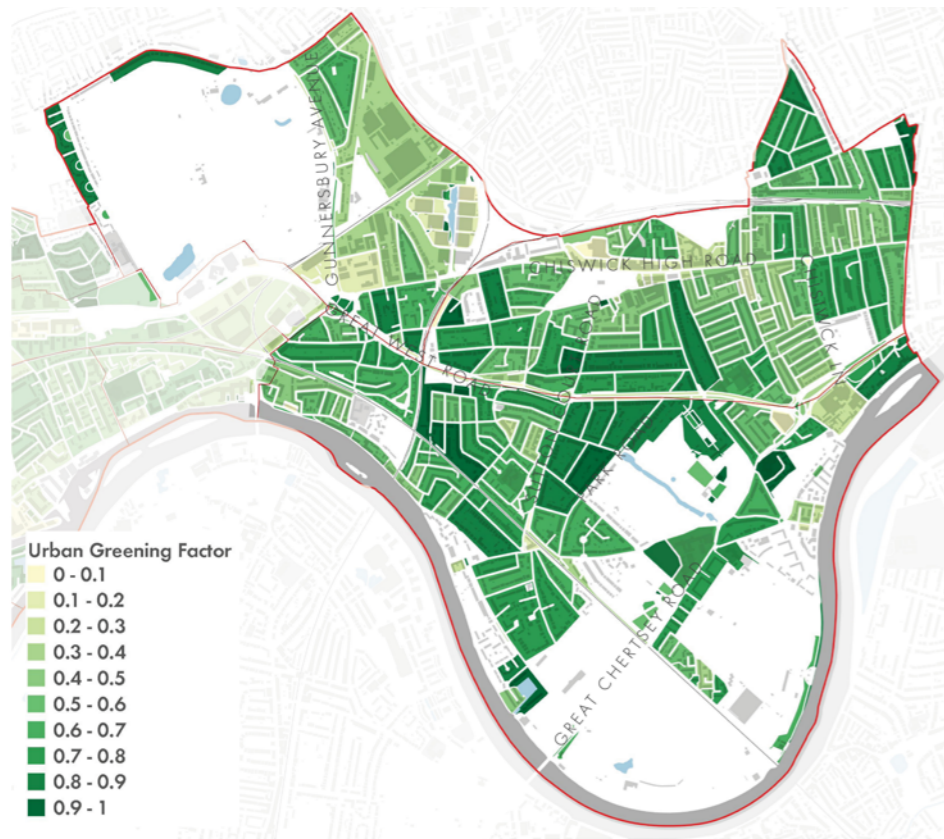


Fig B10.11 Strategic Urban Greening Factor analysis of existing street blocks - this measurement includes the contribution of street trees and planting in the public realm as well as greenery within plots. The adopted London Plan suggests a minimum score 0.3 to support climate adaptation.

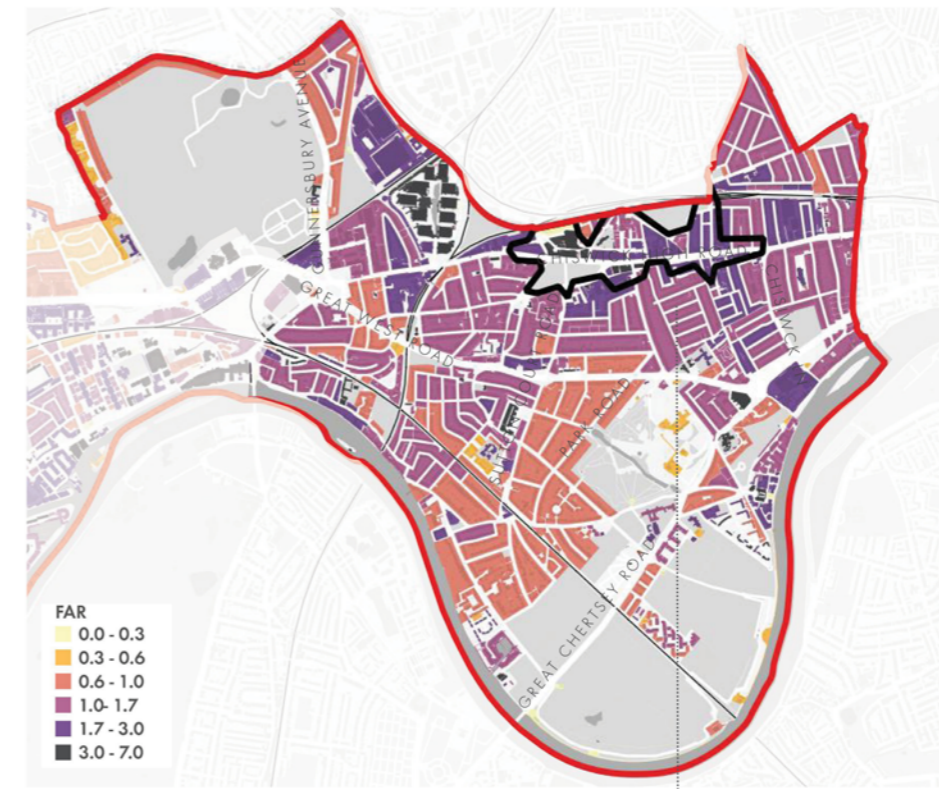


Fig B10.14 Strategic Floor Area Ratio density analysis of existing street blocks - this analysis measures FAR at the street block level rather than plot level and so gives an average estimate of FAR on plots.



Residential plots along the Reckitt Road have an existing FAR density of 1.5

B10.17 For a greener, healthier, cleaner, safer, thriving and liveable Borough, Chiswick should transition towards:

1. **Supporting Chiswick High Road as a vibrant hub for the local community.**

The centre is an important asset and needs support to meet the future needs of its local communities. Chiswick High Road will need to evolve, as it has done in the past, to support changing demands, both in terms of uses and activities as well as movement. Enhancements to active travel have already included the installation of Cycleway 9 along Chiswick High Road. Further interventions should

include continued enhancements to active and sustainable travel, improving access at local stations; maintaining the human scale of local shops; integrating workspaces into the town centre; and revitalising underused civic buildings to support a diversity of uses for families, young people and the elderly and enhance Chiswick's identity and role. This will need to be achieved in a manner that respects its low rise compact form, distinctive architectural character and the patterns of movement across and along the High Road.

2. **Enabling new connections to be established to overcome existing barriers.** The A4 and South Western railway currently bisect this area into a series of east-

west oriented tranches. Opportunities to create better pedestrian, cycle and public transport links north-south across Chiswick should be maximised to provide access to the town centre and District Line as well as linking to Chiswick Station, Dukes Meadows and the Thames Path.

3. **Helping nature take its course through the enhancement of existing green and blue corridors.** The area already supports varied habitats through its existing green and blue infrastructure, particularly at Gunnersbury Triangle which is very diverse ecologically.

Future growth should encourage continuous and generous wildlife habitats along corridors such as the River Thames, including towards the Grand Union Canal at Brentford. Measures need to be taken to reduce flood risk in the area including upgrading defences to cope with future sea level rises. The existing high levels of street trees and good quality spaces in this area need to be positively managed and maintained in the long term. Future development should introduce greenery to contribute to the enhancement of corridors, help to soften the landscape and achieve a net gain in biodiversity. Future development must also make a contribution towards greening in the public realm on key streets and in the town centre, such as increasing street trees and planting. Developments in the north of the area must contribute to the urban greening priority areas identified.

