

May 2024

HOUNSLOW CHARACTER, SUSTAINABILITY AND DESIGN CODES SPD

PART A3 TYPOLOGY DESIGN CODES

 London Borough
of Hounslow

Allies and Morrison
Urban Practitioners

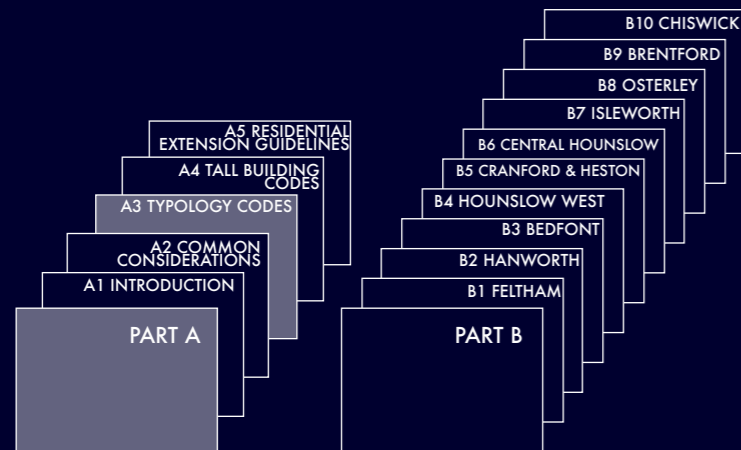
A3 TYPOLOGY DESIGN CODES

Document structure

This Character, Sustainability and Design Codes SPD is comprised of the following parts;

- Part A: Introduction & Design Codes
- Part B: Places

Within each part, there are individual chapters that must be opened separately to view. Use this diagram to help navigate the structure of the Character, Sustainability and Design Codes SPD.



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Introducing typology codes

- A31 There are many different types of site in Hounslow, each with their specific characteristics and considerations. Each site type is known as a typology. Part A2 sets out common considerations and design guidance which applies to all site types and this Part (A3) organises and classifies all potential sites according to one of seven overarching site types which possess typical conditions that recur across the Borough.
- A32 Each site type is introduced with a brief profile on its typical characteristics. Applicants should use this to identify the site type they are working on. In cases where a site shares elements of more than one category, applicants should draw on design codes across all relevant site types.
- A33 A collection of best practice precedents have been assembled for each site type, illustrating how a design response is used in different ways to address constraints and opportunities.
- A34 Design guidance is then set out for each site type under the following headings: access; form of development; relationship to surroundings; street level experience; wider contribution; and climate mitigation and adaptation. These bullet points are supported by a sequence of annotated diagrams used to illustrate key spatial parameters. Each section concludes by considering the development potential of a site type, providing indicative schedules of accommodation and sketch illustrations of how the site could be delivered.
- A35 If the potential development on a site will contain tall buildings, then Part A4 should also be referred to, in addition to Local Plan Policy and the Tall Buildings Study.

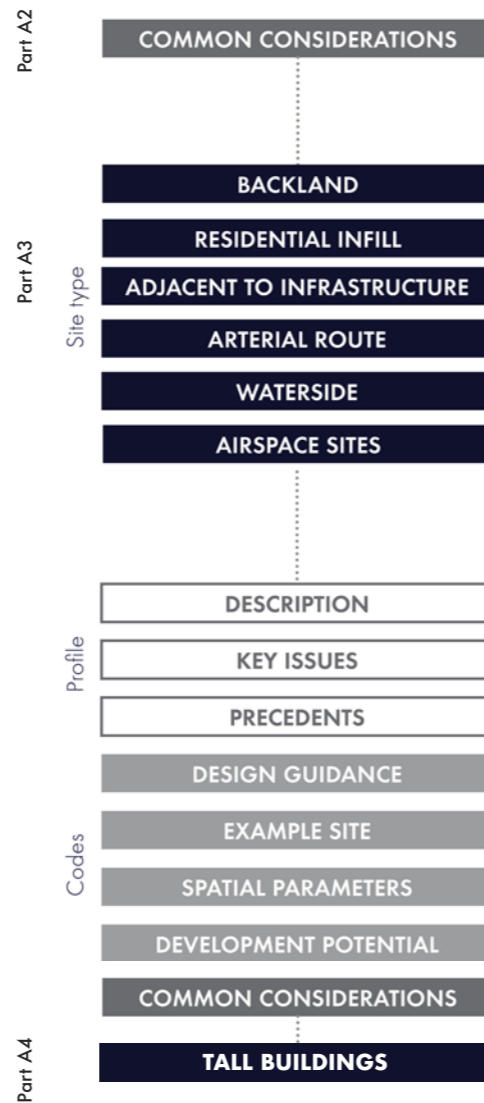


Fig A3.1 Structure of design guidance & codes content

How to use this guidance

- A36 Design guidance and codes have been developed for common considerations and for each site type or typology. The design codes are based on best practice and reflect the Council's aspirations and expectations of good quality design.
- A37 The design codes are not exhaustive and other useful material exists elsewhere - see useful references. The guidance and parameters in this document are based on certain relationships between a site and its surroundings, creating the basis for proposals that are responsive to their setting, protect and enhance local character and amenity.
- A38 The fundamentals of good urban design are established in each site type to allow flexibility for a number of architectural responses to be developed in accordance with the design codes. These build on and complement the Borough design principles established in Part A. In order to demonstrate a design-led approach, applicants are expected to explore a number of options for layouts and building types within the guidance and parameters, in order to identify optimum capacity and resolve a design that is responsive to its local character.
- A39 Design codes and individual design codes should not be adopted in isolation nor do they guarantee planning consent. The guidance and codes have been prepared to add clarity and confidence to good growth. They do not represent a pre-determination of Development Management decisions, with each site and proposal judged on a case-by-case basis.
- A310 All proposals will need to be developed in accordance with the policies in the London Plan, the adopted Local Plan and any emerging policies. Where proposals do not comply with design codes, applicants will need to provide robust justification. Applicants will need to demonstrate how the scheme meets exceptional design standards and does not adversely impact the local character nor compromise the privacy and amenity of neighbouring properties.

Backland

Site description

- A3.11 Backland sites are landlocked sites with limited or no street frontage or access which lend themselves to mews housing or small clusters of idiosyncratic housing development.
- A3.12 Backland sites have been identified across the entire Borough. These sites are typically found in the middle of residential perimeter blocks and serve as garages, builder's yards or left over space.

Key issues

- A3.13 Due to their close proximity to existing dwellings and the potential for functional and visual intrusion into neighbouring amenity space, backland sites require careful design that responds sensitively to the physical constraints of the site.
- A3.14 Backland sites tend to be landlocked; as a result, access for emergency vehicles, refuse and servicing provision, and pedestrians are often key issues on these sites and must be considered at the earliest stage in the planning process. Backland sites also often possess unusual geometries providing a greater scope for architectural innovation.
- A3.15 Amenity space provision is another key issue, as constrained sites can often not provide adequate space. Backland sites are particularly well suited to community-led developments because they can be designed with a unique character and arrangement, and shared spaces often help overcome the constraints of these sites – these factors can help create a sense of community and neighbourliness that often underpins this form of development.



Host area types which are relevant to backland sites



Indication of where backland sites fall within the growth spectrum

Design guidance

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London's Housing Design Standards LPG

Access

- A316 Provide safe and high-quality pedestrian priority access.
- A317 Carefully consider access for emergency vehicles from the start of the planning process.
- A318 Access should be well surfaced, overlooked and lit, with clear definition between footpaths, roads and shared surfaces.
- A319 Publicly accessible developments are preferable to private ones. Gated developments are not supported by the Council.

Form of development

- A320 New developments should respect the form and scale of existing buildings.
- A321 Low to mid-rise buildings are appropriate for this site condition.
- A322 Larger backland sites may be suited to idiosyncratic building types.

Relationship to surroundings

- A323 New development must not compromise the amenity of existing dwellings and should provide sufficient daylight and sunlight to new and surrounding development that is appropriate for its context, while avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- A324 Proposals should respond sensitively to their context through angled fenestration and the careful orientation of new habitable rooms in relation to surrounding buildings.
- A325 Backland sites in Hounslow are typically used for car parking and servicing. Where the character of backland site is calm, verdant and/or biodiverse, it should be protected.

- A326 Where possible, back-to-back gardens should be introduced to repair inactive frontages.
- A327 In circumstances where it is not possible to accommodate all parking on-site, developers are required to undertake assessments to determine on-street parking impact to the satisfaction of the Council. Permits may not be issued to residents if the additional demand would be too great.

Street level experience

- A328 Well-designed and attractive shared spaces should enhance the setting of new homes.
- A329 New pocket spaces, play areas and shared courtyards should be introduced where possible.
- A330 The public realm environment and shared spaces must be of the highest design quality.
- A331 Space for adequate bike storage, refuse collection and, if applicable, car parking, should be integrated into the scheme.

Wider contribution

- A332 The larger the backland site, the more it should contribute positively to the surrounding neighbourhood beyond housing provision.
- A333 Applicants should consider the integration of live-work facilities as part of their proposals.
- A334 If there is an opportunity to improve existing adjacent amenity (e.g. housing estate) this should be taken.

Climate mitigation and adaptation

- A335 Proposals should aim to exceed the minimum Urban Greening Factor standard set out in the London Plan and have regard to the minimum UGF as set out in Part A2.
- A336 In addition and, where possible, there should be no net loss of green cover.



Good practice precedents



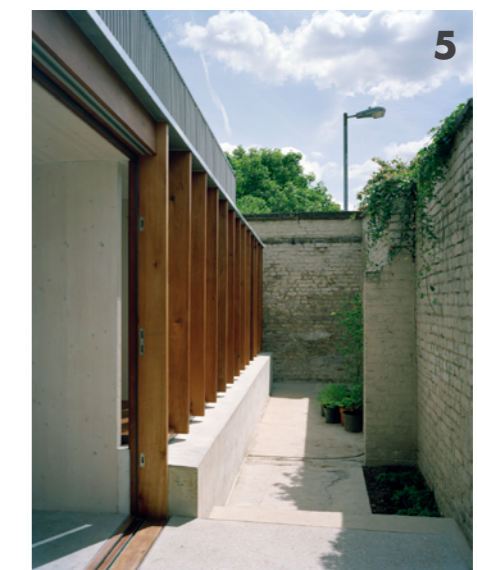
1 Garden House by Hayhurst & Co © Kilian O'Sullivan - Three different roof pitches respond to the scale of the neighbouring buildings. Stepped trays on the roof provide level planting beds which the occupants can actively garden.

2 Copper Lane by Henley Halebrown © Nick Kane - The embodied energy of construction has been considered in recycling waste material from the demolition; timber superstructure; timber cladding; timber fenestration and partial green roofs.

3 Copper Lane © Nick Kane, Henley Halebrown - Community allotments along the perimeter of the site provide green buffers and enhance wellbeing, sustainability and interaction between residents.

4 Ott's Yard by vPPR © Hélène Binet - Two triangular houses make the most of a complex site using geometry, landscape and communal spaces to create a new housing type. Roof lights and windows facing a central courtyard avoid overlooking.

5 Strange House © Hugh Strange Architects - extremely compact development discretely occupying the rear car parking area of terraced houses.



