

May 2024

HOUNSLOW CHARACTER, SUSTAINABILITY AND DESIGN CODES SPD

PART B8 OSTERLEY



London Borough
of Hounslow

Allies and Morrison
Urban Practitioners



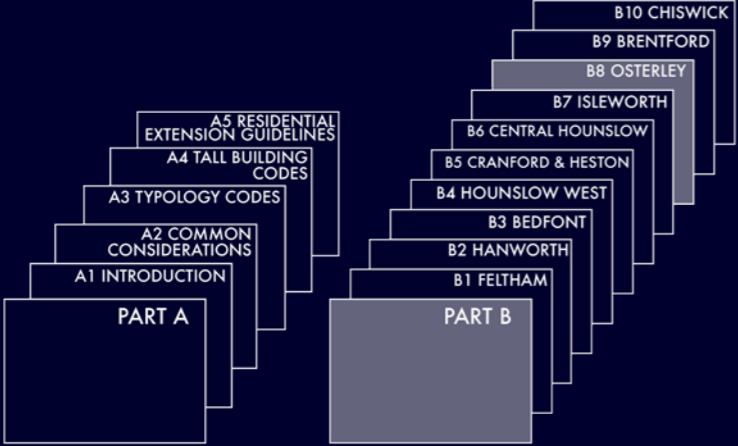
B PLACES

Document structure

This Character, Sustainability and Design Codes SPD is comprised of the following documents;

- Part A: Introduction & Design Codes
- Part B: Places

These are individual files that must be opened separately to view. Use this diagram to help navigate the structure of the Character, Sustainability and Design Codes SPD.



- FELTHAM
- HANWORTH
- BEDFONT
- HOUNSLOW WEST
- HESTON AND CRANFORD
- CENTRAL HOUNSLOW
- ISLEWORTH
- OSTERLEY**
- BRENTFORD
- CHISWICK

* This is document **Osterley**
To view other places please open the relevant documents as named above.

Introducing Hounslow's places

Neighbourhoods and Places

- B8.1** This section, Part B, explores character at a scale best understood by local people - at the neighbourhood and place level. The Borough is comprised of a series of places and neighbourhoods which each have a subtle, or sometimes distinct, character of their own. The map overleaf shows an interpretation of Hounslow's neighbourhoods. It has been developed using the previous study areas set out in the 2014 characterisation, together with local residents.
- B8.2** Identifying where neighbourhoods begin and end is inherently subjective - there will be overlaps and blurred edges between boundaries, even disagreements, as places in the Borough mean different things to different people. Influencing factors like physical infrastructure (roads, railway lines), natural assets (waterways, open spaces), local parades and centres, ward boundaries, can all help to inform the drawing of neighbourhoods that make sense. The degree of consistency or diversity of different aspects, like a particular typology or ethnic group, is also key to how neighbourhoods are perceived.
- B8.3** The purpose of dividing the Borough into these smaller areas is to provide detail on the special qualities and specific challenges for local people in each neighbourhood. This will help to ensure future growth is fairer, greener and more inclusive, and will help shape policies to ensure that the existing identity and character of neighbourhoods is a key consideration. The 37 neighbourhoods have been grouped into ten larger places. The neighbourhoods have been grouped by similarities in character and sense of place informed by: discussions at community

workshops to understand Hounslow's diverse communities; the mapping of physical assets such as topography, landscape and urban morphology; the historic evolution of each area; analysis of land use and housing typologies and their future growth direction.

Structure of Part B - Places

- B8.4** Part B describes the ten places in the Borough. This chapter has three sections:

- 1. Place-level analysis** - this section sets out the baseline analysis for the place and the vision for the place, which has been shaped by the contribution of local people through an online survey and workshops, and have been developed by considering:
 - What are the area's key assets that people love and that should be celebrated and conserved?
 - What are the current social, economic, health and environmental issues of a particular area that we want to address?
 - What opportunities for growth exist in this area and where? This includes thinking about planned investment in infrastructure, Council aspirations for growth and wider trends e.g. changing role of high streets, shift to active travel informed by existing or proposed transport hubs/links/interchanges.
- 2. Neighbourhoods** - this is design guidance that sets out the special qualities for each neighbourhood in turn, and what that means for development in the area. Each neighbourhood page includes photos, key materials and features, the distribution of typologies and where possible, quotes from

local people to help convey what makes each neighbourhood unique.

- 3. Sites** - this section draws on the design codes set out in Part A and for the Neighbourhoods to provide design codes at a site-specific scale on key sites identified by the Council.