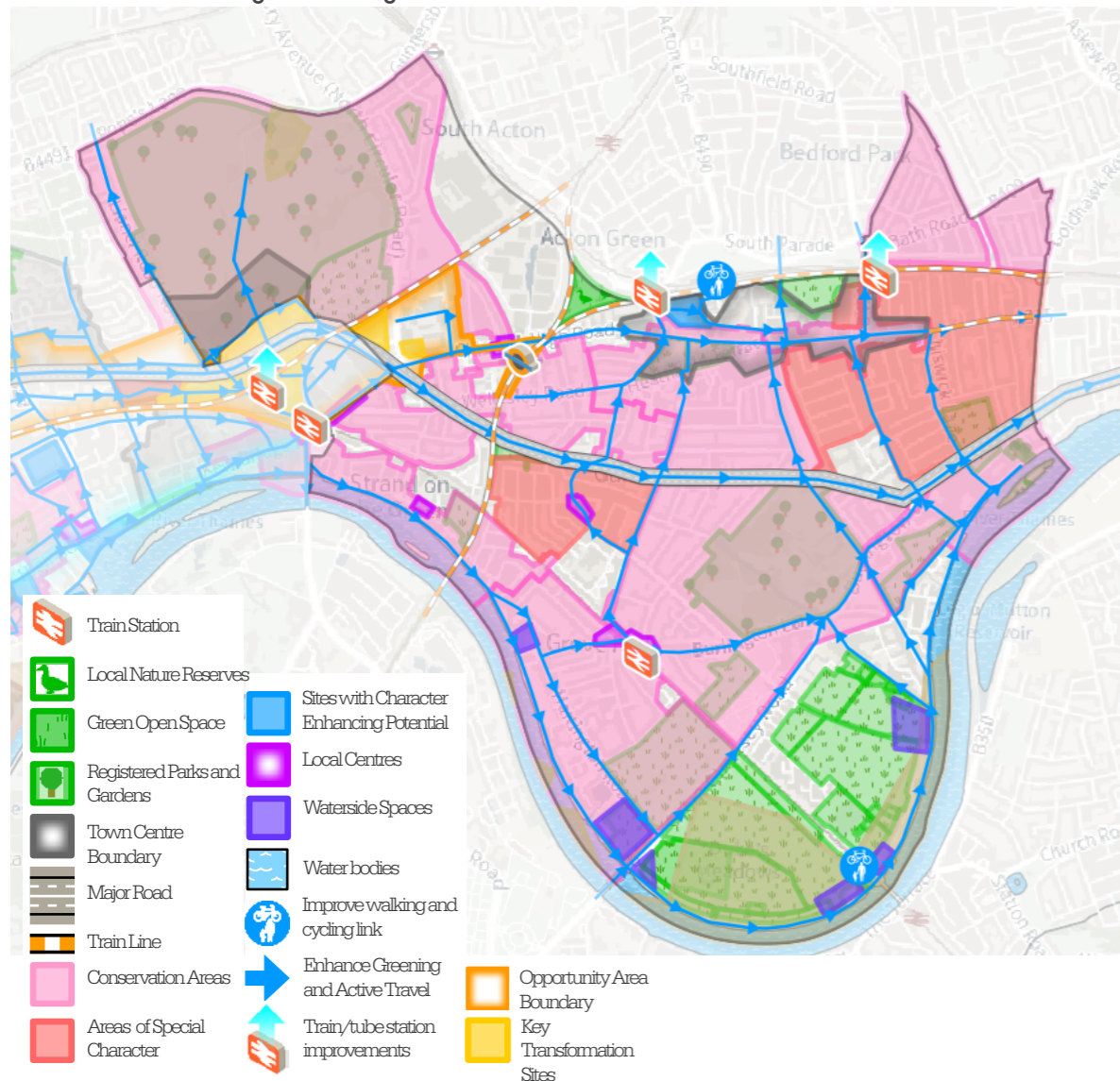
4. **Respecting and reinforcing the wealth of historic fabric and environments - the well celebrated and the more hidden.** New development must respect the extensive historic environments throughout Chiswick. This includes the cohesive character of existing neighbourhoods such as Glebe Estate, exceptional buildings such as Grade I listed Chiswick House and those along Chiswick Mall, many of which

are Grade II listed, as well as its Victorian terraces and 20th Century architecture. New developments must also be sensitive to the setting of assets outside the Borough boundary such as Royal Botanic Gardens, Kew World Heritage Site. Opportunities to integrate archaeological remains in new development to add archaeological interest and contribute to placeshaping should be explored.

5. **Contributing to the waterfront.** In this location, waterfront buildings tend to have a more low-rise, residential and intimate relationship with the River Thames, and street trees and green spaces make an important contribution to the area. This character should be maintained. Opportunities should be taken to promote recreation along the river.

6. **Increasing the sustainability of our existing buildings.** To enhance sustainability of the existing built environment within Chiswick retrofit solutions are necessary. Solutions such as solar panels, air source heat pumps and secondary glazing are encouraged, however must be situated discreetly to avoid visual intrusion. Internal wall insulation, secondary glazing and draught proofing may also provide a solution for home owners to improve environmental performance. Where windows are being replaced it is important that they should match the original windows in design and material.

Fig B10.15 Area transitioning vision diagram



Chiswick High Road & Surrounds



What's special about Chiswick High Road and Surrounds?

B10.18 This neighbourhood is centred on the historic coaching route of Chiswick High Road and is steeped in heritage, showcased through its historic greens, Victorian shopping parades and historic houses, the value of which is recognised by the Chiswick High Road conservation area. Today Chiswick High Road is a popular destination for locals and visitors alike, boasting a range of shops, homes, green spaces, places to eat, drink and be entertained. The High Road serves the surrounding residential mature tree-lined streets which are consistently high quality - from elegant Edwardian mansion blocks on the High Road, to the late 19th century Arts & Crafts-style Garden Suburb of Bedford Park, itself a conservation area and grids of Edwardian and Victorian terraced streets to the south.

B10.19 Chiswick High Road & Surrounds sits within the Chiswick Gunnersbury ward. The median age of residents is 37, higher than the Borough-wide age of 34. It has the lowest proportion of 0-9 year olds, and the highest proportion of those 90+ in the Borough. The area has the highest proportion of residents living in one-person households (2011 Census). Residents are majority white British. Other notable ethnic groups include Indians and West Europeans. 27.5% of residents identify as having no religion, the highest proportion in the borough (2011 Census).

B10.20 Residents are more likely to take part in civic engagement than anywhere else in the Borough (Community Life Survey, 2016/17). The area is well-loved by locals for its local shops and cafes like at Turnham Green Terrace, as well as its distinct historic residential areas around Bedford

Selection of locally distinctive architectural details and materials which may inform new developments



Articulated masonry **Awnings** **Spacious mature tree-lined streets**

Park and Glebe Estate. Locals like the compact and characterful nature of the town centre and the strong sense of community. Although outside of the Hounslow boundary, many residents associate parts of Ealing as being part of Chiswick.

B10.21 Community, institutional and civic uses include a library, school, health centre and Town Hall. Sainsbury's superstore is a popular place to shop. The dominant ground floor uses on the High Road, Turnham Green Terrace and Devonshire Road are retail (including pubs, bars, cafés and restaurants) with office and residential uses on upper floors. Turnham Green Terrace's shopfronts, canopies and signs play an important role in the character of the streetscape.

B10.22 The neighbourhood has two well-loved green spaces - Chiswick Back Common and Turnham Green - bringing both environmental and health benefits for locals. The listed Christ Church on Turnham Green is a significant local landmark. Close-by is Chiswick House and Gardens, the River Thames and other parks and commons which provide natural escapes from the bustling High Street, an attribute of the area particularly valued by local residents. The frequency of street trees and planting in the town centre is generally low and needs to be increased.

B10.23 The area's character is further moulded by the Chiswick High Road, Chiswick House, Bedford Park, Turnham Green, and Stamford Brook Conservation Areas.

B10.24 Chiswick High Road & Surrounds is well served by public transport, including underground stations and several bus routes linking the area with central London and other parts of the Borough, however non have step-free access.

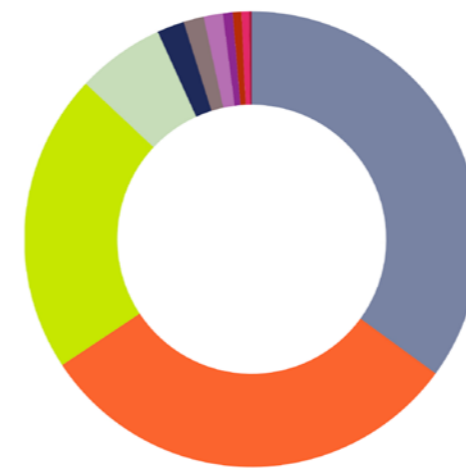
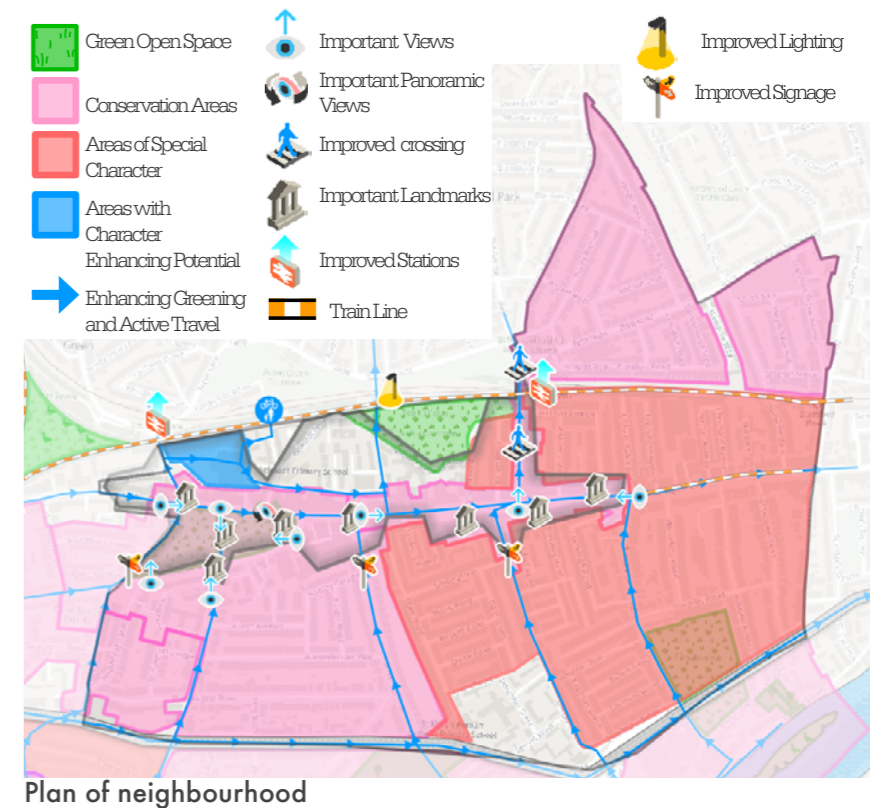
Prompts for character-led development

- New homes should be diverse in size and tenure type to attract people of all ages.
- Active and sustainable travel improvements are required, in addition to public realm enhancements throughout this area including decluttering streets, shopfront upgrades, new seating, lighting, litter bins, cycle parking.
- Any interventions must be respectful to the high quality

and historic character of this neighbourhood and its heritage assets, such as Chiswick Park Station and Christ Church, Turnham Green. Opportunities to re-use and adapt existing buildings is a priority.