Example site

Development context

A337 This small infill site is currently used as private garages and has vehicular access off multiple roads. Pedestrian access to the garages are also possible via a narrow ginnel. It has good pedestrian/cycling linkages to various stations via nearby underpasses.

A338 A review of the existing structures has shown it is not possible to re-use or adapt the existing garages, however opportunities to re-use the materials on site should form part of the design solution.

Character and identity

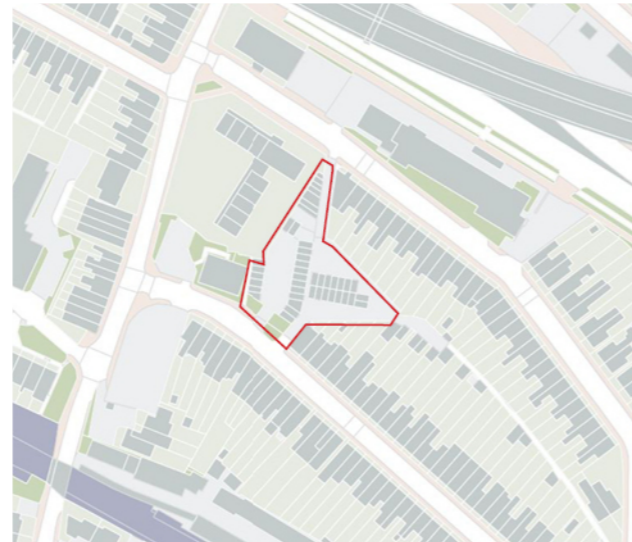
A339 The site occupies the backlands of a residential block of two-storey terraced houses and would typically complete the end of terrace condition. It comprises three informally arranged single-storey garages.

A340 The lawn of the neighbouring housing estate, a free-standing, 4 storey T-shaped block of twentieth-century housing marks the western edge of the garages. The site is on a mild gradient which rises westwards and is also visible from the main road to the west which is elevated considerably.

A341 There are bus stops and a very small retail parade close to the site. Aside from a mature tree and some large hedges at the entrance of the garages, the site lacks any green open space, planting or soft landscaping.

Suitability and sensitivity issues

- Within 3 min walk of a rail station
- Part of an Opportunity Area
- Between two Conservation Areas
- Nearby heritage assets include an historic pub and locally listed station



Location plan

Key information

Ownership	Private
Existing use	Builders Yard (Sui Generis)
Gross areas	0.17 ha
PTAL	2/3
Prevailing height	2 storeys

Suitable land uses

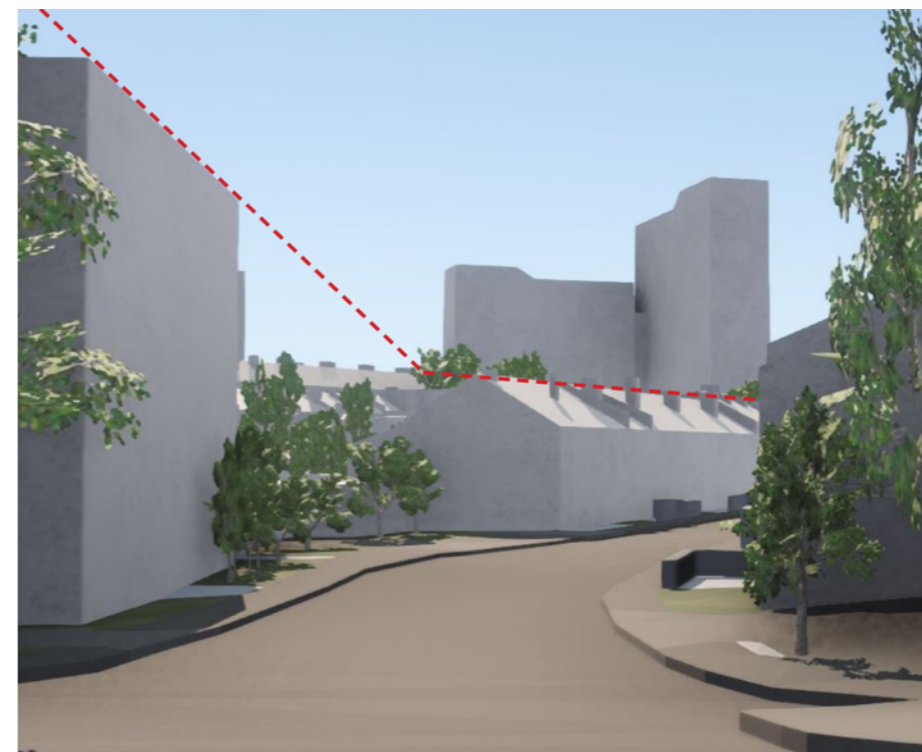
Residential	Yes
Business	Yes - minimal
Retail	No
Community	Yes
Industrial	Yes - light industry

Relevant building types

- Freestanding blocks
- Mews
- Stacked maisonettes
- Short terraces
- Linear blocks



Proposed building heights should not exceed the height of the adjacent estate in order to maintain the cross sectional street profile.



Proposed building heights should mediate between the varying roof heights of adjacent terraces and flats along the road, rising from right to left.



Spatial considerations



1 The development should be subdivided into phases to maximise the functionality of the existing parking and storage facility.



2 The footprint of the backland development should respond to the particular geometry of the site.

The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

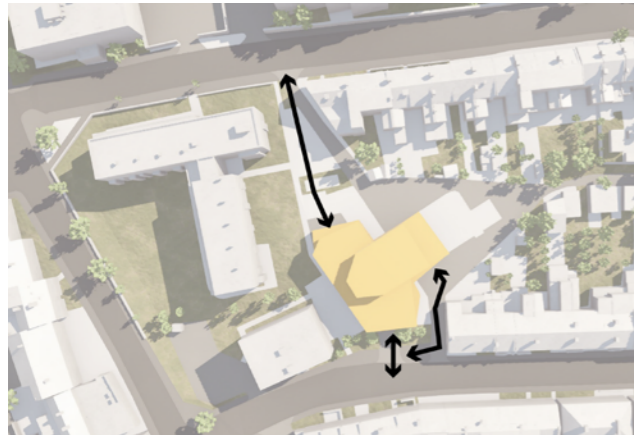
heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



3 Active frontages with community uses (1) at ground floor should be street facing. Residential frontages (2) are more appropriate facing the inner open space.



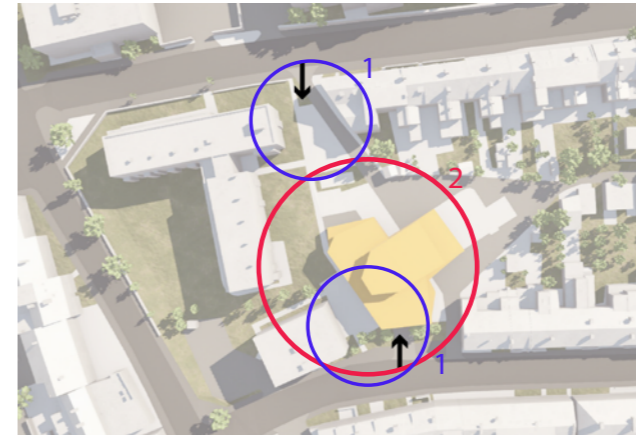
4 Proposed frontages must negotiate between existing building lines, selecting that which is most set back.



5 Existing access points should be retained and, where possible, two-way or multiple in-and-out access points should be provided.



6 The new restricted-access route should provide a link between the development and the existing ginnel leading to the nearby station.



7 Two access points should be provided for deliveries, refuse collection and servicing for the community facility and homes above. Refuse stores should be (1) 15m from the highway and (2) 30m from the dwelling.



8 Building heights should be choreographed to provide visual intricacy. Taller elements are suited to the west to avoid overshadowing the open space in the afternoon.



9 New streets and buildings should repair the inactive residential frontages along the north-west corner with more typical back-to-back arrangements or improved back-to-front configurations.



10 Distances between the habitable rooms of new and existing dwellings should aim for a minimum of 18m. Where this is not possible, visual privacy and overlooking issues can be managed by design to avoid habitable rooms and windows that directly face each other.



11 Urban greening should be maximised via green roofs, green walls and the planting of new trees within the site.



12 Children's play space should be proposed within the site and the landscaping between the lawn surrounding the adjacent estate could be improved.

Development potential

Indicative scheme description

- A342 This site has potential for the development of a single block of dual aspect flats. The irregular form and stepped massing reflects the site's complex geometry and three-dimensional constraints.
- A343 The central open space offers a focal point between the new housing and existing ginnel leading to the nearby station. A new community facility is proposed at ground floor. Proposed building heights range between 1-3 storeys.
- A344 The indicative density reports the Floor Area Ratio (FAR) and Dwellings Per Hectare (DPH). FAR expresses the built density of a scheme and is calculated by dividing the proposed gross floor area by the total site area. Whereas DPH expresses the residential density of a scheme and is calculated by dividing the number of proposed dwellings by the total site area.
- A345 Appropriate FAR/DPH density ranges have been provided as a reasonable benchmark but should be tested further through detailed design.

Indicative schedule of accommodation	
Capacity	
Residential	8 units
Business	N/A
Retail	N/A
Community	760 sqm GEA
Open space	
	250 sqm children's play space
	250 sqm private amenity space
Indicative density	
FAR	0.89
DPH	45
Appropriate density range	
FAR	0.5 - 1.2
DPH	25 - 50
Appropriate building types	
Deck access flats, houses, mews,	
Appropriate building heights	
1 - 3 storeys	



Illustrative scheme showing how the design guidance can be used to maximise the development potential for the site in one scenario. Note: this illustrative scheme does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

Residential infill

Site description

- A3.46 Residential infill sites will typically have active frontages and access from the street. These can be in the form of detached, partially detached or terraced sites which typically back onto rear gardens. These sites are most suited to houses or apartment dwellings.
- A3.47 These infill sites typically are found along main roads, secondary streets within residential neighbourhoods or within housing estates in the form of car parks or left over space.

Key issues

- A3.48 The design must respond sensitively to the street condition, and create a harmonious relationship between new and existing buildings in terms of choice of materials, height, privacy, daylight and sunlight, and access. The relationship between new and existing buildings and the impact on neighbouring properties will need to be carefully resolved, especially where new structures are situated abutting garden boundaries.
- A3.49 Access, circulation, refuse and servicing provision, and amenity space must be carefully considered when accommodating more than one home on the site.

<input type="checkbox"/>	GREEN OPEN SPACE
<input type="checkbox"/>	URBAN CENTRES
<input type="checkbox"/>	LOCAL PARADES
<input type="checkbox"/>	BIG BOX RETAIL
<input type="checkbox"/>	INDUSTRY
<input type="checkbox"/>	INSTITUTIONS / HQ
<input checked="" type="checkbox"/>	VILLA / SPACIOUS
<input checked="" type="checkbox"/>	SUBURBAN
<input checked="" type="checkbox"/>	URBAN TERRACE
<input checked="" type="checkbox"/>	GARDEN CITY
<input type="checkbox"/>	MODERN URBAN
<input checked="" type="checkbox"/>	CUL-DE-SAC
<input type="checkbox"/>	MANSION BLOCKS
<input checked="" type="checkbox"/>	SLAB ESTATES
<input type="checkbox"/>	TOWER

Host area types which are relevant to residential infill sites

conserve reinforce enhance repair transform reimagine



Indication of where residential infill sites fall within the growth spectrum

Design guidance

Access

- A350 Residential access should be along the street frontage, reinforcing the street local hierarchy.
- A351 Arrangement of entrances should complement the rhythm of the street e.g. repetition of front doors in an urban terrace typology.
- A352 Entrances should be well defined and protected by a defensible boundary, using the traditional / prevailing boundary treatment wherever possible.