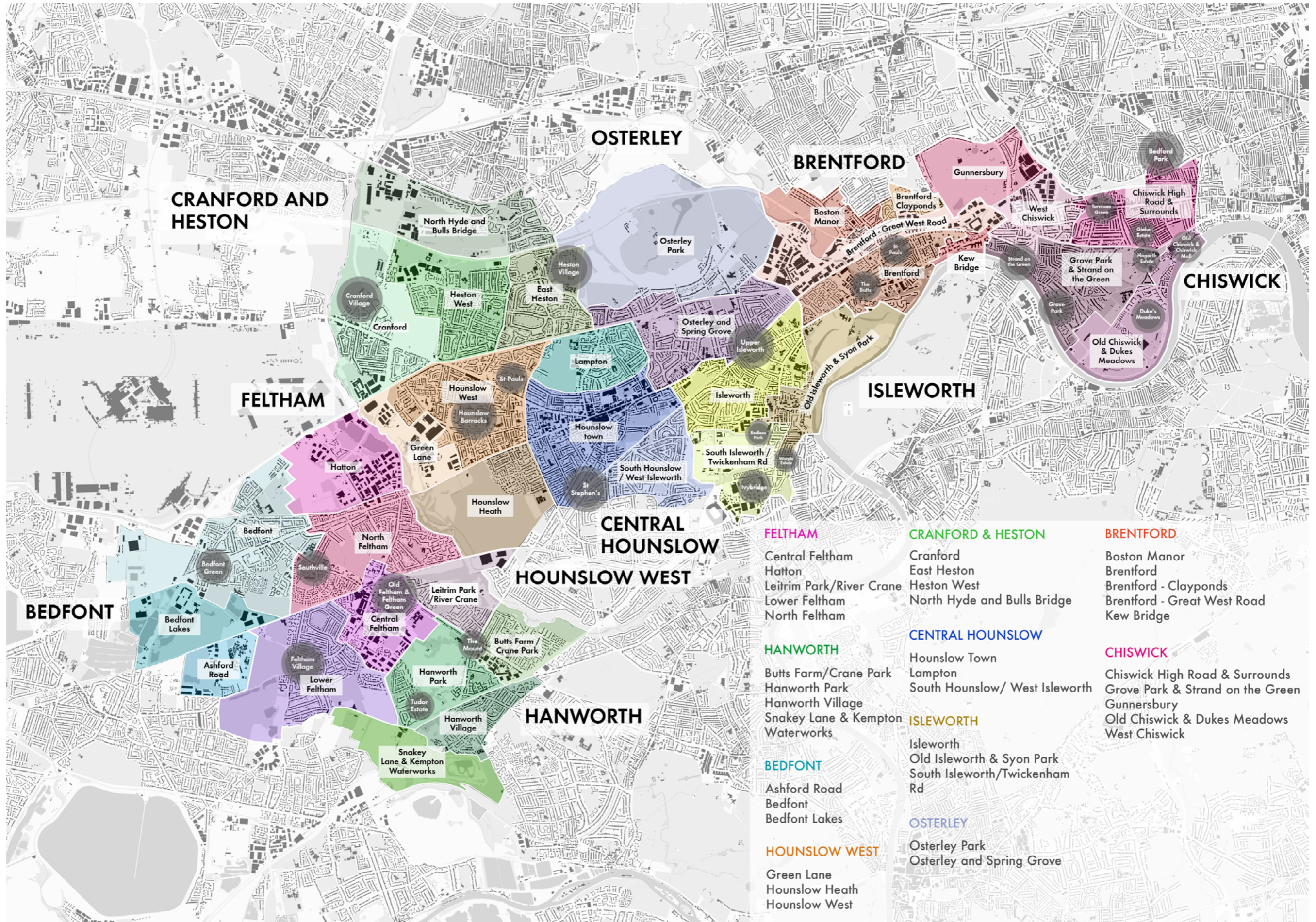
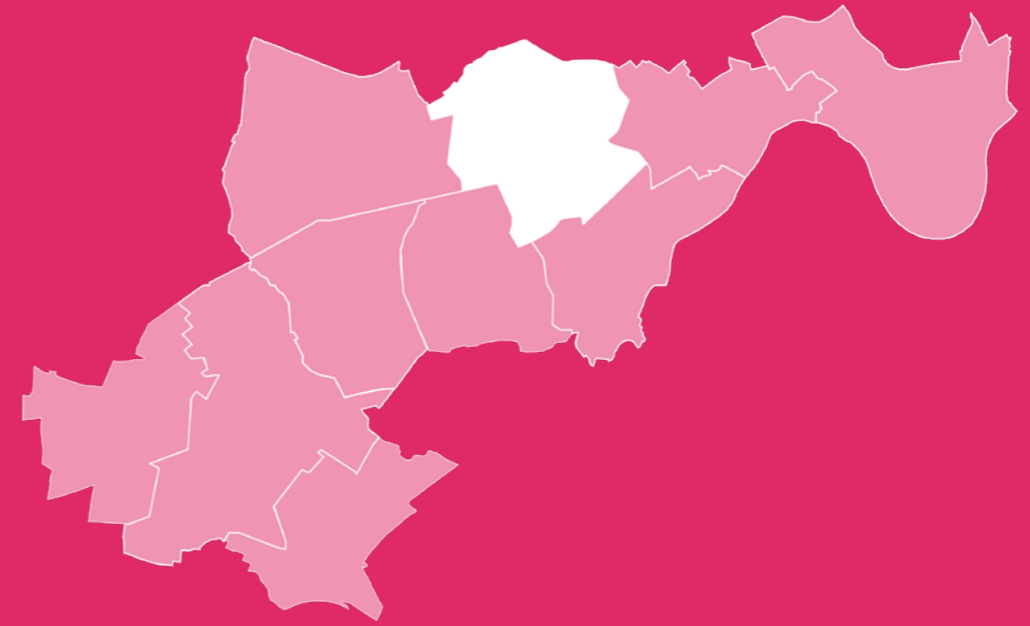


Fig B8.1 Hounslow's neighbourhoods

OSTERLEY



Introducing Hounslow’s places.....5

Towards a greener, healthier, cleaner, safer, thriving and liveable...
Osterley..... 15

Osterley Park23

Osterley and Spring Grove25





Towards a greener, healthier, cleaner, safer, thriving and liveable... Osterley

B8.5 There is a really diverse mix of activities, buildings, local centres and natural assets in this part of the Borough that evolved from its long and layered history. Pockets of Victorian residential areas such as Spring Grove have endured over time, and are now part of a patchwork of residential types - from 19th century terraces to inter-war semis, mansion blocks and post-war housing estates. There are also a significant number of institutions, especially large schools.

B8.6 The area has a transitional feel between the more urban around Brentford and Chiswick and the more consistent suburban character around Hounslow and Feltham. It is home to numerous heritage assets, including two conservation areas and a great multitude of listed buildings, as well as the historic estates at Osterley. The large landscaped park is a key contributor to the character of the area. There are also many smaller parks like Jersey Gardens. Over 50% of Osterley is green space, the largest amount across the six areas of Hounslow. Linking up the green spaces with those in Isleworth through a network of north-south routes forms part of the vision for the area, ultimately to bring environmental benefits and support physical health and wellbeing among local residents.

B8.7 The worst air pollution in Osterley can be found along the major and minor road networks. Interventions to support improvements in air quality along these routes should be prioritised.

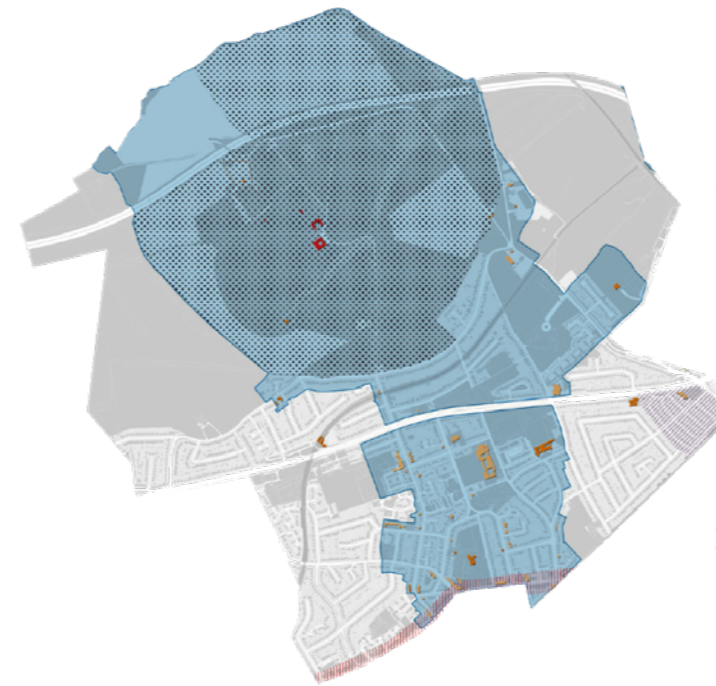


Fig B8.2 Heritage. Blue areas show conservation areas. Red hatch shows archaeological priority area.