- The existing building height range in this area is 2-11 storeys approximately. New buildings should be between 2-4 storeys. Any proposals for tall buildings should refer to the Local Plan.
- Investment at Chiswick Town Hall and library should create enhanced services such as affordable workspace, community space and event space, in addition to a high quality public realm linking Turnham Green, Town Hall Avenue, Heathfield Terrace and Barley Mow Passage to create a civic cluster.
- Developments must contribute to the urban greening priority area through achieving the UGF target on site also in the public realm, such as increasing street trees and planting. This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.

- Positively encourage retention of landscaped front gardens including existing brick wall or hedge boundary treatments, where these form part of the prevailing character of the streetscape.



Chiswick Town Centre Design Codes

Design & Heritage

Building Form

B10.25 Many of the buildings within the town centre have a richness of detailing and there is a variety of styles of buildings. The variety of building designs add interest to the character of the town centre.

B10.26 High quality modern design is welcomed within the town centre, however this should take cues from the surroundings. Proportions, scale, detailing and fenestration can be used to inform new designs which will sit comfortably in a historic setting. The rich heritage context of the high street, with its variety of building styles, makes it possible for distinctive, high quality, new architecture to sit comfortably on the high street, provided a robust study of the context has been undertaken.

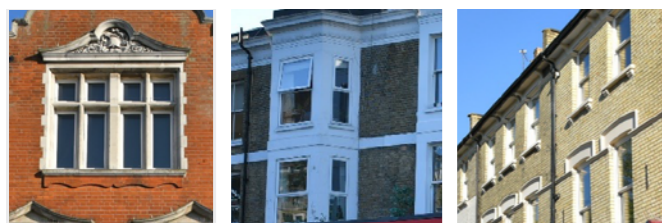
B10.27 Because of the sensitive heritage context, applicants are encouraged to appoint architects with experience of delivering high quality in conservation areas.

B10.28 End of parade sites should provide frontage and/or fenestration to the flank wall, to provide natural surveillance and security.

B10.29 Development should respect the prevailing building line, concentrating non-residential active frontage at ground floors and enhance the public realm.

B10.30 Residential entrances at ground floor should be welcoming and clearly visible if they are interspersed with non-residential uses.

Fig B10.16 Selection of locally distinctive architectural details and materials which may inform new developments



Red brick with stone dressings London Stock Brick with render details Yellow Brick

B10.31 Access to non-residential uses at ground floor should be from the primary route, with separate entrances to each retail and/or commercial unit as required.

B10.32 Multiple entrances (front doors and lobbies) may be required on larger sites and denser developments, distinguishing these between residential and non-residential uses at ground or upper floors.

B10.33 Proposals including mews dwellings with rear residential access should take care to make this safe with public realm and lighting, given the route may typically be used for servicing along the parade.



Fig B10.17 Examples of town centre developments which reflect historic contexts : 168 Upper Street (top) by Amin Taha/Groupwork © Tim Soar, contemporary reinterpretation of an end pavilion that is respectful of the historic character and 294 Old Brompton Road (bottom) by Four Four Six Six © - traditional brick and stone and timber sash windows reflect the historical setting

B10.34 Any advertising or signage should be integrated into the design early on rather than an afterthought.

B10.35 Applicants should consider the integration of community facilities as part of their proposals. Applicants should demonstrate how proposals can be easily adapted between uses, particularly at ground floor to build in resilience.

B10.36 Opportunities for green roofs and street tree planting are particularly relevant to this site type.

Heights

B10.37 Fronts and backs should be clearly defined with active frontages along primary routes with non-residential uses at ground floor.

B10.38 Proposals should respond to the scale and roof form of neighbouring and opposite buildings.

B10.39 Proposals should seek to establish a consistent building height with neighbouring buildings

as well as well as anticipating the scale of future developments on neighbouring sites.

B10.40 Mansards will not be considered, except when they are already the prevalent character of part of the High Road.

B10.41 There is some potential to intensify at the rear of buildings, improving service areas and permeability while respecting the character and massing of the building fronting the High Road.

Views and Landmarks

B10.42 There are a number of important views and landmarks within the town centre, as identified on the map. Development will need to demonstrate that local views have been considered and need to be mindful of maintaining the status, setting and significance of local landmarks. Conservation Area Appraisals identify some views which can be a starting point, however each site will require views testing unique to that site.

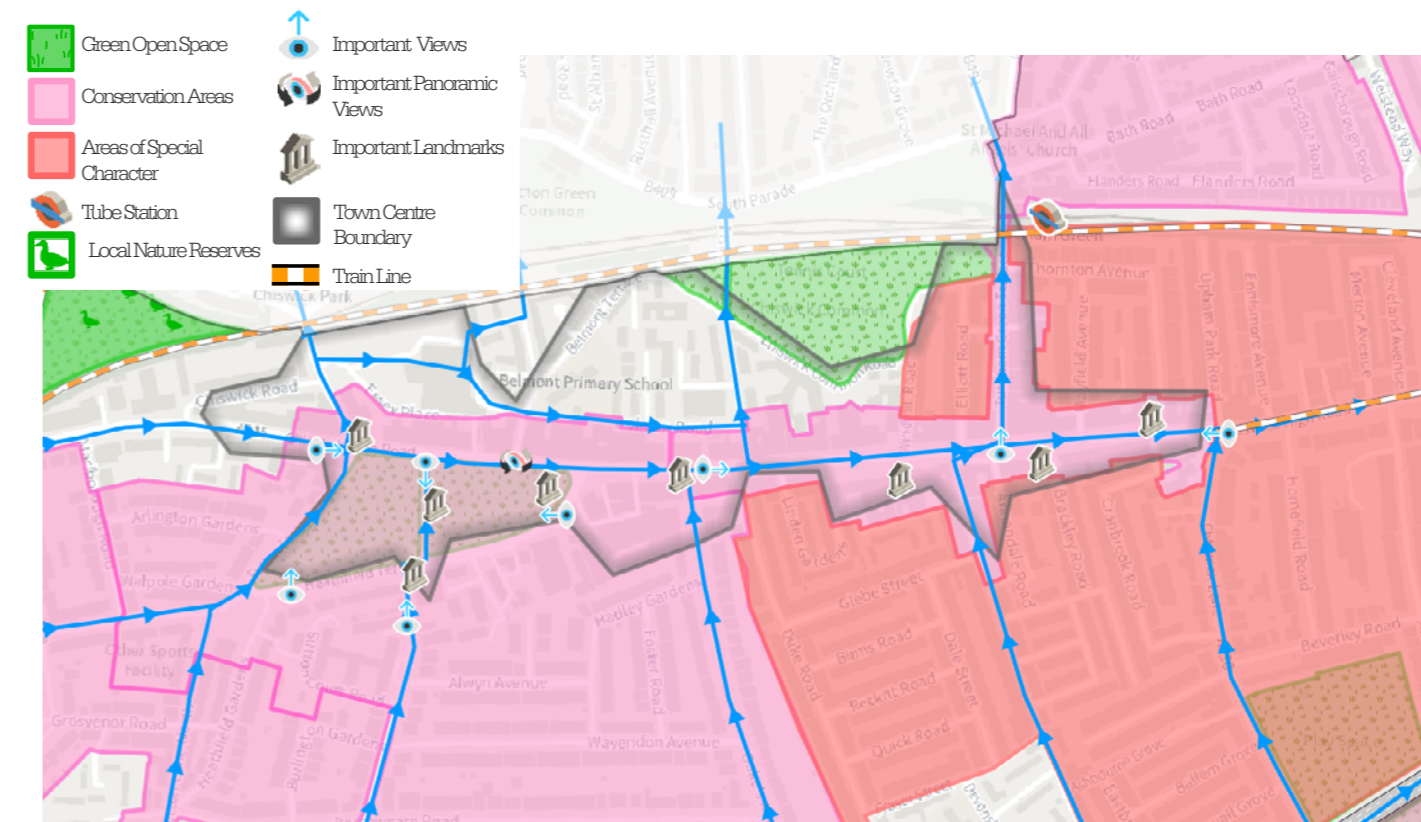


Fig B10.18 Plan showing heritage assets and heritage design codes within Chiswick Town Centre

Public Realm

B10.43 Street trees are a key part of the character of the town centre and add significantly to leafy character. Development that would require the removal of street trees will be resisted.

B10.44 Opportunities to improve planting are welcomed. The Turnham Green Piazza project is a great example of how improvements to planting and street furniture can significantly improve the public realm and biodiversity. Opportunities to do similar projects in other locations in the town centre should be taken.

B10.45 Within the town centre there are various pockets of poor quality public realm, such as the small traffic island in front of the post office, represent opportunities to improve public realm.

B10.46 Resources permitted, the council could undertake new planting and minor improvement, such lighting and tidying of spaces. Or alternatively, areas such as this could be improved could be through a program run by the council for locals to adopt and improve these spaces, or by encouraging developers or statutory undertakers to develop

similar schemes. This could also involve re-wilding these spaces or communal growing. Local businesses could also potentially sponsor the improvement of spaces such as these.