Form of development

- A353 New developments should respect the form and scale of neighbouring buildings, including alignment of parapets, roof line and form and eaves.
- A354 Sites with a wide frontage should reinstate the prevailing character of the street through facade articulation e.g. bay windows, front doors, detailing.
- A355 Narrow sites could be more suited to innovative, idiosyncratic design that provides a contemporary reinterpretation of traditional elements.
- A356 In areas where building heights vary, accommodating an additional storey above the height of neighbouring properties could be appropriate within the roof form or set back from the street.

Relationship to surroundings

- A357 New development must not compromise the amenity of existing dwellings and should provide sufficient daylight and sunlight to new and surrounding development that is appropriate for its context, while avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- A358 Proposals should respond sensitively to their context through the careful consideration of the existing and appropriate building types.
- A359 Proposals should demonstrate a sympathetic response to the materials of existing buildings

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London's Housing Design Standards LPG

as well as their roof lines, angles and pitch orientation.

- A360 Proposals should repair inactive frontages and secure the block by creating a defensible boundary edge - preventing access to the interior of the block.

Street level experience

- A361 Development should respect the prevailing building line, using a defensible boundary treatment where set back e.g. privet hedge.
- A362 Proposals should demonstrate how facade articulation adds to the character of the street e.g. rhythm of gables, fenestration, front doors.
- A363 Proposals should avoid frontage car parking and ensure high quality cycle parking with street access.
- A364 Space for adequate bike storage, refuse collection and car parking, if applicable, should be well integrated into the scheme (see common considerations for guidelines).

Wider contribution

- A365 This site condition is suited to conserving and enhancing local character and should take account of reinterpreting the detailing of town houses and urban terraces where appropriate.
- A366 If appropriate, the provision of non-residential uses may animate the development at different times of the day. Any such activities should not infringe on the amenity and enjoyment of neighbouring homes.

Climate mitigation and adaptation

- A367 Proposals should aim to exceed the minimum Urban Greening Factor standard set out in the London Plan and have regard to the minimum UGF as set out in Part A2.
- A368 Maximising opportunities for soft landscape treatments in front and rear gardens, as well as shrub planting to create a defensible boundary.



Good practice precedents



1 Hafer Road © Peter Barber Architects - Community-led co-housing project to replace a 1950's council block with new apartments on a quiet residential street in Wandsworth. The composition of the street infill block mirrors the proportions of the existing elevations.

2 Kiln Place © Peter Barber Architects - The proposal creates new, low rise, intimately scaled streets to improve connectivity across the (mostly retained) estate, overlooking of public space and security.

3 Ely Court © Alison Brooks Architects - The infill development of the South Kilburn Estate reinstates the historic block and street pattern of the area.

4 Ely Court © Alison Brooks Architects

5 14 - 17 Calais street, Camberwell, Private developer - Four home residential infill development in a historic conservation area. © Allies and Morrison



Example site

Development context

A369 This deep, rectangular infill site was previously occupied by a row of houses which have now been demolished. The houses formed part of a short terrace along a secondary route.

Character and identity

A370 The site occupies a significant portion of a predominantly low-rise and residential city block with a mixture of two-storey terraces and semi-detached houses and four-storey linear blocks of flats. Existing buildings along the secondary route are typically paired semi-detached houses or terraced houses which maintain this rhythm.

A371 The rear gardens at the heart of the site are abnormally large and represent an inefficient use of urban land.

Suitability and sensitivity issues

- Within six min walk of a rail station
- Within 15 min walk of a town centre
- The site has PTAL 4+
- Within 400m of a park over 0.25 hectares
- Part of an Opportunity Area
- Forms parts of run of semi-detached terraces
- Low-scale residential area



Location plan

Key information

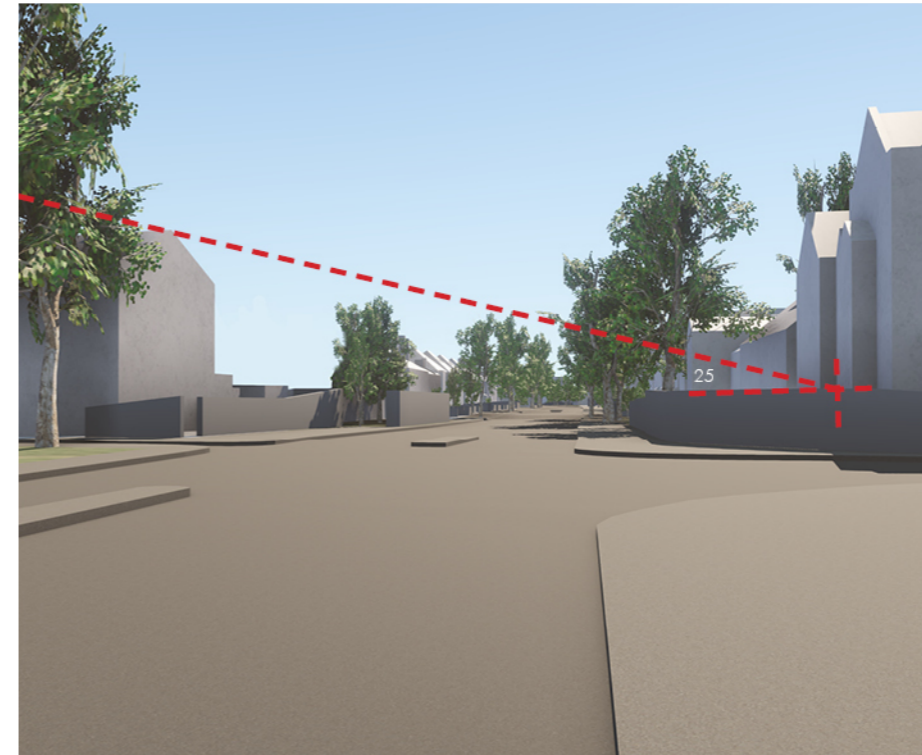
Ownership	Private
Existing use	Derelict houses
Gross areas	0.1 ha
PTAL	2/3
Prevailing height	2 storeys

Suitable land uses

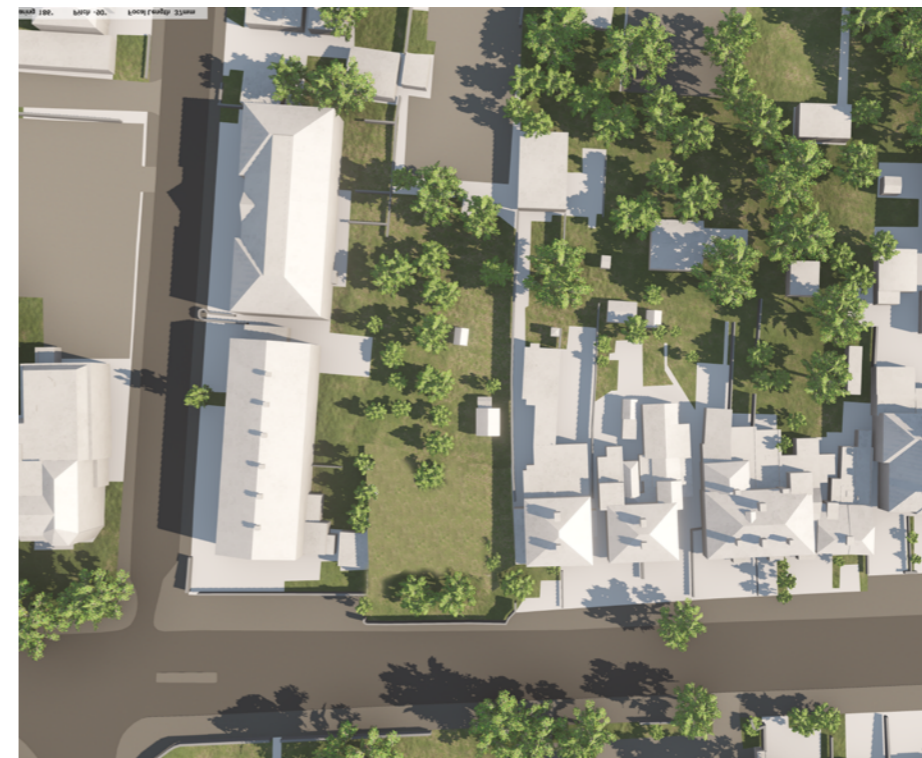
Residential	Yes
Business	Yes - minimal / live-work
Retail	No
Community	No
Industrial	No

Relevant building types

- Detached houses
- Semi-detached houses
- Terraced houses
- Townhouses
- Mews
- Stacked maisonettes
- Linear blocks



Proposed buildings should be subordinate to a 25 degree line taken from the centre of the ground floor windows of existing buildings on the opposite side of the street.



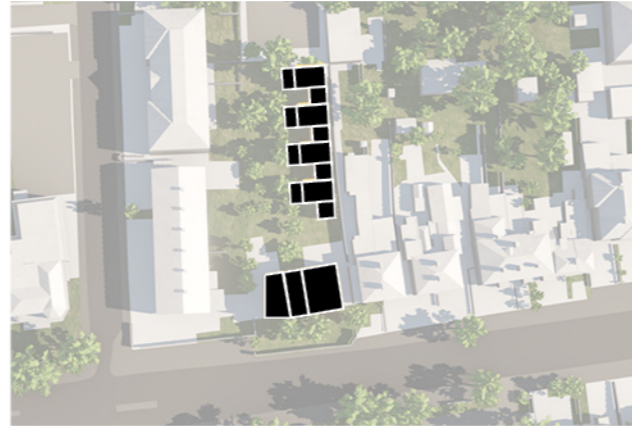
Existing mature trees should be retained and integrated into the site layout and design of outdoor amenity space where possible.



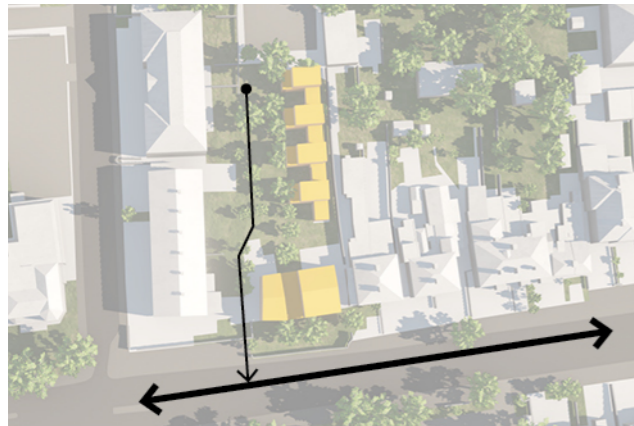
Spatial considerations



1 The street-facing buildings (A) could be developed during an initial phase to minimise disruption to homes within the site (B) which could be built at a later stage.