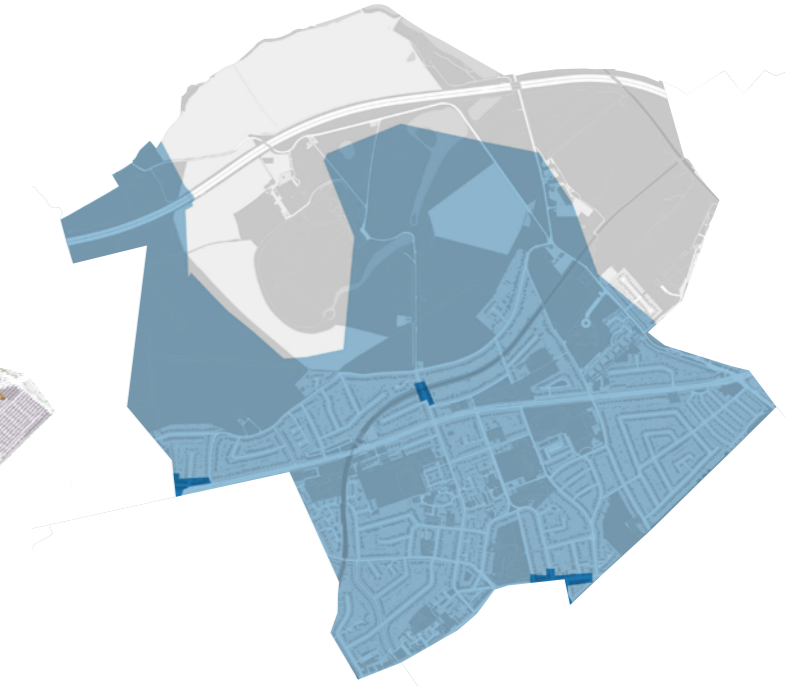


Fig B8.3 Proximity to centres (15 min walk) Light blue shows areas within a 15 min walk from a centre (dark blue)

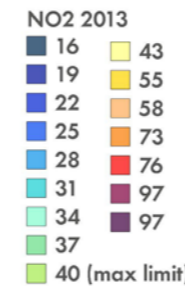
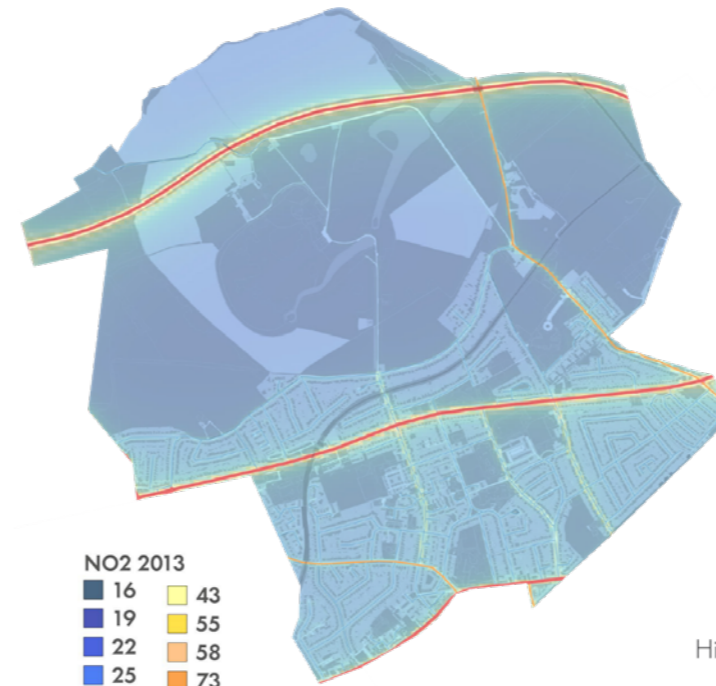


Fig B8.4 Air Quality Poorer air quality is found along the major routes

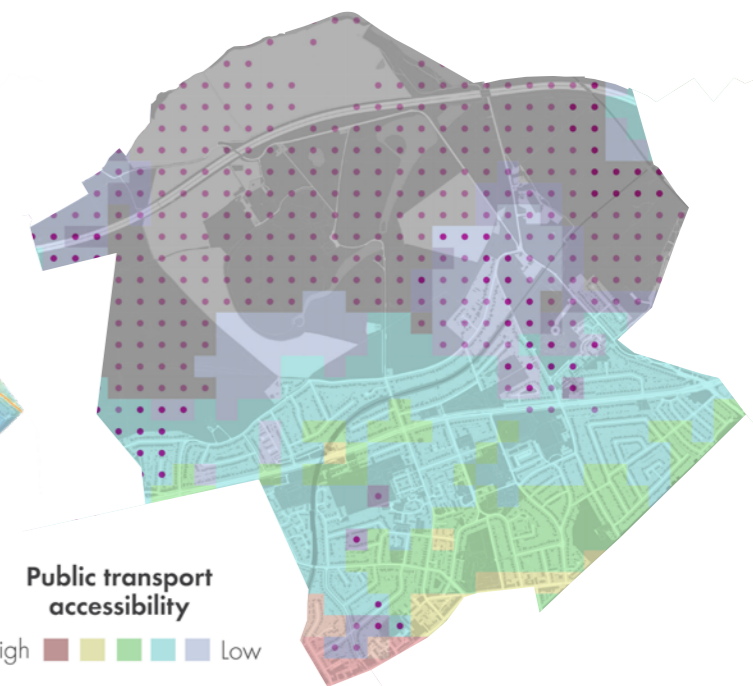


Fig B8.5 Public Transport Accessibility Levels (PTAL)

B8.8 Residents in Osterley reflect the built diversity of the area - in the Osterley & Spring Grove ward, no one ethnic group dominates, but instead there are similar proportions of white British and Indian residents, as well as a significant Pakistani community. Osterley sees a greater mix of faiths compared with Isleworth, with significant numbers of people identifying as Christian, Muslim, Hindu and Sikh. The mean age of the Osterley & Spring Grove ward population was 37 years compared to a mean age of 35 years for residents in the whole of Hounslow in 2011.

green assets are really important. That means existing green corridors need to be supported and enhanced, but also that tree-lined streets need to be recognised and supported for their role in the green network. On these streets, it is the combination of trees and planting both in the public realm as well as within plots that make them attractive and function well as green connections.

B8.9 Osterley and Spring Grove has the lowest average population density of any ward in Hounslow, with 21 people per hectare, compared to the Hounslow average of 54 people per hectare. This is largely due to Osterley Park in the north of the area where population densities are naturally low.

B8.10 Osterley and Spring Grove ranks as one of the least deprived ward in Hounslow, with 7.72% of people living in income deprivation. Osterley and Spring Grove ward preforms the worst in the borough for geographical barriers to accessing services, which relies on distance measured to key services such as GP, schools, etc.