B10.47 Opportunities to improve the dead space in parking areas along Old Market Place should be explored. This could be in the form of parklets or improved planting, including along the boundary with the High Road. Improved urban lighting – creative solutions in pavements or other ideas to make the space look more inviting. Encourage a redesign of the brick boundary wall to this section so the wall becomes part of the landscape, incorporates seating and improves permeability.

B10.48 Public art can significantly enrich the public realm, as demonstrated by the Chiswick timeline and the Enwrought Light sculpture and associated culture trail. Further opportunities to install more public art or enhance the existing public art should be explored.

B10.49 Potential to install a heritage trail through Chiswick linking sites outside the town centre to the town centre.

B10.50 Existing access and movement arrangements around and into the site need to be factored into proposals.

B10.51 Commercial servicing arrangements must be integrated into the design at an early stage.

B10.52 Public realm improvements should include interventions to promote active and sustainable travel.

Shopfronts

B10.53 Alterations to shop fronts to improve step free access can be done while maintaining the character of the shop front.

B10.54 Pavement dining spaces should be discreet and demarcated by temporary fixtures.

B10.55 Signage on awnings should be discreet and limited to the banner at the bottom of the awning.

B10.56 Permanent structures such as pergolas are not appropriate for the character of the High Road, temporary measures such as umbrellas should be used instead if shade is required.

B10.57 Internally illuminated signs are not appropriate for the town centre because of its conservation area status.

Dolman Road

B10.58 Dolman Road is a unique part of the town centre, in that it is partially a service area for high road shops, but also has the health centre, a school and residential building along it. There is an opportunity to improve the High Road servicing side of this street which would result in an improved public realm, better links to the high street and Essex Place:

- Improvement of this area will need to take account of the continues servicing needs of the High Road, and recognize that maintaining the vibrancy of the High Road is a priority and development cannot prejudice the viability of the businesses along the High Road.
- Development along Dolman Road should have active frontages to help enliven this

road. Principle access to any residential developments should be from the High Road, unless there are very clear reasons why this cannot be achieved. There can be secondary accesses on Dolman Road. Development should look to improve wayfinding and pedestrian routes between Dolman Road and the High Road development should be of a scale similar to the High Road building, mainly three to four stories dependant on views testing and of a fine grain. Any development should be of a high quality modern design, which takes cues from the surrounding context, without being pastiche.

- Highway widening/improvements would likely need to be part of any development proposals towards the western end of the street. Where development schemes come forward, building lines should be set back and pavements restored. Greening, including street trees and planting, should also be introduced.
- Design of this road should differentiate itself from the High Road and have its own distinct character, this could be through reinforcing the industrial character of this road in the design of new buildings.
- Development along Dolman Road should also include public realm improvement and urban greening.



Fig B10.19 Turnham Green Piazza project showing improvements to the public realm and biodiversity



Fig B10.20 Parklet in Hammersmith. Example of an improvement to dead space.

Movement

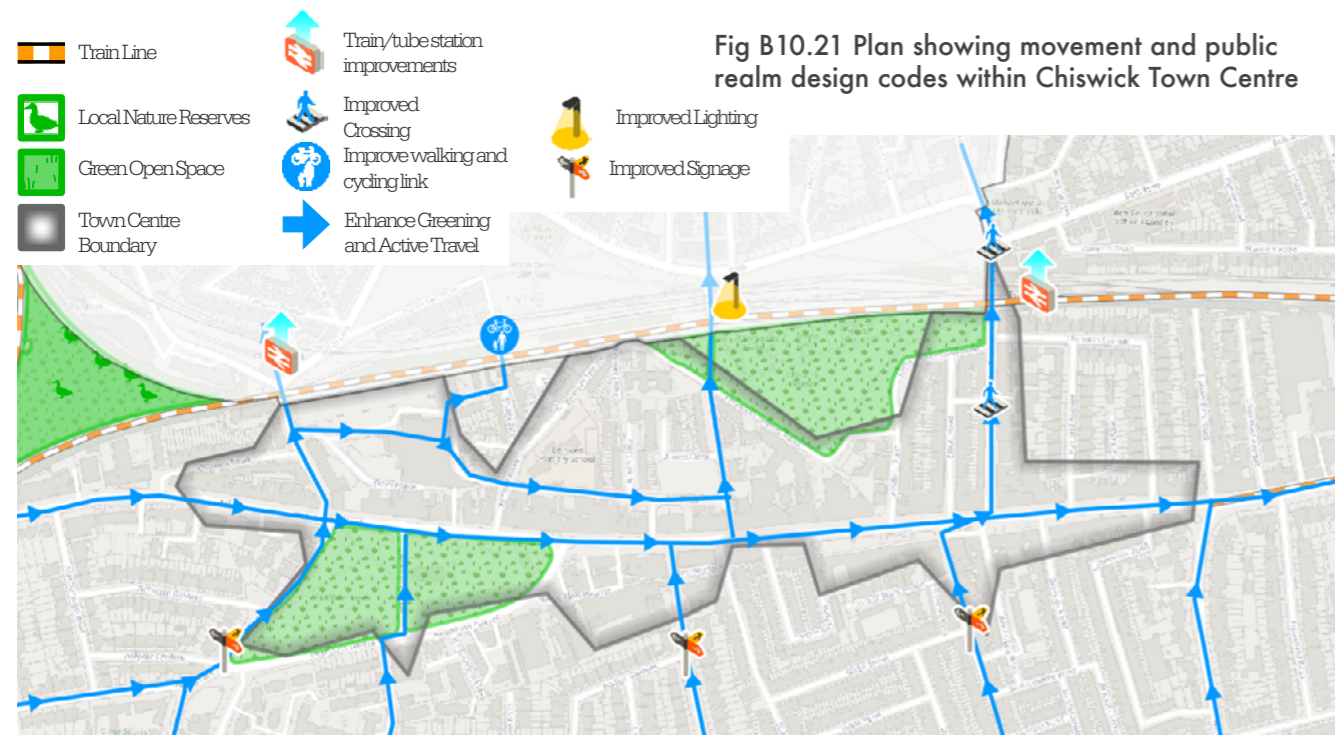
- B10.61** Resident's noted at consultation that there are a lack of dropped kerbs within the area, which causes difficulty for those with limited mobility to cross the streets. The Council has a Pedestrian Accessibility Programme and is introducing dropped kerbs wherever needed.
- B10.62** Improvements to Acton Lane, Fishers Lane, Duke's Avenue, Sutton Court Road, Sutton Lane North and Turham Green Terrace are priorities for enhancing cycle and pedestrian accessibility. Improvements to these streets could include:
- Segregated cycle facilities;
 - Inclusion of bicycle parking facilities;
 - Increasing street greening and street trees;
 - Maintenance of existing street trees;
 - Introducing additional zebra crossings;
 - Removal of some car parking spaces (where appropriate).

Stations

- B10.59** Step free accessibility at local stations is a key issue for residents, as none of the stations in the vicinity of the town centre have step free access, requiring people with limited mobility to travel to Hammersmith, Acton Town or Chiswick Train Station. Step-free access and station upgrades are key to encouraging more people to use sustainable transport. The Council will work with TfL and landowners to deliver improvements and will expect development to contribute to improvements, where appropriate.
- B10.60** The following improvements are required at Turnham Green Station:
- An improved arrival and departure experience, with a widened and resurfaced pavement and a new zebra crossing
 - Improvements to the appearance of the station building and platforms including cleaning, repair and repainting of the platform canopies.

Key policies and guidance

- Priority Cycle Route Network
- Emerging Cycle Mapping



West Chiswick

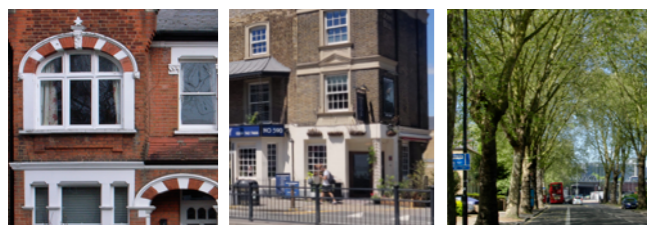


What's special about West Chiswick?

B10.63 The area sits mainly within the Chiswick Gunnersbury Ward, with a small part sitting within the Chiswick Riverside Ward. Both Chiswick Gunnersbury ward and Chiswick Riverside wards are ranked among the least deprived wards in Hounslow and both have high levels of civic assets and an active civil society attracting a lot of third sector funding. The average age of people living in the Chiswick Gunnersbury Ward and the Chiswick Riverside wards are 37 and 38 respectively, higher than the Borough wide average which is 34 (GLA, 2015) and are home to a predominantly white British population, however also have a significant Asian population (10% & 9% compared with the UK average of 6.9%) and a higher than average Black population (4% & 7% compared to the UK average of 3%) (Census 2011). Although Chiswick High Road continues through the neighbourhood, it becomes more residential in character. There are still retail and commercial uses along much of the High Road but it has a separate character to the town centre, and plot sizes along the High Road tend to be larger. Gunnersbury Station is located within this neighbourhood.

B10.64 The character of the area is varied; it is home to two conservation areas (Wellesley Road and Thorney Hedge), along with part of a third (Turnham Green), but also large modern developments such as Chiswick Business Park. The area also has the Gunnersbury Triangle Nature Reserve, a local highlight. The green oasis is enclosed by railway lines and is managed by London Wildlife Trust alongside a dedicated team of volunteers.