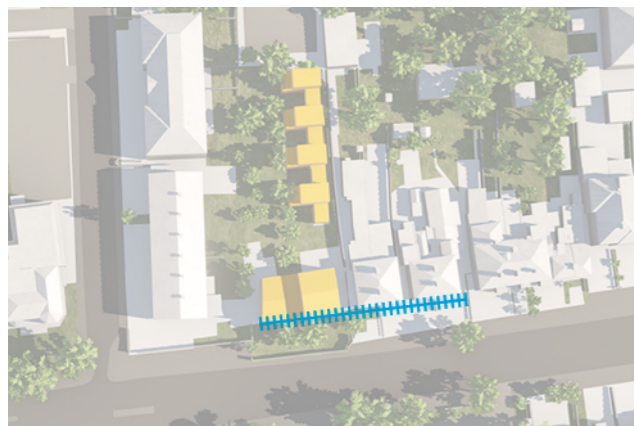
2 The urban grain of proposed buildings should be varied with larger floorplates along secondary roads and a finer grain along tertiary routes.



5 A tertiary street/mews should link the secondary street to buildings deeper within the site.



6 Access to the mews should be restricted to residents only.



9 New buildings should repair the missing frontages by creating improved relationships with adjacent buildings.



10 Distances between the habitable rooms of new and existing dwellings should range between 18-21m. Where this is not possible, visual privacy and overlooking issues can be managed by design to avoid habitable rooms and windows that directly face each other.

The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



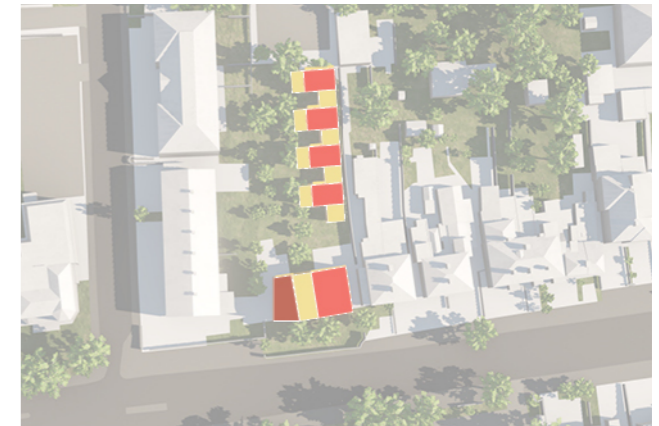
3 Active residential frontages should be outward facing towards the secondary and mews streets.



4 Proposed frontages must extend existing building lines to create continuity along the perimeter of the block.



7 Low blank walls and gable ends should be used strategically to avoid overlooking and overshadowing of existing dwellings.



8 Building heights should be low (1-2 storeys) towards the rear of the site and rise towards the outer edge.



11 Tree planting in new private gardens and along the secondary route should be extended to amplify the green character of the street and block.



12 A dual mews/play street could be proposed within the site alongside private terraces and new private gardens.

Development potential

Indicative scheme description

- A372 This site has potential for two discrete developments. First, a row of stacked maisonettes along the secondary road and then, a short run of mews houses towards the rear. The massing of the stacked maisonettes reflects the rhythm of the existing buildings on the street. The modest scale and form of mews houses maximise the development potential of the site without overlooking or overshadowing existing dwellings.
- A373 The lower level maisonettes benefit from private rear gardens while the mews houses are served by private first floor terraces as well as a tree-lined play street for residents outside.
- A374 The indicative density reports the Floor Area Ratio (FAR) and Dwellings Per Hectare (DPH). FAR expresses the built density of a scheme and is calculated by dividing the proposed gross floor area by the total site area. Whereas DPH expresses the residential density of a scheme and is calculated by dividing the number of proposed dwellings by the total site area.
- A375 Appropriate FAR/DPH density ranges have been provided as a reasonable benchmark but should be tested further through detailed design.

Indicative schedule of accommodation	
Capacity	
Residential	8 units
Business	43 sqm GEA
Retail	N/A
Community	N/A
Open space	
	260 sqm play street
	178 sqm private amenity space
Indicative density	
FAR	0.62
DPH	80
Appropriate density range	
FAR	0.5 - 1.0
DPH	60 - 100
Appropriate building types	
Stacked maisonettes, houses, mews	
Appropriate building heights	
1 - 4 storeys	



Illustrative sketch of one potential development scenario. Note: this sketch does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

Adjacent to infrastructure

Site description

A376 These are sites which are near railways and goods yards but may also comprise business parks, areas of industry or out of town retail locations. Existing buildings in these locations are typically single storey sheds with large areas of surface car parking and outdoor servicing. Major infrastructure sites have been identified mostly at the centre of the Borough on land adjacent to the Overground, Southwestern Railway as well as the District and Piccadilly underground lines. Wherever possible opportunities to re-use the materials on site should form part of the design solution.

Key issues

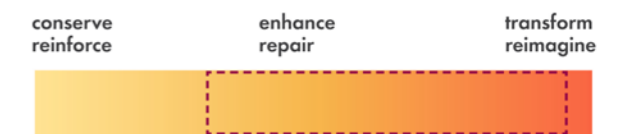
A377 Development on these sites will need to be coordinated with Network Rail / TfL to ensure rail operations are not affected. Applicants need to ensure that minimum required distances are maintained between the development and railway operational area through consultation with the relevant rail provider.

A378 New homes in these locations will need to mitigate vibration, noise and air pollution from industrial or transport-related activities, particularly in relation to living rooms and amenity spaces. A physical buffer between new dwellings and railways/platforms should be provided and overhanging balconies should be avoided.

A379 These sites vary in size and potential capacity, but each have similar constraints, often with limited access and street-facing frontages. Larger sites have the potential for higher density developments. Sites close to future planned infrastructure such as Lionel Road and Golden Mile stations should allow for sufficient public realm provision so that the environs of future stations will not be compromised.



Host area types which are relevant to sites adjacent to infrastructure



Indication of where sites adjacent to infrastructure fall within the growth spectrum

Design guidance

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London's Housing Design Standards LPG

Access

- A380 Access to the development should be welcoming and legible from the surrounding street and kept separate from access to the railway.
- A381 Access for maintenance of the railway should be factored into the design through early consultation with TfL/Network Rail.

Form of development

- A382 Proposals should respond to the form and scale of existing buildings if the character of the surrounding area is consistent.
- A383 Due to physical constraints, reduced visibility from surrounding streets and unusual site geometries, major infrastructure sites are suited to more innovative forms of development.

Relationship to surroundings

- A384 New development must not compromise the amenity of existing dwellings and should provide sufficient daylight and sunlight to new and surrounding development that is appropriate for its context, while avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- A385 Proposals should respond sensitively to their context through the use of angled fenestration and the careful orientation of new habitable rooms in relation to railways/platforms.
- A386 Opportunities to repair blank or inactive frontages should be sought wherever possible.
- A387 The typically peripheral, industrial character of sites adjacent to major infrastructure should be considered early in the design process and opportunities for new connections and improved wayfinding are encouraged.

Street level experience

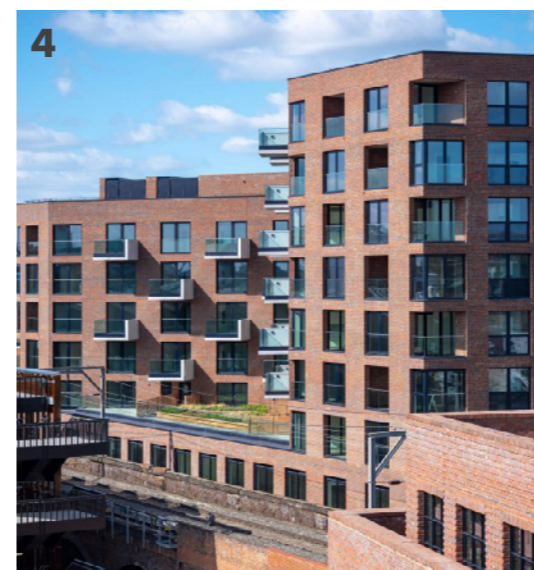
- A388 Recessed first-floor balconies and attractive defensible, street-facing spaces at ground floor will enhance the setting of new homes.
- A389 New pocket spaces and play areas are suitable ways of animating the buffer zone between buildings and railways.
- A390 The proposed public realm environment and shared spaces must be of the highest design quality.
- A391 Space for adequate bike storage, refuse collection and car parking, if applicable, should be integrated into the scheme.

Wider contribution

- A392 Proposals must not compromise access to the railway and should improve access to stations as well as safeguard potential for future access and rail improvements.
- A393 If appropriate, the re-purposing of redundant railway arches for creative, commercial or leisure purposes could produce stronger links between residents and the wider community.
- A394 Land adjacent to major infrastructure, such as railway sidings, often acts as a biodiverse wildlife corridor. Proposals on these sites should not, therefore, negatively impact on these natural habitats and should seek to extend or improve them where possible.

Climate mitigation and adaptation

- A395 Proposals should incorporate recommendations for climate adaptation adjacent to infrastructure land set out in Network Rail's Route Weather Resilience plans and Climate Change Plans 2024 – 2029.
- A396 Opportunities for green walls, SUDs and street tree planting are particularly relevant to this site type.



Good practice precedents



- 1 100 Union Street by AHMM © Tim Soar - A flexible, four-storey office building, introduces an animated frontage with floor to ceiling windows that reference the industrial character of Union Street.
- 2 Westbourne Park Baptist Church by Allies and Morrison © James Newton Photographs - This community-led project manages the transition in scale between buildings either side and provides a new church, family centre, children's library, community hall and affordable apartments.
- 3 Mint Street © Kilian O'Sullivan / Pitman Tozer - A 21st-century reinterpretation of the traditional mansion block providing 67 peaceful apartments on a noisy site next to a railway.
- 4 Hawley Wharf by AHMM © Tim Soar - Recessed balconies and an inset courtyard with projecting balconies ensure the new homes have sufficient private outdoor amenity space.
- 5 Silchester Housing © Haworth Tompkins - reinforces the character of existing residential street patterns, animating corners with community spaces and retail, and creating a new mews alongside the existing railway arches.

Example site

Development context

A3.97 The site comprises an industrial estate and a car park adjacent to a minor train station. It is accessed from a secondary road and is used by multiple businesses operating from two buildings.

Character and identity

A3.98 The site occupies an important section of a B-road which is characterised by free-standing and semi-detached town houses, a station building and a public house. The area is predominantly residential with some employment uses close to the station

A3.99 The prevailing height of nearby buildings is between two and three storeys, often with pitched roofs. To the north is a row of mature trees separating the site from back gardens.

A3.100 Most of the site is covered in asphalt or paving - there are very few mature trees within the red line boundary. To the south is the railway line and platform. A pedestrian footbridge forms the eastern boundary of the site linking it to a nearby park.