B8.11 Large landscaped gardens and parks are an important characteristic to this area and they underpin a strong green infrastructure base. In this area the links between existing

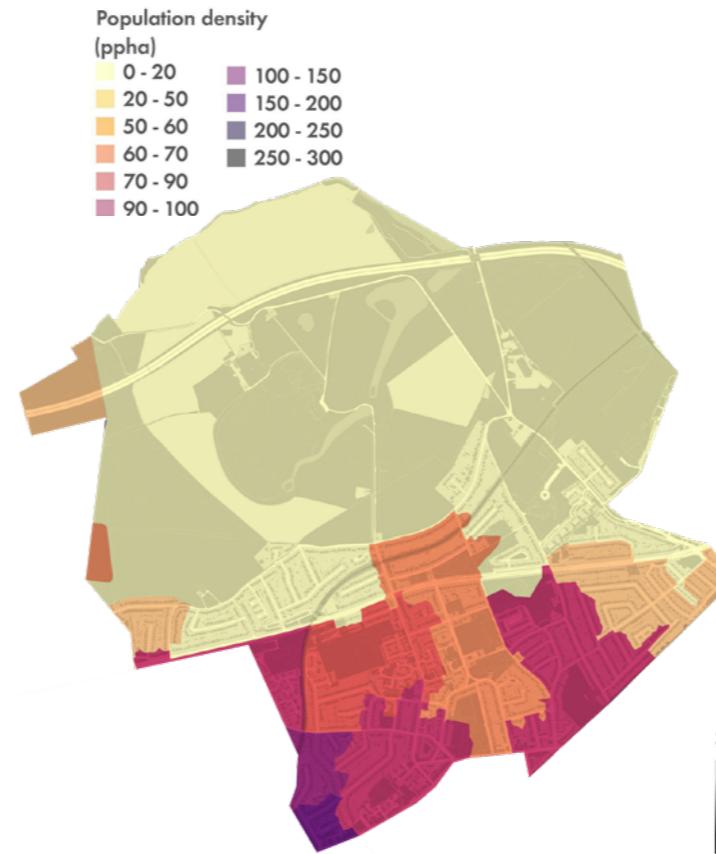


Fig B8.6 Population Density. Darker shades indicate a higher density.

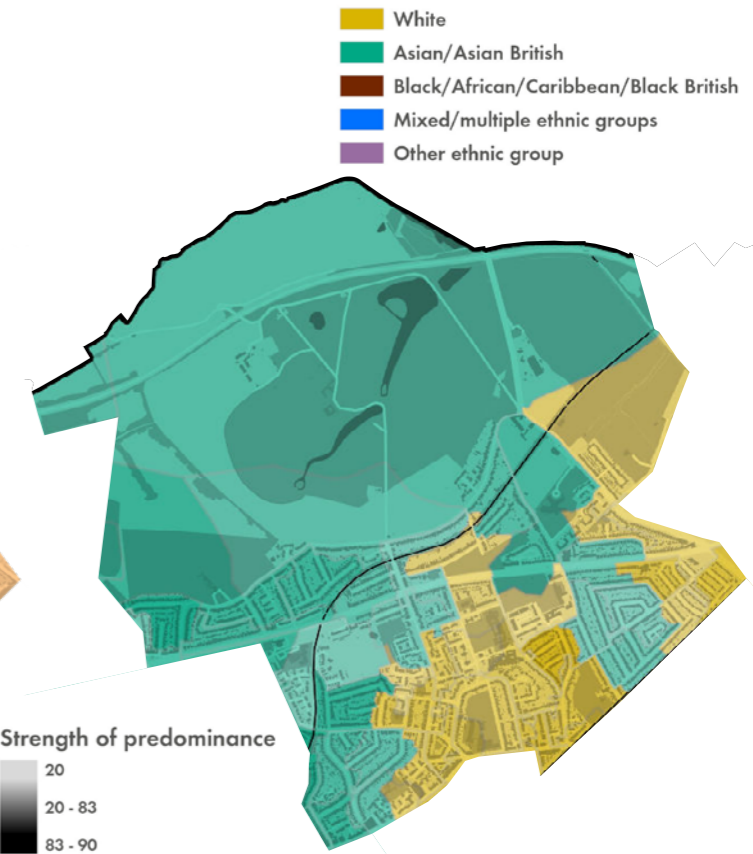


Fig B8.7 Ethnic groups predominance.

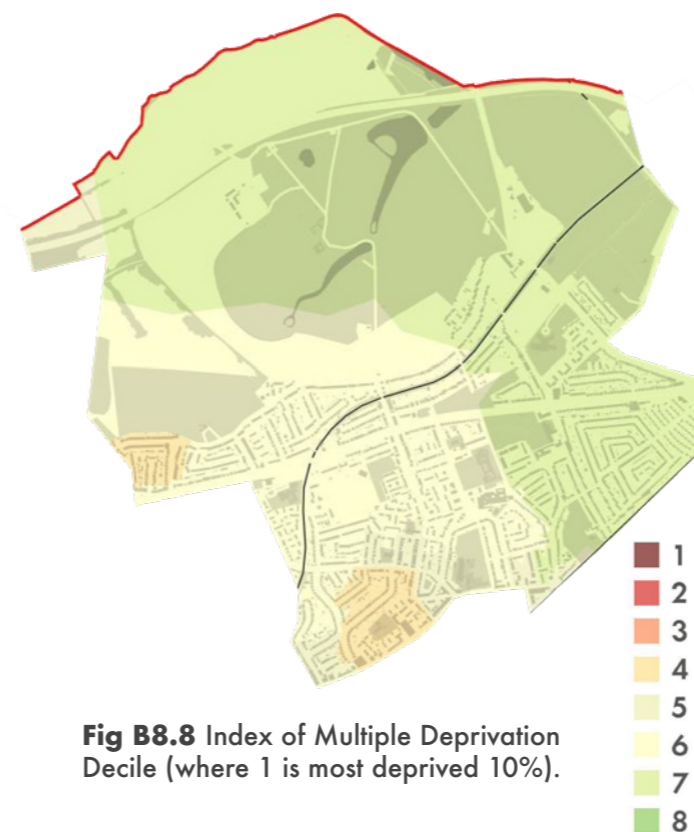


Fig B8.8 Index of Multiple Deprivation Decile (where 1 is most deprived 10%).