Selection of locally distinctive architectural details and materials which may inform new developments



Brick & masonry detailing Ground floor articulation Tree Lined Streets

The neighbourhood's boundary is bordered by two further conservation areas: Strand on the Green to the south and the Gunnersbury to the north. The frequency of street trees and planting in the west of the area is generally low and needs to be increased.

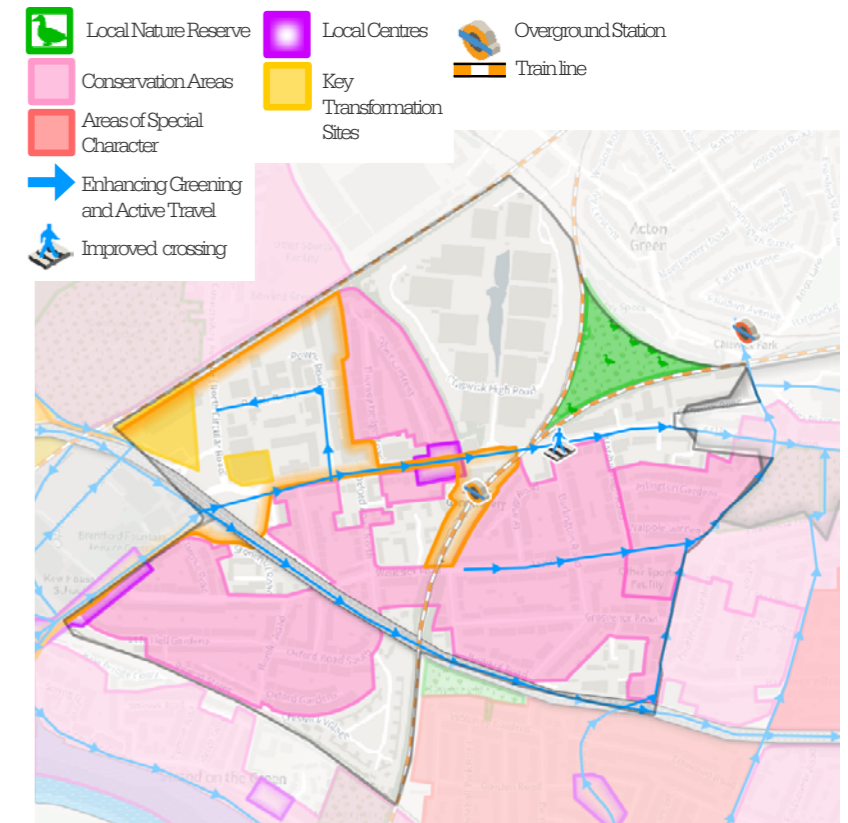
B10.65 Away from the High Road, the neighbourhood is primarily residential, with some notable exceptions. The residential areas are mainly comprised of high quality Victorian housing which varies across the neighbourhood from larger detached villas to smaller terrace houses. There are also pockets of 20th and 21st century architecture, most of which sit comfortably within the area.

B10.66 The exceptions to this predominantly Victorian residential character are Chiswick Business Park and the Power Road Estate. These two employment sites have quite different characters, although both add positively to the character of the area. Chiswick Business Park is a 21st century development of office buildings planned around a central green space. The buildings are well designed and the landscaping adds significantly to the site. The Power Road Estate is a predominantly low rise group of buildings, some of which are locally listed, and it is an important hub for SMEs.

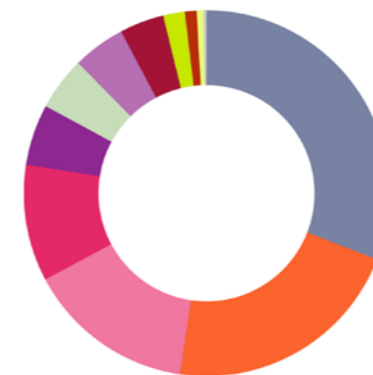
Prompts for character-led development

- New developments must be generous in their provision of private and community outdoor amenities and children's play areas to address the deficiency in public open space.
- Developments must contribute to the urban greening priority area within the industrial estates/business parks by achieving the UGF target and through greening in the public realm, such as increasing street trees and planting. This will require a considered analysis of existing green infrastructure and a concerted effort to integrate new types of greenery into the built environment including green roofs and walls. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.

- Any new developments should contribute positively to the public realm. Much of the south of neighbourhood has a green and leafy character, a trait which should be carried through to any new development.
- New development at Power Road should reuse and repurpose existing buildings to retain and enhance the character of the area. Car parking could be located to the rear of buildings, situated away from the gardens of homes on Thorney Hedge Road. Development should also include enhancements to the public realm including pocket spaces, public art, meanwhile uses and generous buffer tree planting to shield the residential homes to the east..
- Improvement to Gunnersbury Station to alleviate congestion and the installation of step-free access. Improvements to the crossing over Chiswick High Road, including the removal of barriers.
- The existing building height range in this neighbourhood is 2-19 storeys approximately. New buildings should be between 2-5 storeys. Any proposals for tall buildings should refer to the Local Plan.



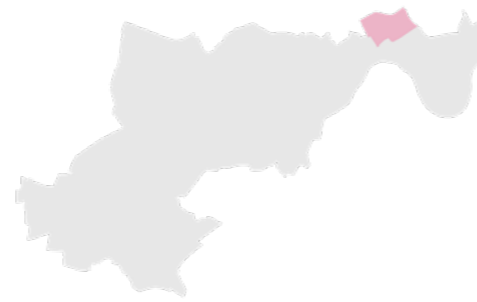
Plan of neighbourhood



Villa typology	30.9%
Industry	10.3%
Institutions headquarters	14.9%
Urban terrace	21.4%
Cul-de-sac	5.4%
Green open space	4.7%
Big box	3.9%
Parades	0.6%
Centres	1.9%
Infrastructure	0.3%



Gunnersbury



What's special about Gunnersbury?

B10.67 The neighbourhood sites within the Chiswick Gunnersbury Ward. The character of the neighbourhood is comprised of three main parts, Gunnersbury Park, the residential areas surrounding it, and the Acton Works site in the south of the neighbourhood. Chiswick Gunnersbury Ward is home to a predominantly white British population, with a significant Chinese community in the residential enclave to the north. Indian and West European locals also reside in the area (2011 Census). The median age of residents is 37, higher than the Boroughwide average which is 34 (GLA, 2015).

B10.68 Gunnersbury Park is a large piece in the neighbourhood and sits within its own conservation area. Gunnersbury Park is home to numerous listed building, including the Large and Small mansions, and the park is a Grade II* registered park and garden. The Large Mansion, itself Grade II* listed, recently underwent a significant restoration and now houses the Gunnersbury Museum. The park and the café are popular places to spend time, for locals and visitors alike. Situated within the park's grounds are a museum, tennis courts, sports hub, children's play area and community garden. The busy Gunnersbury Avenue forms its eastern edge.

B10.69 The conservation area also includes the area on the other side of Gunnersbury Avenue, which is a quiet and leafy residential area, characterised by inter-war cottage estate style

Selection of locally distinctive architectural details and materials which may inform new developments



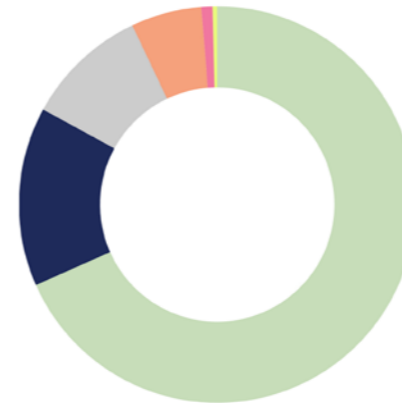
Half-timbering Bay windows Pitched roofs Brick and render

housing with tree-lined streets, front gardens and a private communal space in the centre at Triangle Way. This area is covered by an article 4 direction to maintain the architectural quality and character of the estate.

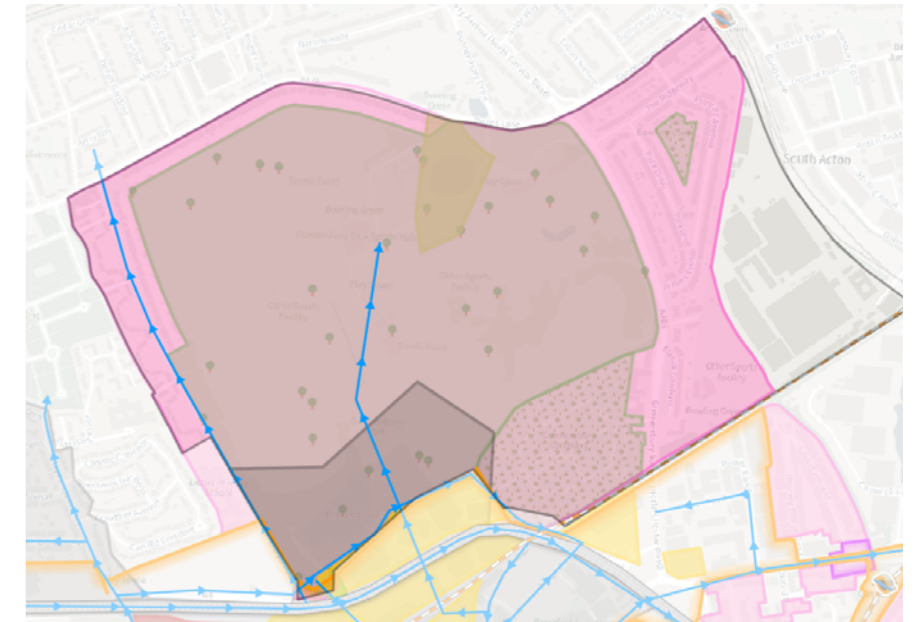
B10.70 To the north is a local shopping parade on Gunnersbury Lane, close to Acton Town underground station just outside the Borough. Gunnersbury Cemetery is a peaceful sanctuary to the south of the park. To the east of the area is the London Underground maintenance facility. The character of the area is varied; it is home to two conservation areas (Wellesley Road and Thorney Hedge), along with part of a third (Turnham Green, which was expanded following appraisal in 2020), but also large modern developments such as Chiswick Business Park.