Suitability and sensitivity issues

- Within 15 min walk of a town centre
- The site is well served by bus stops
- Within 400m of a local park
- Part of an Opportunity Area
- Potential contamination and historic landfill needs to be addressed as part of any development
- Industrial uses need to be retained on site
- Located within an area of special character
- The train station one other nearby building are locally listed building



Location plan

Key information

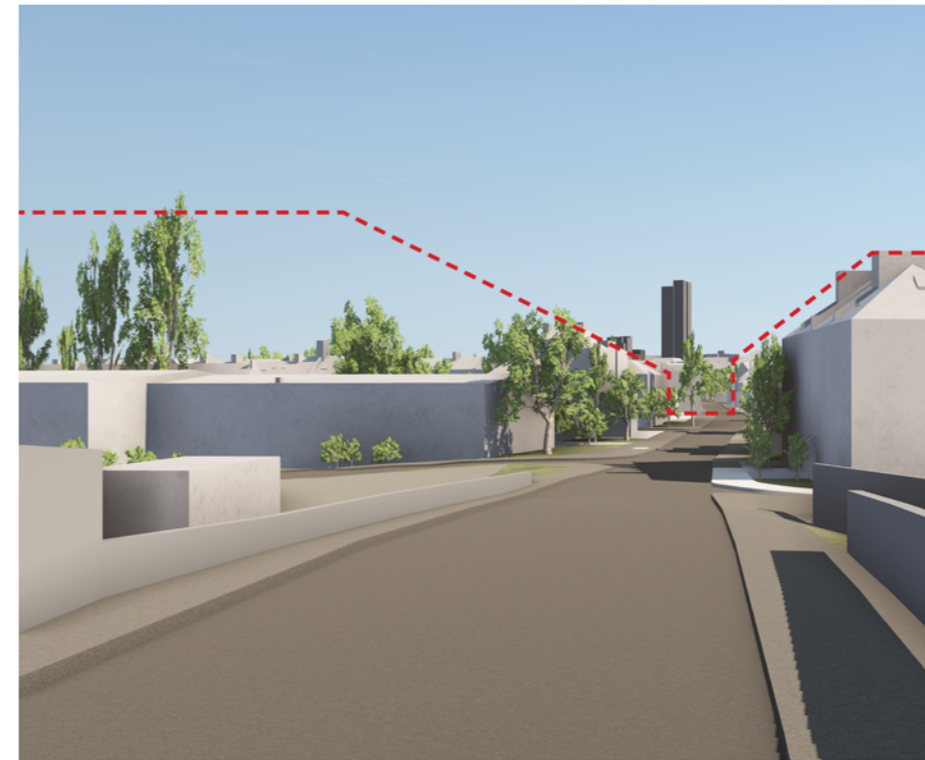
Ownership	Private
Existing use	Industrial Estate
Size	0.80 ha
PTAL	2/3/4/5
Prevailing height	2 storeys

Suitable land uses

Residential Units	Yes - predominantly
Business	No
Community	Yes - minimal
Retail	Yes - along high street
Industrial	Yes

Relevant building types

- Mews
- Stacked maisonettes
- Freestanding blocks
- Linear blocks
- Mansion blocks
- Perimeter blocks
- Taller elements



In this particular case, the prevailing character is of free-standing, detached town houses of predominantly 2-3 storeys, proposed buildings should maintain the cross-sectional profile of the street by not exceeding the roofline of existing buildings along the outer edge of the site.



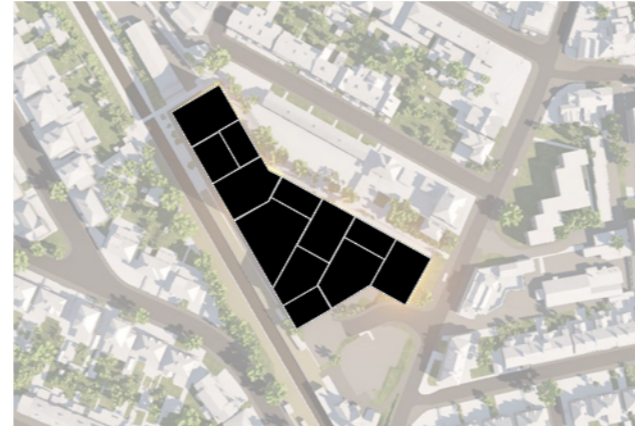
Sites adjacent to transport infrastructure present opportunities to create or, as in this case, repair existing urban blocks by providing continuous development along the perimeter.



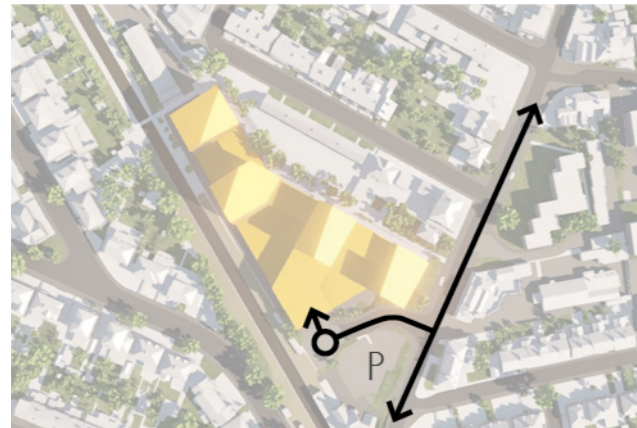
Spatial considerations



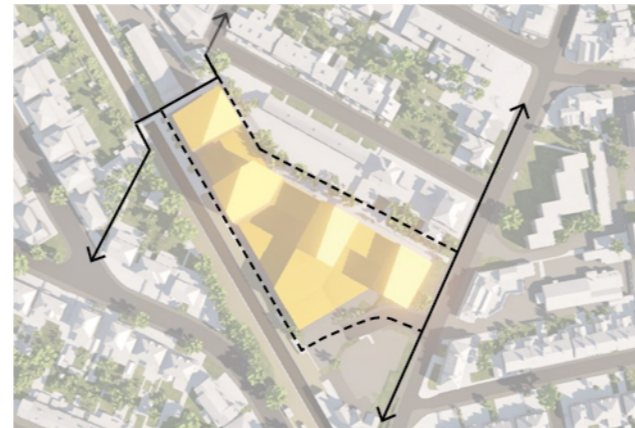
1 The site should be subdivided into short (A) and medium-term phases (B) to maximise the use of the main building. The existing car park (C) could be developed at a later phase.



2 The future grain may be coarser in this location, reflecting the need for ground floor industrial space and the fact that the site is land-locked by the main road and railway line.



5 Residential and servicing access to the site be retained with an improved turning circle for delivery vehicles. The existing car park could be reconfigured for public realm improvements.



6 Continuous pedestrian linkages (dotted) should be created, linking the site to surrounding public routes (solid).



9 Retain a minimum distance (determined by the rail operator) for the safeguarding zone. The boundary between the development and the railway platform should be improved and made more solid to screen new homes from train-related activity. Buffers such as planted walls or art murals may create a better defined and more attractive separation.



10 Distances between the habitable rooms of new and existing dwellings should range between 18-21m. Where this is not possible, visual privacy and overlooking issues can be managed by design to avoid habitable rooms and windows that directly face each other.

The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

TYPOLOGY CODES

heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



3 Active industrial and community frontages should be maximised along the ground floor. Intermittent residential entrances can also enhance street level activity around the site.



4 Proposed buildings lines should align with those of existing buildings. South-facing frontages should accommodate a sufficient buffer from the railway.



7 Proposed buildings should not dominate or overshadow nearby heritage assets. Their material palette should also be sympathetic to the character of historic buildings.



8 Building heights should maximise daylight and minimise overshadowing. Taller elements should be near the centre of the site, stepping down towards the periphery.



11 Urban greening should be maximised via green roofs, green walls and the planting of new trees along the pavements.



12 New, shared public open space and south-facing private amenity spaces should be provided above ground alongside balconies and winter gardens.

Development potential

Indicative scheme description

- A3.101 A large perimeter block containing industrial uses at ground floor with residential apartments above.
- A3.102 The buildings form interlinked horseshoe blocks which are south-facing and organised around private courtyards.
- A3.103 The massing is varied and steps up towards the centre of the site.
- A3.104 Taller elements are to the north and lower ones to the east and west for better sunlight.
- A3.105 Building heights range between two to five storeys.
- A3.106 A new community facility is at ground floor.
- A3.107 Small retail unit located along the main road at ground floor.
- A3.108 The indicative density reports the Floor Area Ratio (FAR) and Dwellings Per Hectare (DPH). FAR expresses the built density of a scheme and is calculated by dividing the proposed gross floor area by the total site area. Whereas DPH expresses the residential density of a scheme and is calculated by dividing the number of proposed dwellings by the total site area.
- A3.109 Appropriate FAR/DPH density ranges have been provided as a reasonable benchmark but should be tested further through detailed design.

Indicative schedule of accommodation

Capacity

Residential	47 units
Business	410 sqm GEA
Community	410 sqm GEA
Industrial	4,800 sqm GEA

Open space

1,190 sqm private amenity space
600 sqm shared public open space

Indicative density

FAR	1.17
DPH	58

Appropriate density range

FAR	1.0 - 3.0
DPH	50 - 120

Appropriate building types

Courtyard blocks and stacked maisonettes with mixed use podia

Appropriate building heights

2 - 6 storeys *any proposals for tall buildings should refer to Local Plan Policy and the Tall Buildings Study.



Illustrative scheme showing how the design guidance can be used to maximise the development potential for the site in one scenario. Note: this illustrative scheme does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

Arterial route

Site description

- A3.110 These are sites along key vehicular corridors such as M4, A4, A316 and the Staines/London Road. These arterial routes can disconnect adjacent neighbourhoods, but the scale of these major highways also presents the opportunity for diversity and intensification alongside the corridor.
- A3.111 Developments affecting arterial roads that form part of the Transport for London Road Network (TLRN) will need to be coordinated with TfL, ensuring that the impact of these construction works on pedestrians, cyclists, public transport and other road users is minimised.

Key issues

- A3.112 The scale of existing buildings along arterial routes varies significantly. New schemes should take account of the surrounding building scale and massing.
- A3.113 Provision for cycle and pedestrian routes should be factored in early in the design process. The delivery of planned improvements should be coordinated with the Council, including for connections across the arterial route between adjacent neighbourhoods.
- A3.114 Continual building lines and frontages through new development will better frame the arterial route and shift the character from that of a thoroughfare to more of a place. The set back and landscaping in front of new buildings along the corridor will improve the arrival experience for residents and soften the route itself. Tree planting will help to establish a boulevard feel and contribute to carbon sequestration.
- A3.115 Though development must take account of excessive noise levels and poor air quality, mechanical ventilation and non-opening windows should not result in poor quality habitable environment or poor façade articulation.



Host area types which are relevant to sites along arterial routes



Indication of where sites along arterial routes fall within the growth spectrum

Design guidance

Access

- A3.116 Existing access and movement arrangements around the site need to be factored into proposals.
- A3.117 Access to non-residential uses at ground floor should be from the primary route, with separate entrances to each retail and/or commercial unit as required.
- A3.118 Multiple entrances (front doors and lobbies) may be required on larger sites and denser developments, distinguishing these between residential and non-residential uses at ground or upper floors.
- A3.119 Proposals including dwellings with rear residential access should take care to make this safe with public realm and lighting, given the arterial route may typically be used for servicing.

Form of development

- A3.120 Site may allow for increases in massing and height if near a transport node but due regard should be given to surrounding character and height.

Relationship to surroundings

- A3.121 New development must not compromise the amenity of existing dwellings and should provide sufficient daylight and sunlight to new and surrounding development that is appropriate for its context, while avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- A3.122 Proposals should respond sensitively to their context through the careful consideration of the existing and appropriate building types.
- A3.123 Corner sites should provide frontage and/or fenestration to the flank wall, to provide natural surveillance and security.

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London's Housing Design Standards LPG

- A3.124 In sites with poor accessibility, building height and density should be limited to limit demand for car parking and amenity space.