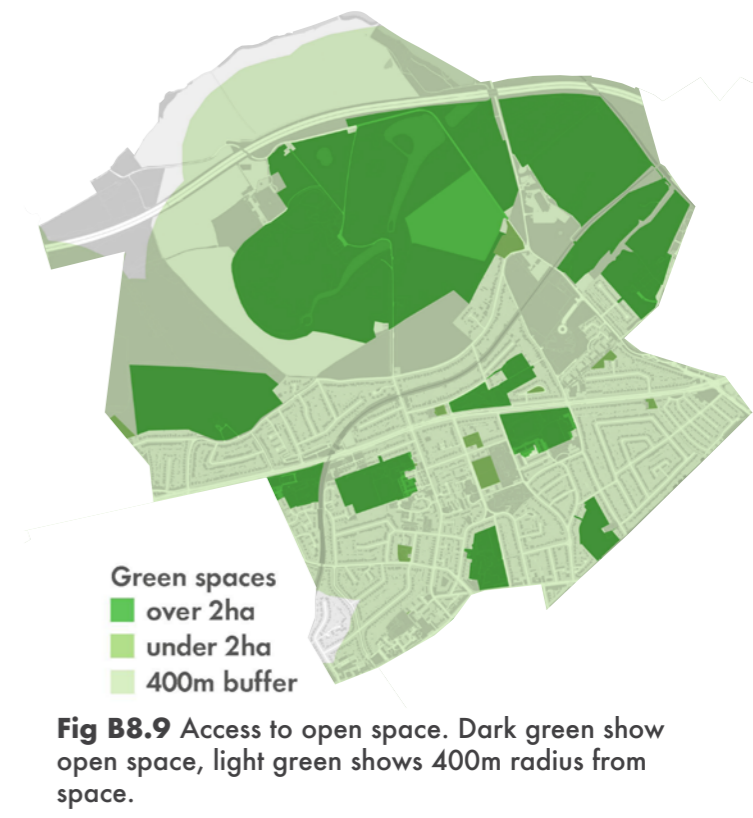


Fig B8.9 Access to open space. Dark green show open space, light green shows 400m radius from space.

B8.12 As Figure B8.10 illustrates, street blocks in Osterley contribute significantly to the existing amount of greening. The suburban residential neighbourhoods generally achieve a score of 0.7. An urban greening priority area has been identified in the north eastern section of this area around Syon Lane (see Green Infrastructure Strategy). Interventions in these locations will need to be sensitive to enhance their resilience.

B8.13 Generally in this area there is a need for future interventions to support green corridors and links between existing assets and so increasing the density of street trees and planting on such links will be priority. Opportunities to increase the incidence and frequency of street trees on adjacent streets would be a priority for contribution.



Fig B8.11 Streets such as Wood Lane which provide links between the area's green spaces should see additional greening to support their role as green infrastructure corridors and walking routes.

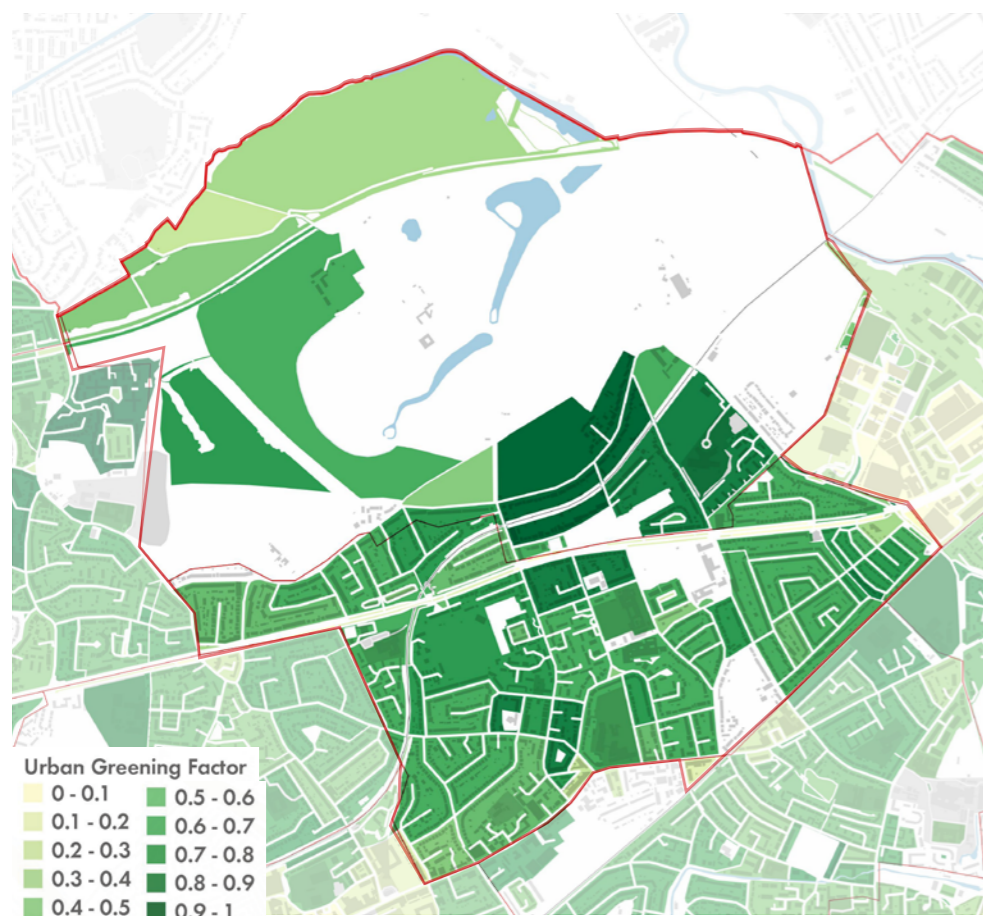


Fig B8.10 Strategic Urban Greening Factor analysis of existing street blocks - this measurement includes the contribution of street trees and planting in the public realm as well as greenery within plots. The adopted London Plan suggests a minimum score 0.3 to support climate adaptation.

B8.14 The existing buildings across the area are predominantly two to four storeys. This area is a patchwork of typologies which have built up over time and underline the area's diversity. Looking forward there will need to be consideration of how the large areas of suburban housing can be enhanced and intensified to improve environmental performance of existing stock.

B8.15 New development should respect existing density levels and aim to achieve a Floor Area Ratio of greater than 1.5. This may require a 50% uplift in building heights in some locations (for example, the potential for three storeys in two storey areas).



Fig B8.13 Approximate existing building heights.