Prompts for character-led development

- There are very few opportunities for redevelopment in this neighbourhood, most of the neighbourhood is already developed for residential properties or is within the park.
- Smaller scale extensions to existing houses should conform with our residential extension guidance.
- Any development that does come forward needs to be based on a contextual understanding of the area and should be of a similar size, scale and massing as the surroundings.
- The existing building height range in this neighbourhood is 2-4 storeys approximately. Interventions could successfully conserve the modest scale of this neighbourhood with a height range of between 2-4 storeys.



Green open space	68.3%
Suburban	14.6%
Infrastructure	10%
Cottage estate	5.8%
Institutions headquarters	0.9%
Parades	0.4%



Plan of neighbourhood

- Green Open Space
- Conservation Areas
- Registered Parks and Gardens
- Enhancing Greening and Active Travel
- 20% most deprived
- IMD decile nationally



Grove Park & Strand on the Green



What's special about Grove Park & Strand on the Green?

B10.71 Grove Park & Strand on the Green takes in the area of Chiswick south of the A4 up to the A316. Locals enjoy the area's tree-lined streets, handsome building, and access to the River Thames. The local shops at Grove Park Road and the riverside pubs are also highlights. Grove Park Primary, Strand-on-the-Green Infant and Primary, and Chiswick School are the main local schools and buses from Great Chertsey Road connect the area with Richmond, Chiswick, Hammersmith and Ealing. Trains from Chiswick Station link the area to southwest and west London, and Clapham Junction. Chiswick House Gardens is a valuable green space in the neighbourhood.

B10.72 The area sits within the Chiswick Riverside and Chiswick Homefields wards. The average age across the two is 38 years, compared with the Borough-age median of 34. The area has the highest proportion of 50-69 year olds in the Borough (GLA, 2015). Across both wards, 26.5% of residents identify as having no religion, the second highest figure in the Borough after Turnham Green (2011 Census). Thriving faith communities do exist however - the Iranian Christian Fellowship is one example based on Sutton Court Road. Residents are predominantly white British, although Irish and Western European residents are notable groups.

B10.73 The neighbourhood contains three conservation areas: Strand on the Green,

Selection of locally distinctive architectural details and materials which may inform new developments



Verdant gardens
B10-35

Red brick and decorative features

White render

Chiswick House, expanded in 2021, and Grove Park, also expanded in 2021. Each one has a unique character. Strand on the Green is notable for its intrinsically tranquil setting beside the water's edge, with fishermen's cottages, boat builders' sheds, public houses, maltings and larger and more elegant private houses added in the late eighteenth century. Views of Strand on the Green, especially those from Kew, are important. Chiswick House is centred around the grand Chiswick House and its gardens, along with the mainly 19th and 20th century housing surrounding it. Grove Park's character is defined by its handsome Victorian and early 20th century buildings, there are some interesting later 20th century developments.

B10.74 The residential areas include Victorian terraced streets and villas, as well as more suburban housing, low-rise apartment blocks and cul-de-sacs. The area has a green leafy character with wide tree lined streets. Low-scale dwellings and boathouses along the Thames contribute to a modest and informal relationship to the riverfront. The area also includes the first large Victorian railway suburb at Grove Park.

Prompts for character-led development

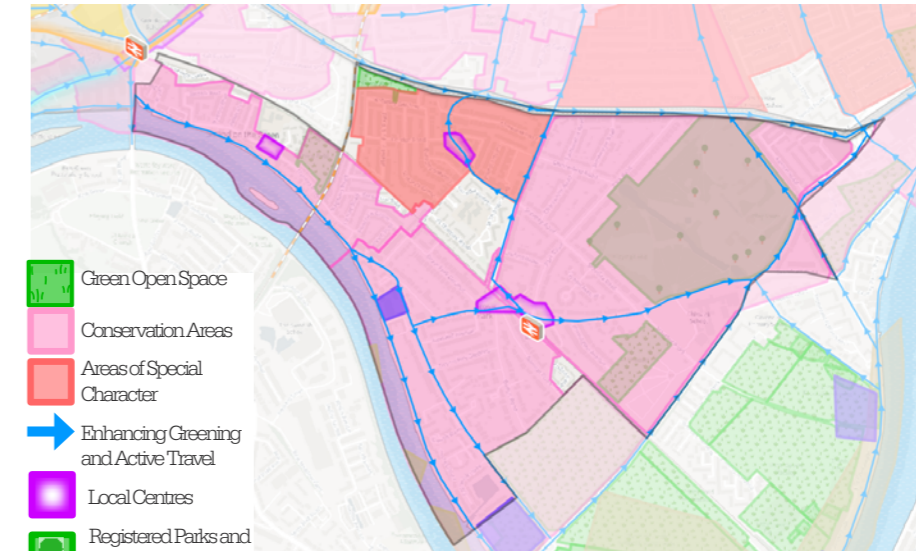
- Any interventions must be respectful to the modest scale and leafy character of this residential neighbourhood. Opportunities to re-use and adapt existing buildings must be prioritised. New developments must be of a high design quality, respect the scale and character of the area, offer a mix of land uses where appropriate, and contribute positively to the streetscape.
- The existing building height range in this neighbourhood is 2-6 storeys approximately. Interventions could successfully conserve the modest scale of this neighbourhood with a

height range of between 2-4 storeys.

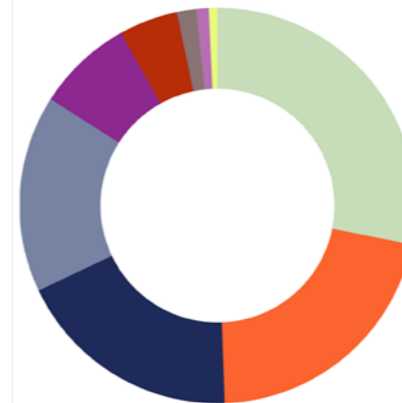
- As part of any proposed enhancements along the riverside consideration should be given to the provision of appropriate riparian life saving equipment.
- Vast swathes of southern parts of the neighbourhood, including areas around Strand on the Green and Grove Park have poor PTAL ratings of between 0 and 2, which makes it difficult for residents to access Chiswick town centre using public transport. Improving access to new or extended bus routes is a key priority, in addition to removing barriers to active travel.

"Tree-lined streets, period properties, easy access to the river, nice parks and open spaces"

- Local resident



Plan of neighbourhood



Green open space	28.2%
Urban terrace	21.3%
Suburban	18.5%
Villa typology	16.1%
Cul-de-sac	7.8%
Modern urban	4.8%
Slabs	1.6%
Mansion blocks	1%
Parades	0.7%



Old Chiswick & Duke's Meadows



What's special about Old Chiswick & Duke's Meadows?

B10.75 Duke's Meadow and Old Chiswick sits in the area between the Thames and the Great Chertsey Road, with a small piece included which is just over this road. The area encompasses Duke's Meadow, which is an important green space and resource for the community in Chiswick. The other main part of the neighbourhood is Old Chiswick, which is the remnant of the original village of Chiswick. The neighbourhood also includes a mix of styles of 20th century housing, Cavendish School, Chiswick Old Cemetery. The area has a green and leafy character with mainly low rise buildings.

B10.76 The area sits within the Chiswick Homefields ward. The average age within the ward is 37, compared with the Borough-age median of 34. 27.2% of residents identify as having no religion, the second highest figure in the Borough after Turnham Green (2011 Census). There is a significant Black African community around Edensor Gardens and Alexandra Gardens (2011 Census). The neighbourhood is bordered by busy A roads – the Great Chertsey Road and the A4. There are no train or tube stations in the neighbourhood, but it is served by local buses.

B10.77 Old Chiswick is what remains of the original village of Chiswick, and much is covered by the Old Chiswick Conservation Area, with its parish church of St Nicholas (Grade II* listed)

and burial ground, an extensive row of grand houses facing the river and the small island of Chiswick Eyot, and a large brewery. The more industrial brewery buildings sit side-by-side with grand houses lining the riverfront, creating a unique mix of industrial character and refined architecture. The buildings along Chiswick Mall benefit from gardens which sit across the road from the houses and front the Thames.

B10.78 The neighbourhood benefits from several green spaces. Duke's Meadow is the largest of these green spaces and offers opportunities for recreation with various sports facilities, a riverside walk, allotment gardens and play area. The area also has sports grounds and rugby pitches on the other side of the A316 from Duke's Meadows. Boat houses are also present which help make the most of opportunities for recreation on the Thames.