Street level experience

- A3.125 Where appropriate in relation to existing building lines, developments should step back from the edge of the pavement to secure an enhanced public realm.
- A3.126 Developments should include landscaped buffer space between road and residential frontages. Water and/or green features should be considered to humanise the entrances.
- A3.127 Residential units should be avoided on ground floor along main roads. This may be mitigated through duplex apartments.
- A3.128 Space for vehicular access should be minimised and parking should be limited to site edges to avoid dominating communal amenity space.
- A3.129 Shared vehicular access routes with pedestrian priority should be proposed where neighbouring sites may come forward.
- A3.130 Active travel should be encouraged through the provision of prominent cycle stores and configuring the site layout for integration into the wider cycle network and clean air routes.

Wider contribution

- A3.131 Delivery, servicing and waste collection strategies should be coordinated with the Highways Authority early in the design process.
- A3.132 This site condition is suited to enhancing and transforming local character and should have due regard to the scale of neighbouring and opposite buildings to enhance local distinctiveness.
- A3.133 Applicants must undertake noise and air quality studies early on to inform their proposals,



Good practice precedents

- 1 South Kilburn Estate © Alison Brooks Architects - mansion terraces and point blocks framing private communal gardens and a pedestrian boulevard at Kilburn Park Road
- 2 333 Kingsland Road © Nick Kane, Henley Halebrown - The project combines a 350-pupil primary school with an apartment building. The relatively compact footprint of the residential tower frees up much of the site for the school and protects it from the noise and fumes from Kingsland Road.
- 3 Camden Courtyards © Sheppard Robson - The site is bounded on two sides by busy roads, so providing an aspect into a calm quiet courtyard ensures better quality outlook for the residential apartments.
- 4 67 Southwark Street © Allies and Morrison - A slender tower works within a compact, triangular site along Southwark Street, using a shift in massing to emphasise the transition in urban grain and street hierarchy. © Nick Guttridge
- 5 Kidderpore Gardens © Allies and Morrison - Mid-rise mansion blocks reflect the scale of adjacent buildings within this Conservation Area along the busy Finchley Road. © Nick Guttridge



not just to mitigate poor environmental conditions once designed. Residential amenity should not be subject to poor quality environmental conditions.

- A3.134 Applicants should demonstrate how proposals can be easily adapted between uses, particularly at ground floor to build in resilience.

Climate mitigation and adaptation

- A3.135 Proposals should avoid vast cross-sectional street profiles to mitigate the risk of the urban heat island effect.

Example site

Development context

A3.136 Currently used as a builder's yard, the site is wide, deep and has an orthogonal geometry. There is an existing dropped kerb and vehicular access off an A-road. The site is well served by nearby bus stops for five routes.

Character and identity

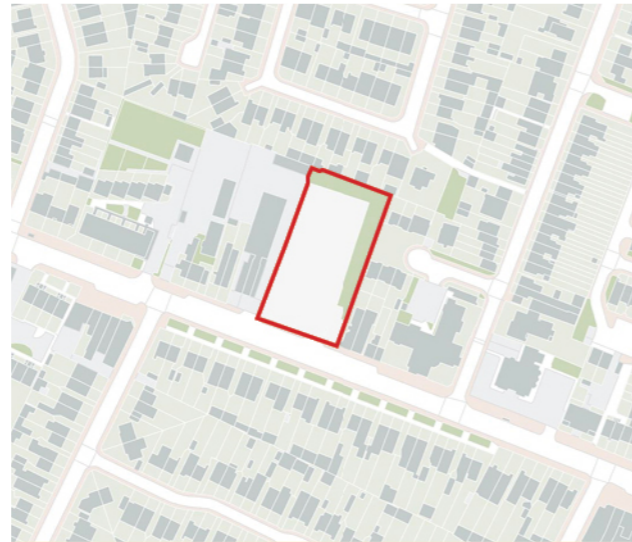
A3.137 Situated along an ancient, arterial route this builder's yard forms part of a short retail and employment parade. It is embedded within a neighbourhood of sizeable, two-storey semi-detached suburban houses near one major and one local centre.

A3.138 More recent developments along this section of the A-road take the form of three to four-storey linear blocks with flat roofs and set-backs. These tend to be set back but also accessed from the main road.

A3.139 The building line either side of the site varies by up to four metres. The north-eastern corner is framed by a row of mature trees which separate the builder's yard from neighbouring side and rear gardens.

Suitability and sensitivity issues

- Within 15 min walk of a town centre
- Within 400m of a large park
- Part of an Opportunity Area
- A small portion of the site is within the BAA Public Safety Zone (PSZ)
- Development needs to comply with Place Policy P4 regarding control of development in PSZs
- Within the 63 dB LAeq 16hrs noise contour of Heathrow Airport



Location plan

Key information

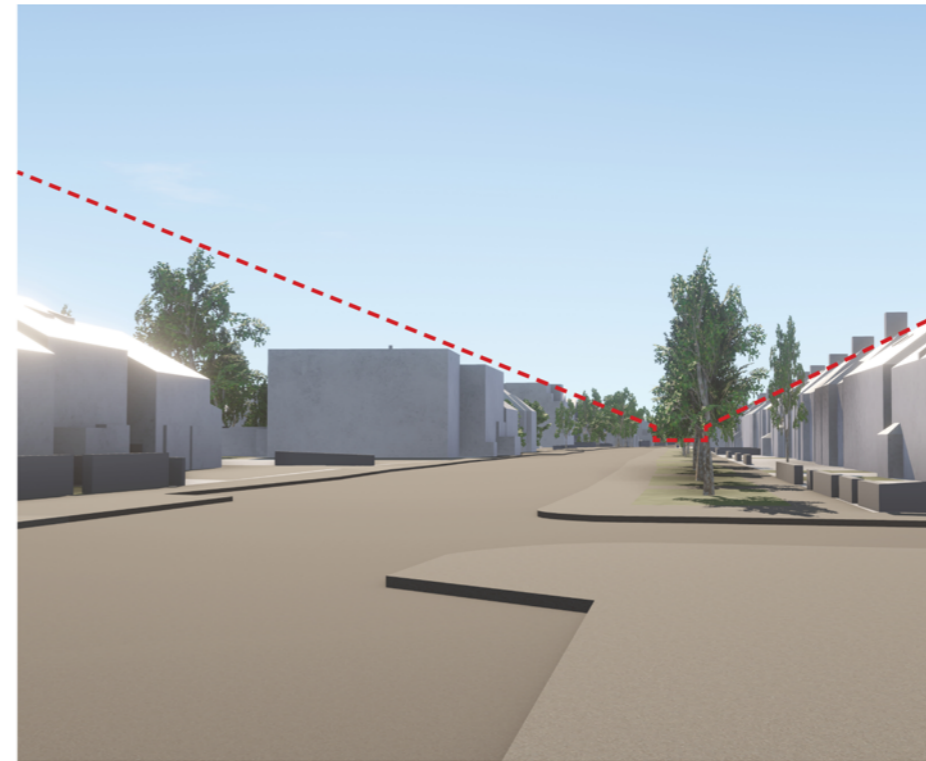
Ownership	Private
Existing use	Builders Yard
Size	0.17 ha
PTAL	2/3
Prevailing height	2 storeys

Suitable land uses

Residential Units	Yes
Business	No
Community	No
Retail	Yes - minimal
Industrial	Yes

Relevant building types

- Semi-detached houses
- Terraced houses
- Mews
- Stacked maisonettes
- Freestanding blocks
- Linear blocks
- Mansion blocks
- Perimeter blocks
- Taller elements



In this particular case, the prevailing character is of two storey semi-detached suburban houses and more recent three to four storey linear blocks, proposed buildings should maintain the cross-sectional profile of the street by not exceeding the roofline of existing buildings along the outer edge of the site.



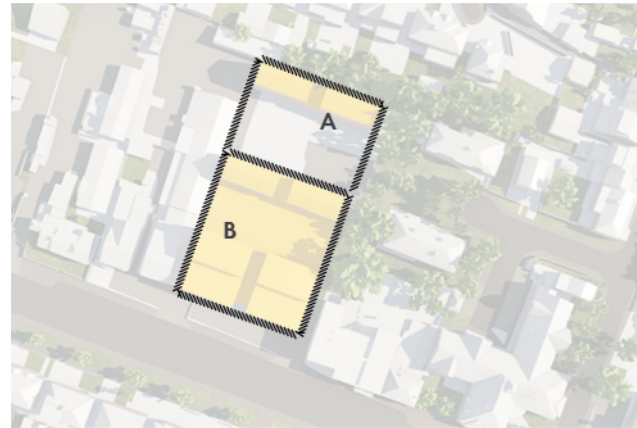
Sites adjacent to along arterial routes present opportunities to create new urban blocks by subdividing or amalgamating fragmented parcels of land, particularly towards the rear of the site.



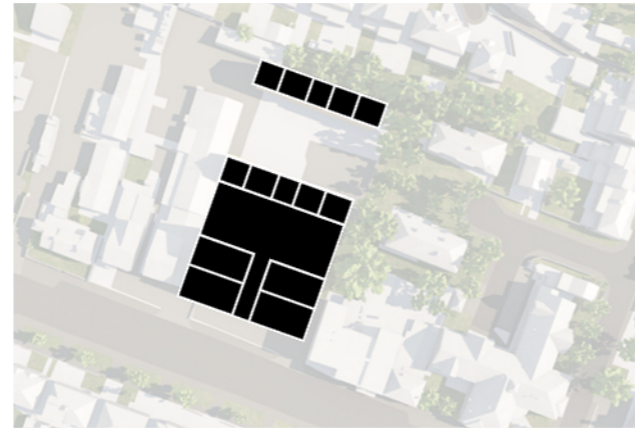
Spatial considerations

The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



1 A phased approach beginning with the redevelopment of the backland (A) should take to maximise the operation of the builder's yard (B) on site.



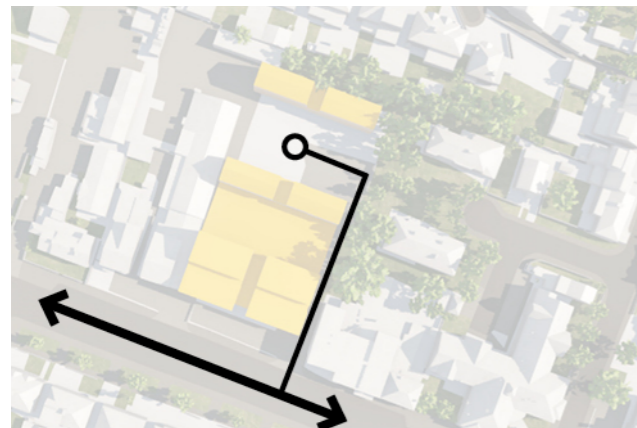
2 The grain of future developments should be varied. Larger floorplates are acceptable along A-roads whereas a finer grain is more appropriate towards the suburban interior.