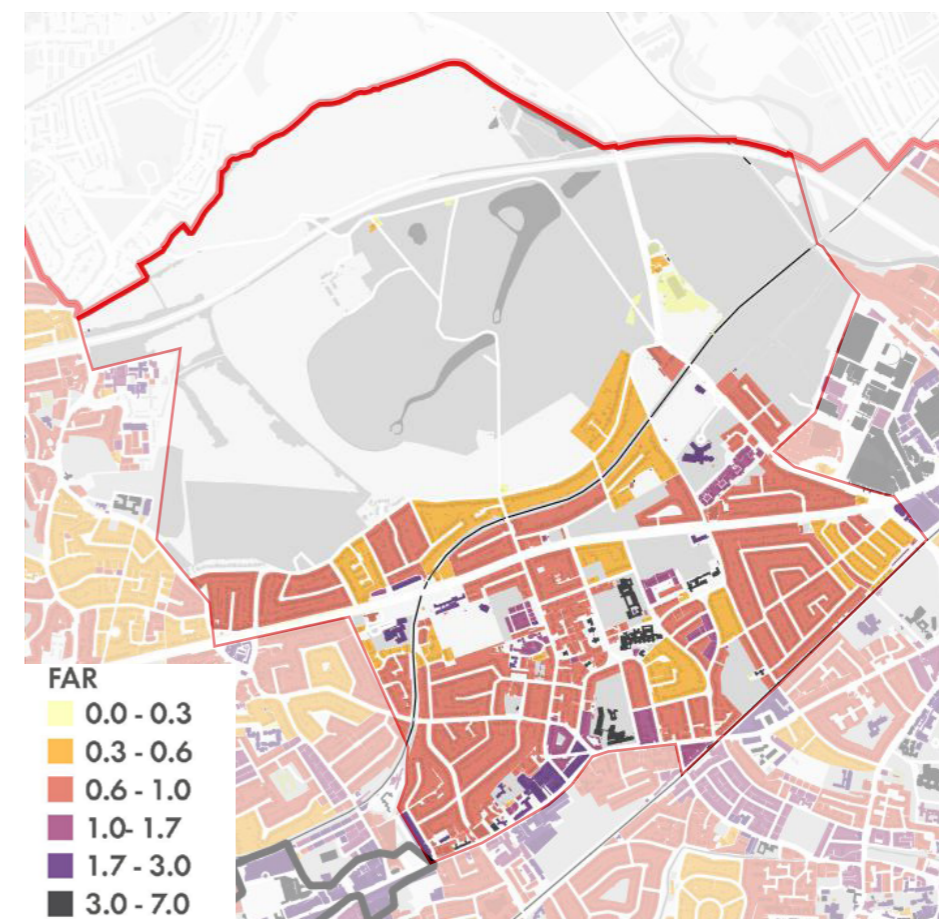


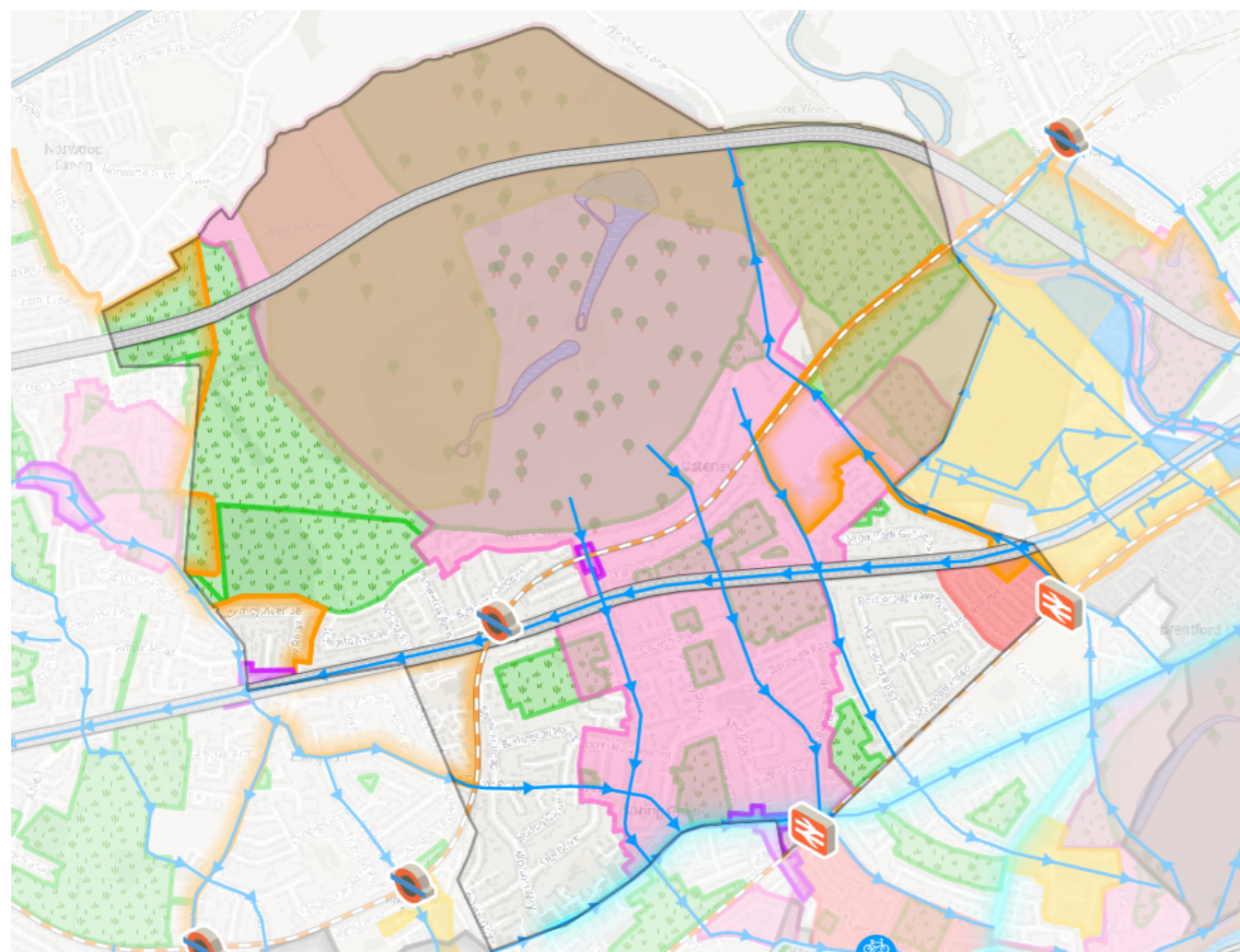
Fig B8.12 Strategic Floor Area Ratio density analysis of existing street blocks - this analysis measures FAR at the street block level rather than plot level and so gives an average estimate of FAR on plots.

B8.16 For a greener, healthier, cleaner, safer, thriving and liveable Borough, Osterley should transition towards:

1. **Celebrating and strengthening the distinctive character and role of shopping parades and small centres in Osterley.** The area does not have a major centre, but instead has smaller centres and shopping parades that are real assets for the community. As the roles of local centres become more important, future growth should help to

enhance, revitalise and diversify uses in these locations to better support local shopping, leisure and community needs. Local centres include the London Road shopping parade by Isleworth train station, at London Road and Spur Road junction, and along Thornbury Road. These should be easy to access by foot or bike and, as such, there is a need to improve walking and cycling access, including cycle parking.