Prompts for character-led development

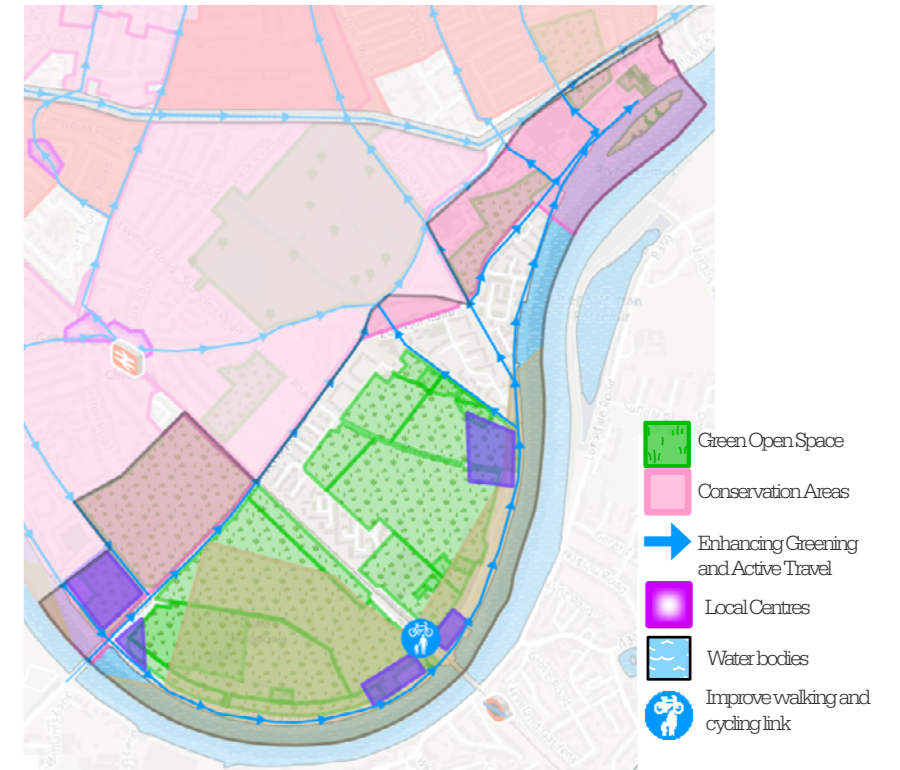
- As part of any proposed enhancements along the riverside consideration should be given to the provision of appropriate riparian life saving equipment.
- The existing building height range in this neighbourhood is 2-5 storeys approximately. Interventions could successfully conserve the modest scale of this neighbourhood with a height range of between 2-4 storeys.
- There are very few opportunities for developments in the neighbourhood, however any interventions must be respectful to the modest scale and leafy character of this

residential neighbourhood. Opportunities to re-use and adapt existing buildings must be prioritised. New developments must be of a high design quality, respect the scale and character of the area, offer a mix of land uses where appropriate, and contribute positively to the streetscape.

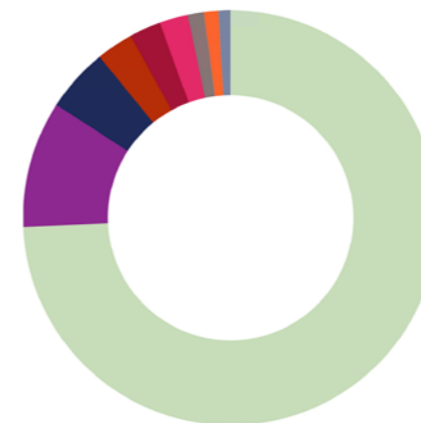
- Areas around Duke's Meadow have poor PTAL ratings of between 0 and 2, which makes it difficult for residents to access Chiswick town centre using public transport. Improving access to new or extended bus routes is a key priority, in addition to removing barriers to active travel and actively promoting walking and cycling. The Council encourages extension of bus routes to serve the sports facilities at Duke's Meadows.

“Tree-lined streets, period properties, easy access to the river, nice parks and open spaces”

- Local resident



Plan of neighbourhood



Green open space	74.3%
Cul-de-sac	9.9%
Suburban	5%
Modern urban	2.9%
Big box	2.4%
Industry	2.2%
Slabs	1.3%
Urban terrace	1.1%
Villa typology	0.9%



Selection of locally distinctive architectural details and materials which may inform new developments



White Render Half timbering Red brick and decorative features Weatherboarding



SITE DESIGN CODES

CHISWICK

Sainsbury's Chiswick Site Code41

Sainsbury's Chiswick

Site overview

The site contains a key town centre food store with an associated surface car park, bounded north by a railway. To the east are a mix of low-scale residential building types - including recent new builds. To the south, mixed-use commercial units back onto Essex Place with front access from Chiswick High Road. More mixed-use commercial units can be found along the parade on Acton Lane to the west.

Stakeholders

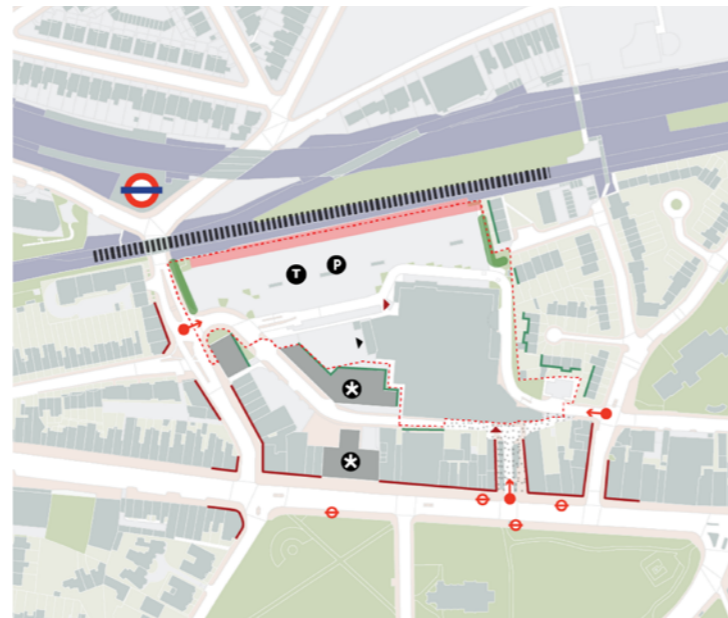
- Sainsbury's
- Transport for London
- Network Rail

Suitability and sensitivity issues identified in Part C Types

- Adjacent to Turnham Green Conservation Area
- Within 200m from a green open space of high quality (Turnham Green)
- Within close proximity to listed buildings
- Within Chiswick Town Centre
- High levels of public transport accessibility (PTAL score 4-5)

Constraints and opportunities

- Top 5% performing stores in the country with majority of takings being from in store sales (91.5%). While the site undergoes redevelopment, trading would ideally continue on site, however this should not compromise the design quality of the development proposal
- The site sits within close proximity to a tube station, Chiswick Park, and bus stops making it well served by public transport.
- Maintaining space for servicing from the west is imperative; allowing generous space for lorries to turn.
- TFL corridor to the rear of the site, space must be allowed for in case of emergency. Noise mitigation must be factored into design.
- Opportunities exist to intensify and optimise the site density by introducing new and affordable homes in an area that is well served by public transport and other amenities.



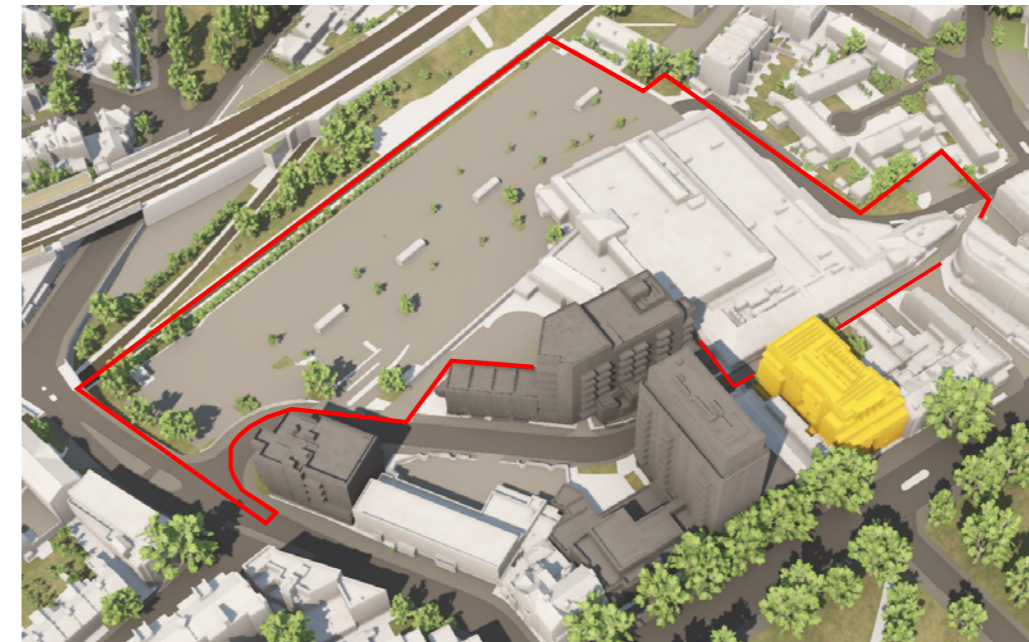
Location plan

- Site boundary
- Major route
- Railway line
- Commercial frontage
- Sensitive frontage
- Commercial access
- ▲ Servicing access
- Car parking
- ⊠ Existing tall building
- Potential for taller element
- Consented scheme