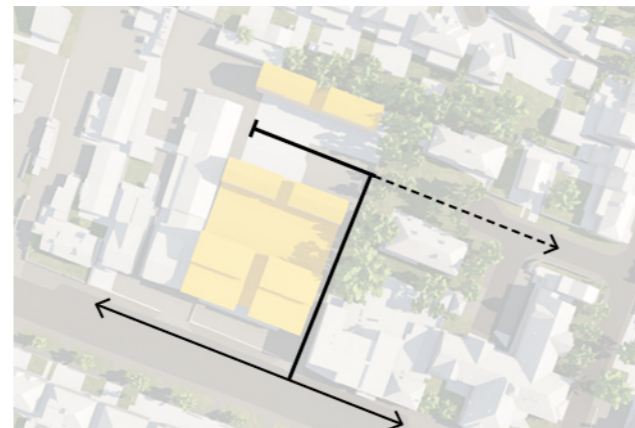
3 Commercial frontages at ground floor should be located along the A-road. Active residential frontages are more appropriate towards the rear of the site.



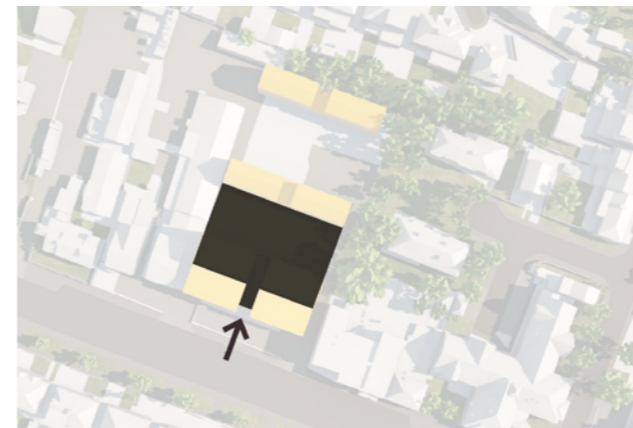
4 Building lines along the A-road should negotiate the shift in pavement width between adjacent buildings. Residential frontages to the rear should align with the adjacent houses.



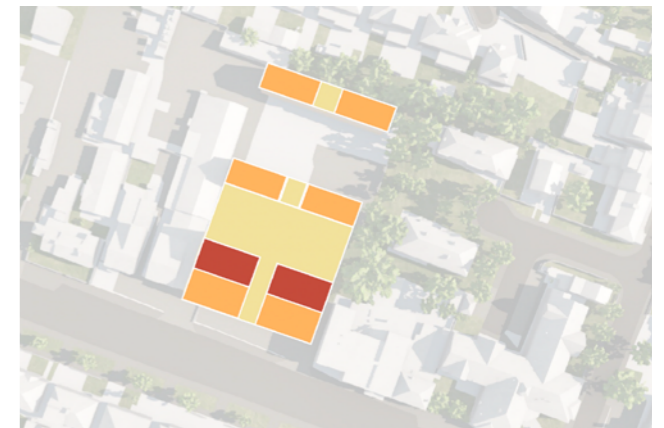
5 Existing vehicular entry points should be retained and extended to allow vehicles to turn around.



6 The potential for an eastward pedestrian linkage to nearby neighbourhoods should be explored.



7 A flexible industrial space could be situated within the residential podium. This space should have a dedicated access and could later be converted into parking or retail storage.



8 The massing should respect the scale of nearby dwellings. Set-backs should be used to prevent new buildings from dominating existing ones.



9 New streets and buildings should repair the inactive residential frontages along the northeast corner with more typical back-to-back arrangements or improved back-to-front configurations.



10 Distances between the habitable rooms of new and existing dwellings should range between 18-21m. Where this is not possible, visual privacy and overlooking issues can be managed by design to avoid habitable rooms and windows that directly face each other.



11 New trees should be planted to mitigate the sound and improve the character of the A-road. Smaller trees towards the rear of the site would help to provide a sense of enclosure.



12 Roof terraces, balconies and shared courtyards above podium level should be proposed as private residential amenity space in the larger block. To the rear, private gardens and a children's play area are more suitable.

Development potential

Indicative scheme description

- A3.140 A large floorplate podium block containing flexible industrial uses at ground floor with mansion houses and stacked maisonettes above.
- A3.141 This building mediates between the arterial road condition to the south and the quieter suburban condition to the north.
- A3.142 The massing of each block reflects the scale and rhythm of nearby semi-detached houses.
- A3.143 Proposed building heights range between 1-4 storeys.
- A3.144 The indicative density reports the Floor Area Ratio (FAR) and Dwellings Per Hectare (DPH). FAR expresses the built density of a scheme and is calculated by dividing the proposed gross floor area by the total site area. Whereas DPH expresses the residential density of a scheme and is calculated by dividing the number of proposed dwellings by the total site area.
- A3.145 Appropriate FAR/DPH density ranges have been provided as a reasonable benchmark but should be tested further through detailed design.

Indicative schedule of accommodation

Capacity

Residential	24 units
Business	N/A
Retail	280 sqm GEA
Industrial	890 sqm GEA
Community	N/A

Open space

230 sqm private amenity space

Indicative density

FAR	0.95
DPH	64

Appropriate density range

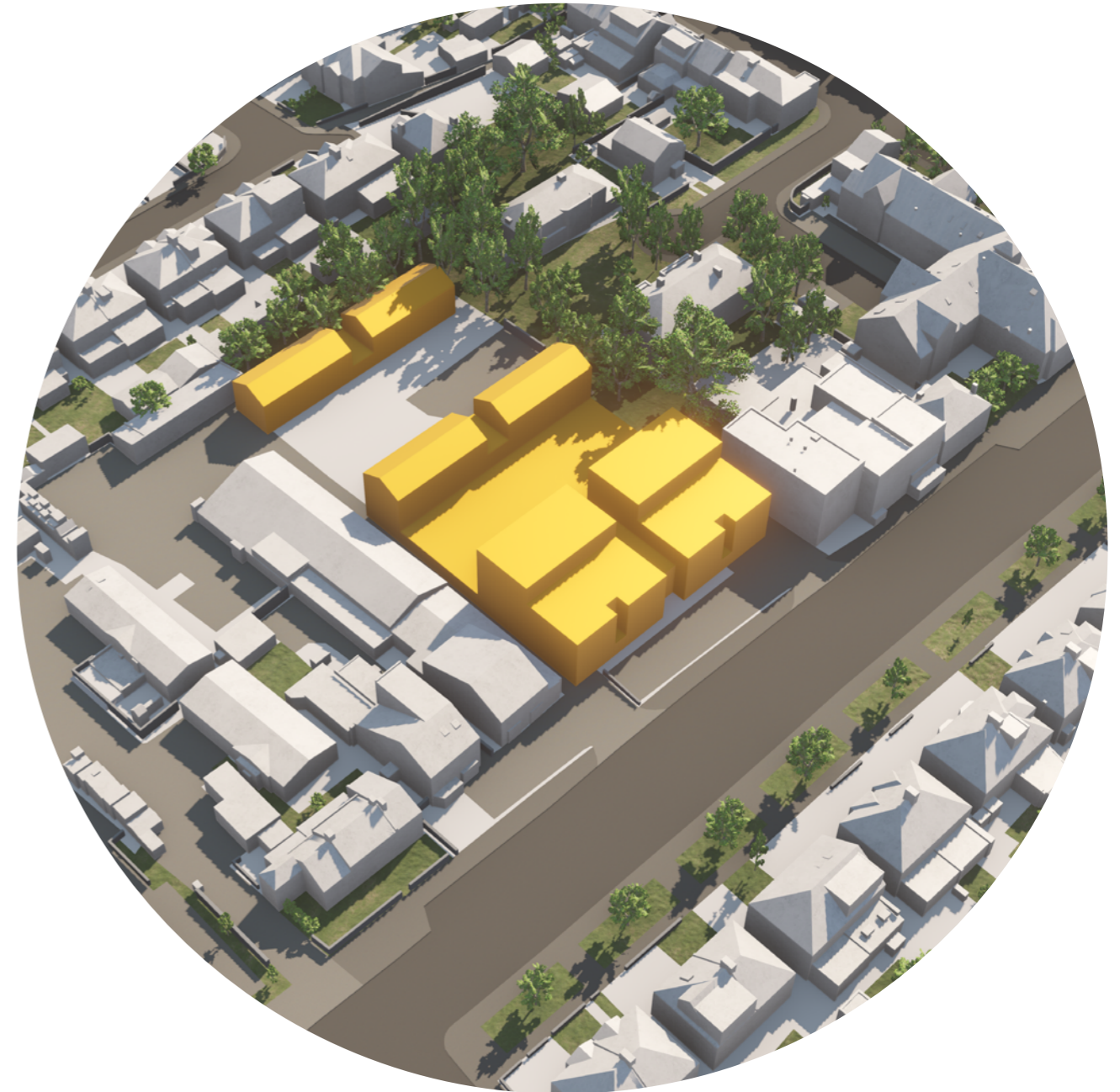
FAR	1.0 - 2.5
DPH	50 - 200

Appropriate building types

Mansion block, linear blocks, deck access flats, stacked maisonettes

Appropriate building heights

2 - 6 storeys *any proposals for tall buildings should refer to Local Plan Policy and the Tall Buildings Study.



Illustrative scheme showing how the design guidance can be used to maximise the development potential for the site in one scenario. Note: this illustrative scheme does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

Waterfront

Site description

A3.146 Waterfront sites are adjacent to bodies of water such as artificial reservoirs, natural lakes or major rivers such as the Thames and lesser ones such as the rivers Crane, Brent, Longford and Duke of Northumberland and canals such as the Grand Union Canal.

Key issues

- A3.147 Applicants will need to carefully consider the existing character of the waterfront - is it urban, industrial, fragmented or natural? And, how does the scheme respond to this?
- A3.148 Hounslow's waterways and water bodies should continue to have a range of characters. Form, material and building typology will influence this. All proposals should establish or enhance pedestrian and cycle access to the waterside through the development, ensuring these are as direct as possible. Where public access to/along the waterway is poor or non-existent, it is especially important that stakeholders set a future vision for how individual sites can contribute to the wider movement network.
- A3.149 Applicants should also consider how public spaces can be integrated alongside the waterside. Trees, planting and integrated seating will be an important part of this.
- A3.150 Front doors should be considered early in the design process - how can street access to homes and the towpath each be presented with positive frontage? Windows and balconies overlooking the water should be maximised to benefit from the natural landscape for residents' health and wellbeing, as well as providing passive surveillance of waterfront paths.
- A3.151 Local and longer views along waterways will need careful consideration.

<input checked="" type="checkbox"/>	GREEN OPEN SPACE
<input type="checkbox"/>	URBAN CENTRES
<input type="checkbox"/>	LOCAL PARADES
<input type="checkbox"/>	BIG BOX RETAIL
<input type="checkbox"/>	INDUSTRY
<input type="checkbox"/>	INSTITUTIONS / HQ
<input checked="" type="checkbox"/>	VILLA / SPACIOUS
<input type="checkbox"/>	SUBURBAN
<input checked="" type="checkbox"/>	URBAN TERRACE
<input type="checkbox"/>	GARDEN CITY
<input type="checkbox"/>	MODERN URBAN
<input checked="" type="checkbox"/>	CUL-DE-SAC
<input type="checkbox"/>	MANSION BLOCKS
<input type="checkbox"/>	SLAB ESTATES
<input type="checkbox"/>	TOWER

Host area types which are relevant to waterfront sites

conserve reinforce enhance repair transform reimagine



Indication of where waterside sites fall within the growth spectrum

Access

A3.152 Public access to the waterfront should be improved by means of legible, safe and welcoming routes. Careful consideration should be given to lighting provision, ensuring that lighting improves the safety of routes, for instance with low-level edge markers, without negatively impacting on ecology. Design guidance in the External Lighting impacts on Biodiversity section in A2 has requirements for external lighting.