Fig B8.14 Area transitioning vision diagram



2. **Creating green, enjoyable and safe cycling and walking routes between Osterley Park in Osterley and Syon Park and the riverside in Isleworth.** Osterley Park and Syon Park are valued green spaces for local residents and attract visitors from further afield. There are opportunities to promote walking and cycling links between them by improving the environmental quality of routes in Osterley like Wood Lane, Ridgeway Road and Syon Lane. This could be through planting street trees, re-greening front gardens and investing in existing parks and green spaces along these routes like Jersey Gardens. Provision for quiet cycling routes, an improved pedestrian environment and a reduction of the impact of traffic would also support active travel in this area.

3. **Giving greater importance to the area's ancient routes as a focus for new activities, including community and cultural uses.** There are significant opportunities to help enhance the character of London Road through the redevelopment of available sites. These ancient routes are resilient and adaptable and should be the focus for intensification. New development along these routes should be accompanied by public realm enhancements that contribute to greening, promote active travel, and improve the setting of historic assets along these corridors.

4. **Improve greening and the environmental performance of existing buildings.** Although greening is good in the majority of the area, an urban greening priority area has been identified in the eastern section of this area around Syon Lane (see Green Infrastructure Strategy). Developments in the north of this area must specifically contribute to the urban greening priority area identified around Syon Lane. New development should therefore achieve the minimum Urban Greening Factor target (see Urban Greening Factor code in Part

A2) on site and then additionally contribute towards greening in the public realm. The growth diagram for Osterley shows that the sub area should be conserved or enhanced. To enhance sustainability of the existing built environment within Osterley retrofit solutions are necessary. Solutions such as solar panels, air source heat pumps and secondary glazing are encouraged, however must be situated discreetly to avoid visual intrusion. Internal wall insulation, secondary glazing and draught proofing may also provide a solution for home owners to improve environmental performance. Where windows are being replaced it is important that they should match the original windows in design and material.



Fig B8.15 Giving greater importance to London Road as a focus for new activities is a priority for this area

Osterley Park



What's special about Osterley Park?

- B8.17** Osterley Park is within the Osterley Park and Spring Grove Ward. The area is ethnically diverse with a roughly equal proportion of Indian and White British residents, as well as a smaller but notable Pakistani community. The median age of residents is 35 (compared to the Borough-average of 34).
- B8.18** The area's special character is largely defined by the extensive green space reaching beyond the M4 to the north and taking in Osterley Park farm, as well as to the east of Windmill Lane. The jewel in the crown is Osterley Park, a great asset for local residents and visitors alike. It is characterised by the large landscaped grounds and rural setting of the park itself, as well as the historic, Grade I listed Osterley House that sits within it which gives the area its special interest and conservation area status. The Osterley Park conservation area was extended in January 2020. The high brick boundary wall which envelops the park gives a clear edge and enclosed character to the park itself, in contrast with the interwar suburban housing built along its perimeter on spacious plots with generous front gardens and forecourts. The residential pockets are significant too as they were built around the time of the railway and later the Great West Road, and their historic architectural features form the setting of the park.
- B8.19** The shopping parade along Thornbury Road grew up around the old station (opened in the 1880s). The station has since been renamed and

Selection of locally distinctive architectural details and materials which may inform new developments



Roof rhythm Bow windows Roughcast render

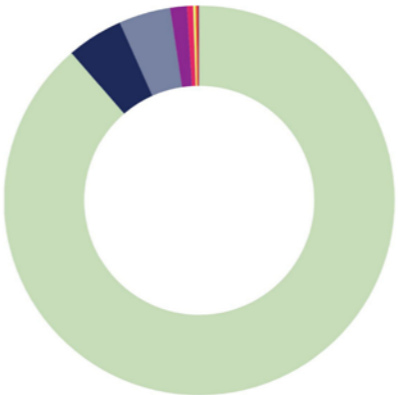
relocated, but the building remains and now houses a popular bookshop. The local parade gives a village-like character, with grocery stores, a pharmacy and cafes. Osterley Park Cafe and Hare and Hounds pub on Windmill Lane are local favourites.

- B8.20** The arrival of the Great West Road in 1925 spurred suburban growth, and later to the north Osterley's own estate land was bisected by the M4 motorway bringing considerable noise impact. Generally however the neighbourhood is characterised by quiet tree lined streets with a high quality environment. The neighbourhood benefits from smaller green spaces too - including Wyke Green and Jersey Gardens - both mentioned by local people as being particularly valuable to this area. Key community assets include Osterley Library, Nishkam School and extensive leisure and recreational facilities.

Prompts for character-led development

- The area sits within a conservation area, and therefore will see minimal change. Great West Road impedes north-south movement for walking and cycling. There is potential to really improve connections across and to green these routes to contribute to the area's ecological network. Development should be mindful of any further loss to boundary walls, and harmful extensions, alterations or accretions which impact negatively on the area's historic rural character. There is an opportunity for shop front improvements and public realm upgrades at the shopping parade on Thornbury Road to bolster this local centre.
- The existing building height range in this neighbourhood is 2-3 storeys approximately and new building heights should also fall within this range to conserve the built character of the area.
- All development sites must make a contribution towards greening in the public realm on streets and corridors in the local area, such as increasing street trees particularly on connections between green infrastructure assets. This will require a considered analysis of existing green infrastructure and a landscape-led response to site design. The

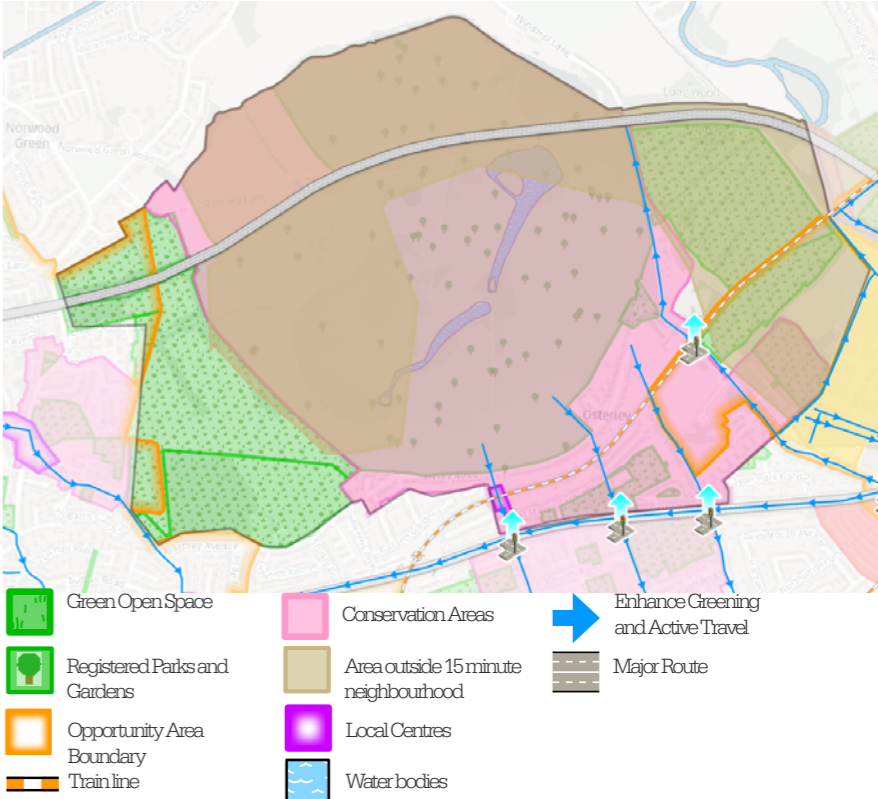
introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.