Key information from Site Allocation

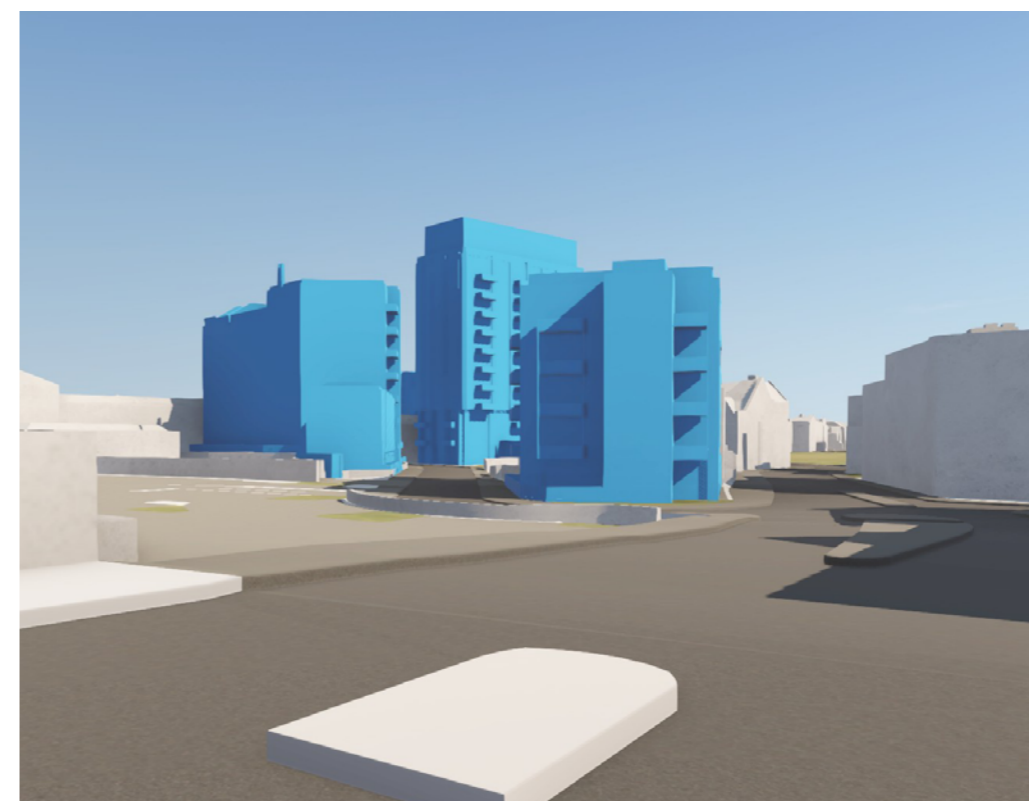
Existing site:	Emerging single Local Plan Site
Source	Allocations
Ownership	Sainsbury's
Existing use	Retail superstore and associated car parking
Size	1.8 ha
PTAL	4/5
Prevailing height	2-13 storeys

- Opportunity to improve the connectivity of the site and the links to Chiswick Park Station and Chiswick Town Centre through enhanced public transport, walking and cycling infrastructure and improved public realm.
- Opportunity to enhance the retail environment for customers by providing a modern retail foodstore
- Opportunity to improve access to Turnham Green and Chiswick Park stations, including potential contribution to step-free access.
- Most of the site is located within an Archaeological Priority Area.



Aerial view facing north-east showing the scale of nearby buildings

- Recently completed scheme
- Consented scheme
- Site boundary



Key street level view facing south-east approaching the site from Chiswick Park Station



Design guidance

Objectives

- Redevelopment of the Sainsbury’s Chiswick supermarket and surface car park to provide a modern food store embedded within a mixed use development.
- The provision of high quality housing that is well integrated within Chiswick town centre.
- Improvement of pedestrian and cycle movement to and through the site, particularly along Essex Place.
- Enhance the existing Essex Place Square market and deliver active public realm along the interior streets.
- Enhance the legibility and permeability of the site and the wider area through the scheme’s design, form and movement routes.
- Vehicular access to Acton Lane should be improved to provide better pedestrian and cycle facilities.
- Parking provision for the retail floorspace should be at a level that is appropriate to support the retail use and should reflect the viability of the large retail store within the town centre and the local circumstances as per London Plan Policy T6.3 (G) and paragraph 10.6.4 of the London Plan.

Access and Movement

- Vehicular access from Acton Lane should be improved to facilitate vehicular movements associated with the retail car parking and servicing, in addition to better pedestrian and cycle facilities.
- Vehicular movements within the site should be limited to that required to service the retail and residential development and access the retail store. The remainder of the site should be only accessible to pedestrians, cyclists and waste disposal vehicles.
- It is important to reduce the interface of large vehicles servicing the retail store and pedestrian movement. A design solution to this could be to use the area to the south of the railway line to facilitate servicing of the store. Other design solutions may be acceptable where they can evidence that pedestrian movement will not be

impinged by delivery and servicing.

- Routes through the site should provide legible routes and wayfinding to Chiswick Park Station.
- Access for maintenance of the railway should be factored into the design through early consultation with TfL/Network Rail.
- The new supermarket should have welcoming entrances that are clearly visible from the various access points into the site.
- Level changes between the site and Acton Lane closer should be carefully considered to improve access from Chiswick Park station. Level access to the site should be provided.
- Improvements to the public realm along Essex Place Square are being made to emphasise pedestrian movement. Similar improvements are expected as part of the development, with pedestrian access linking from Essex Place.

Form of development

- Buildings should respect the scale of housing to the east of the site
- More compact, indoor car parking should be embedded within an underground or ground floor podium, for use associated with the retail store.
- Building lines should set back further from Essex Place to widen this street, ensuring the development mediates between differing setbacks of existing buildings.
- In order to minimise visual impact from Turnham Green, there is potential for taller blocks to be located where they are largely shielded from view behind Empire House, especially from the key viewpoints marked on the spatial framework.
- Blocks located in the southern portion of the site should avoid visual intrusion in views from Chiswick High Road.
- The development should incorporate a range of heights to break up massing and avoid a continuous, bulky mass or ‘wall’ of development from surrounding areas, such as from Acton Green and Turnham Green and to respect views of the Church of St Albans.

- Proposed development should explore opportunities to a comprehensively distribute massing and height to ensure views from Turnham Green are not negatively affected.
- High quality design is essential- care should be taken to include high-quality architectural expression and articulate the massing and façade grain. Other design solutions to soften the visual impact of the scheme could include implementing inset terraces and balconies and using appropriate materials and colours to soften the visual impact of the scheme.
- The scheme design should be of high quality and should be of a character and identity based on a study of surrounding urban forms and urban grain.

Relationship to surroundings

- There is an opportunity for the scheme’s design to play a role in way finding, signalling routes to the station.
- Development proposals should be accompanied by a noise mitigation strategy.
- Development proposals should respect views from Turnham Green, should safeguard the setting of Turnham Green and the Church and preserve the open aspect of Turnham Green.
- Development proposals should also respect Chiswick Park Station and the prominence of its tower, avoiding competing with it.
- Views testing should be undertaken from the key points identified on the development proposal diagram in addition to longer range views.

Street level experience

- Blank frontages around the supermarket and car park should be avoided, except where required for operational purposes.
- High quality paving and street lighting will be required to improve way finding and the sense of security along Essex Place.
- New streets should aim to have a minimum carriageway width of 5.5m and 2m footways on

- either side excluding private defensible space.
- Pedestrian experience should be enhanced through an appropriate sense of enclosure (i.e.. the ratio of building height to street width), an enhanced public realm and landscaped open space. This should include significant greening and introduction of street trees.

Wider contribution

- The existing market on Essex Place Square should be extended further north to tie in with public realm improvements. Improvements should bring together a coordinated public realm ensuring a seamless tie in with neighbouring developments. Market stalls should face into the pedestrianised centre of Essex Place Square.
- Inner streets should facilitate cycling and pedestrian movement across the site, particularly between Belmont Primary School and the station.
- The site should include a significant area of open green space and green landscaping that is publicly accessible.

Climate mitigation and adaptation

- SuDs and tree planting along inner streets of the site can help to mitigate surface water flooding as well as noise.
- Proposals should incorporate recommendations set out in Network Rail’s Route and Climate Change Adaption Plans 2024-2029.

Good practice precedents for guidance



Fig B10.22 Spatial framework

*For more information on site capacity and building heights refer to the Local Plan and Tall Buildings Study



- 1 Grahame Park by Peter Barber Architects © Morley von Sternberg - A mixed-use scheme with a large supermarket at ground floor and residential use above. A ground floor colonnade creates a shelter for user and the consistent material character of the development blurs the distinction between its upper and lower floor uses.
- 2 Sainsbury's Fulham Riverside by Lifschutz Davidson Sandilands Architects © James Brittain - Positive example of how to integrate a large food store with high density, mid-rise housing while reflecting riverside wharves characteristic of the area through the roofscape.
- 3 Hawley Wharf by AHMM ©Tim Soar - Recessed balconies together with an inset courtyard ensure the new homes have sufficient private outdoor amenity space.
- 4 Thanopoulos Supermarket Kifisia by Klab Architecture © Mariana Bisti - The building uses perforated aluminium to mimic pitched roofs, which gives the supermarket its strong design identity, while also reflecting a modest scale and residential character. The recessed ground floor utilises timber and downlighting, creating a warm and welcoming entrance to the store.