Neighbourhood typologies

- 89% Green space
- 5% Suburban
- 5% Villa
- 1% Cul-de-sac
- 1% Industry
- 0.2% Parades

Plan of neighbourhood



- Green Open Space
- Registered Parks and Gardens
- Opportunity Area Boundary
- Train line
- Conservation Areas
- Area outside 15 minute neighbourhood
- Local Centres
- Water bodies
- Enhance Greening and Active Travel
- Major Route



"Osterley Park itself is special, the top right side of Wyke Green & Jersey Gardens"
- Local resident

Osterley and Spring Grove



What's special about Osterley & Spring Grove?

- B8.21** Spring Grove developed northwards in the 19th century to meet Osterley, and is a meeting point between Osterley's interwar southern expansion and Isleworth's steadier northern expansion. In 1925 the Great West Road cut through the north end of Spring Grove and today Thornbury Road is the only continuous route linking either side. The area enjoys tree lined streets and a good distribution of local centres and open spaces. Osterley Sports & Athletics Centre is an important leisure facility.
- B8.22** Spring Grove is within the Osterley Park and Spring Grove Ward. The area is ethnically diverse with a roughly equal proportion of Indian and White British residents, as well as a smaller but notable Pakistani community. The ward has the second highest rate of volunteering in the Borough - 69% of local residents have volunteered in the last 12 months (Community Life Survey 2016/17). The median age of residents is 35 (compared to the Borough-average of 34).
- B8.23** There is a concentration of institutional uses and schools here, alongside a really diverse set of residential types including 19th century terraces, inter-war semis, mansion blocks and cul-de-sacs. Much of the area is designated as a conservation area with a high concentration of historic assets to the south including Spring Grove House, on the grounds of West Thames College.

B8.24 Isleworth Crown Court, West Thames College and Spring Grove Primary School are key buildings in the area. The shops on London Road serve local residents and include a Sainsbury's and post office, as well as Ballucci cafe, a popular coffee shop for locals. The area is well connected to modernist, Grade II listed Osterley tube station and Isleworth Railway station to the south. Buses along London Road link the neighbourhood with Brentford, Chiswick, Richmond, Cranford and Feltham. The H28 runs through the neighbourhood linking Osterley with Isleworth.

Prompts for character-led development

- The existing building height range in this neighbourhood is 2-4 storeys approximately. New buildings should be between 2-8 storeys in height, subject to review. Any proposals for tall buildings should refer to the Local Plan.
- Maintain the local emphasis on and importance of Thornbury Road and help to enhance the environment to reflect this historic link route
- Sensitively emphasise London Road and improve its environmental quality through high quality sustainable design and public realm improvements.
- Be sensitive to historic assets and their settings. Improve greening and support local walking and cycling between Osterley Park and Isleworth.
- All development sites must make a contribution towards greening in the public realm on streets and corridors in the local area, such as increasing street trees particularly on connections between green infrastructure assets. This will require a considered analysis of existing green infrastructure and a landscape-

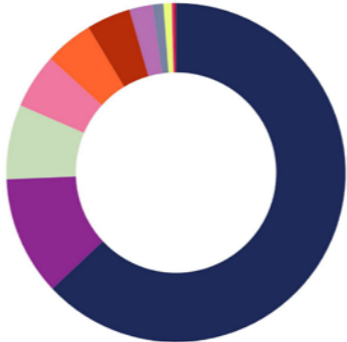
Selection of locally distinctive architectural details and materials which may inform new developments



Roughcast render and white detailing Red brick and white frames Deep front gardens Half timbering Bay windows and reveals London stock brick

led response to site design. The introduction of new green elements must maximise their environmental performance and contribution with particular attention to sustainable drainage and biodiversity.

- Developments must specifically contribute to the urban greening priority area identified around Syon Lane.

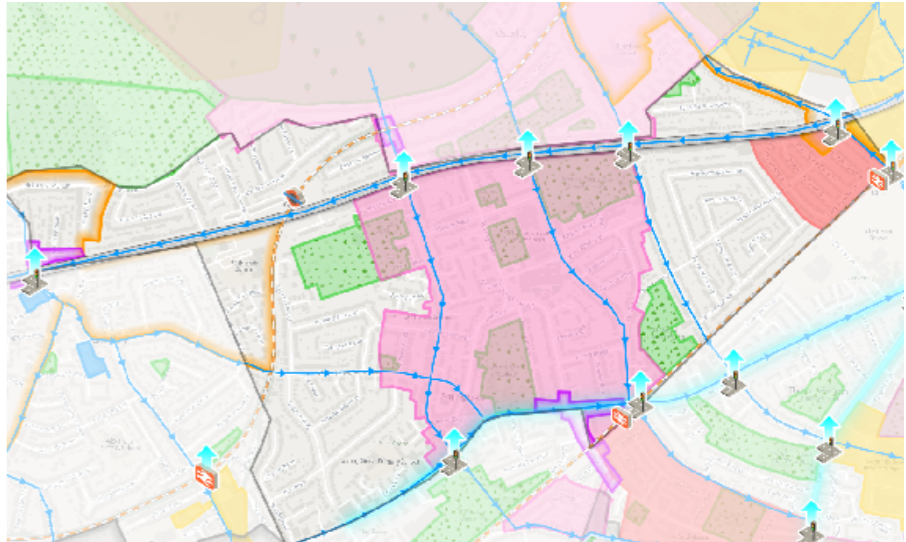


Neighbourhood typologies

- 63% Suburban
- 11% Cul-de-sac
- 7% Green space
- 5% Institutions
- 5% Urban terrace
- 4% Modern urban
- 2% Mansion blocks
- 1% Villa
- 1% Parades

"Beautiful park, good neighbourhood, good public transport, good schools, close to shopping centres"
 - Resident's view on what's special

Plan of neighbourhood



- Green Open Space
- Registered Parks and Gardens
- Opportunity Area Boundary
- Train line
- Conservation Areas
- Areas of Special Character
- Local Centres
- Major Route
- Enhance Greening and Active Travel
- Evolve potential of historic route
- Train Station
- Tube Station
- Junction Improvements