A3.153 Establish or enhance pedestrian and cycle access and routes to and along the waterway through any scheme, ensuring these are as direct as possible to enhance riverside connectivity. These should be fully publicly accessible or should allow connectivity should adjacent sites be redeveloped in the future.

Form of development

A3.154 The built form of developments should encourage views towards the waterfront.

A3.155 The scale of development should be sympathetic to the size of the water body.

A3.156 The form and material expression of the new developments should make reference to existing historical buildings along the waterfront.

A3.157 Proposals should carefully consider when existing building lines will be followed as opposed to when set backs will be required to accommodate a towpath and riverside walk as part of future developments.

Relationship to surroundings

A3.158 Waterfront sites are often situated within or near Conservation Areas and heritage assets. As such, proposals should take account of historical buildings and structures as well as key views and sensitivities.

A3.159 New buildings should enhance the setting and amenity of existing buildings through carefully designed massing and by reflecting their scale, proportions or design details to avoid canyoning effects on the waterway.

A3.160 Maximise windows and balconies overlooking the waterway, to take maximum benefit from the natural landscape for residents' health and wellbeing.

A3.161 The layout of developments should be optimised to respond to gradients in the site topography and its impact on local views.

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London's Housing Design Standards LPG

A3.162 Developments should encourage access to waterways and water bodies for better daylight/sunlight, ventilation and outlook.

A3.163 Applicants will need to consider existing uses, such as operational boat yards, at an early stage in the design process to protect existing uses and occupiers.

Street level experience

A3.164 Developments should preserve the sense of enclosure where nearby, existing buildings maintain a close proximity to the water.

A3.165 Front doors along the towpath will encourage an active and positive waterside frontage.

A3.166 Proposal should demonstrate how the safety and privacy of the development can be guaranteed.

Wider contribution

A3.167 Existing public open spaces along the waterfront should be enhanced and new public spaces will be encouraged.

A3.168 Proposal should contribute positively to the amenity value of the waterbody. Applicants should consider the integration of retail, community facilities and public open spaces as part of their proposals.

A3.169 Rivers and water bodies are typically wildlife corridors and habitats for different plant, animal and insect species. Proposals on these sites should not, therefore, negatively impact on these natural habitats and should seek to extend or improve them with a minimum net gain in biodiversity of 10% or greater, where possible.

A3.170 Riverside development should also consider the provision of appropriate riparian lifesaving equipment, on which further guidance can be found at: <https://pla.co.uk/sites/default/files/2024-02/asaferriversidev15.pdf>

Climate mitigation and adaptation

A3.171 Developments should be sufficiently set back from the water and mean high tide of rivers to mitigate the risk of flooding.

A3.172 Sustainable urban drainage and flood barriers will be a key part of the flood resilience strategy.



Any works in, on or over the Tidal Thames would require a River Works Licence (RWL) with the Port of London Authority (PLA). Applicants should make contact at an early stage with the PLA to discuss any such proposals.

Good practice precedents



1 Banham Headquarters by Allies and Morrison © Ståle Eriksen - the scheme which is bounded by a river and a railway embankment organises various buildings in response to both the topography and the architectural context.

2 Ram Quarter by EPR Architecture ©Simon Kennedy - Mixed-use masterplan which has transformed the Young's Brewery site into a new urban quarter that celebrates the river Wandles edge.

3 Lock Keepers by Allies and Morrison © Nick Guttridge - The residential development takes inspiration from the wharf-like, industrial vernacular of the East London context within which it is set.

4 Brentford Lock West © Duggan Morris - The butterfly rooflines, handmade-brick walls and golden balconies of the new homes are all references to the canalside setting of Brentford.

5 Hawley Wharf by AHMM © Tim Soar - The scheme introduces three new public spaces and several new pedestrian routes which reconnect the site and waterside with its neighbourhood.



Example site

Development context

A3.173 The site is between an historic street and a river. It is currently occupied by a large and complex building which is in current use as offices and is no longer fit for purpose.

Character and identity

A3.174 The topography slopes down towards the river. A private road currently provides access to the employee surface car park. The existing office building is set-back from the river unlike nearby buildings. To the north there are more recent developments of four storeys with pitched roofs. Along the eastern edge is a compact, tree-lined street characterised by two-storey buildings.

A3.175 The site is within close proximity of two Grade II listed buildings and a clock tower which can also be seen from the edge of the site. The site is also within close proximity of a locally listed public house along the river.

Suitability and sensitivity issues

- Good cycling accessibility
- Within 400m of a park
- Site already developed and adjacent to mid-rise buildings
- Part of an Archaeological Priority Area
- Within a Conservation Area
- Adjacent to a Site of Importance for Nature Conservation (SINC)
- Site maintains a frontage to a river
- Part of site within Flood Zone 3a



Location plan

Key information

Ownership	Private
Existing use	Office
Size	0.30 ha
PTAL	1b/2
Prevailing height	2 storeys

Suitable land uses

Residential Units	Yes
Business	Yes
Community	Yes
Retail	No
Industrial	No

Relevant building types

- Town houses
- Terraced houses
- Mews
- Stacked maisonettes
- Freestanding blocks
- Linear blocks
- Mansion blocks
- Perimeter blocks



Identify key wayfinding points and pedestrian routes through the site to encourage active travel and better linkages between the river and its surrounding neighbourhoods.



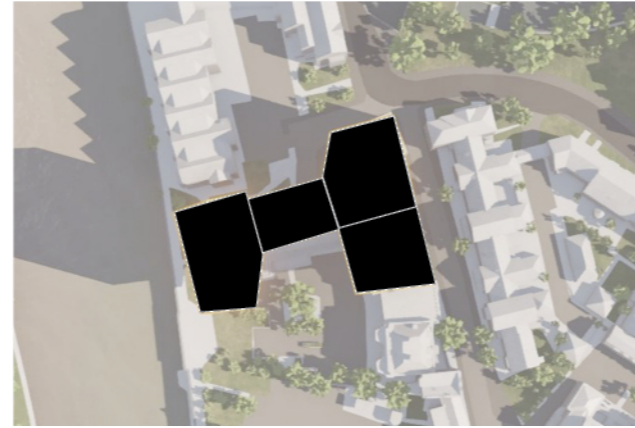
Identify the edge conditions surrounding the site and assign a suitable function and scale of massing for buildings along each edge relating to the role of the surrounding streets, e.g. predominantly residential (pink), community/retail (orange) or commercial (blue)



Spatial considerations



1 A phased demolition would allow one section of the building (B) to remain in use while the first phase of the new development (A) is under construction.



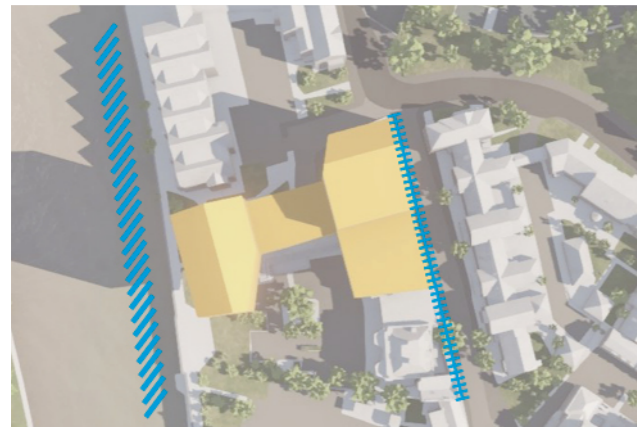
2 The grain should reflect the form of building types along nearby streets and the riverside.



5 Surface car parking should be removed in favour of on-street parking. All other vehicular access arrangements on site should be retained.



6 Public routes (solid) around the site and along the river should be retained and improved. Private pedestrian routes (dotted) should be introduced for access to the courtyard.



9 New buildings and linkages should enhance the aspect and sense of continuity along surrounding streets as well as repair or enhance the character of the waterside public realm.



10 Distances between the habitable rooms of new and existing dwellings should range between 18-21m. Where this is not possible, visual privacy and overlooking issues can be managed by design to avoid habitable rooms and windows that directly face each other.

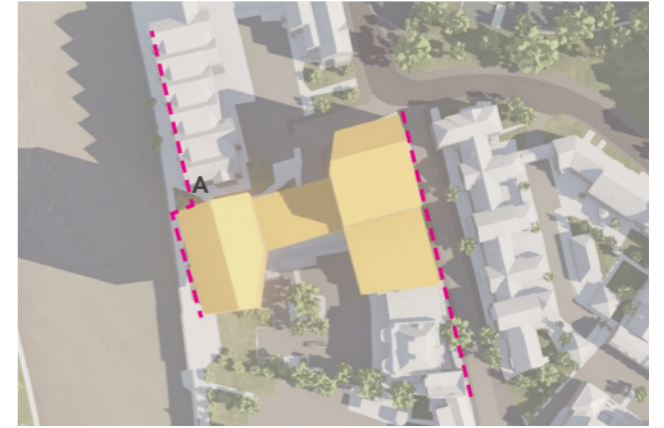
The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

TYPOLOGY CODES

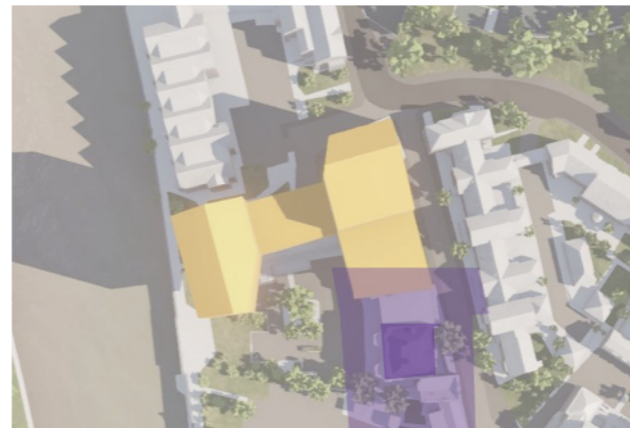
heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



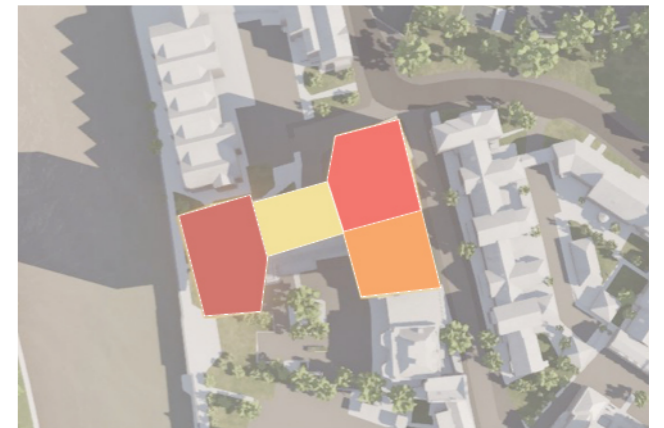
3 Active frontages with commercial uses are suitable along outward streets. Active community and retail (F&B) uses at ground floor are more appropriate along the waterside.



4 Proposed frontages should align with adjacent buildings except along the waterside where the visual prominence of pedestrian linkages (A) is desirable.



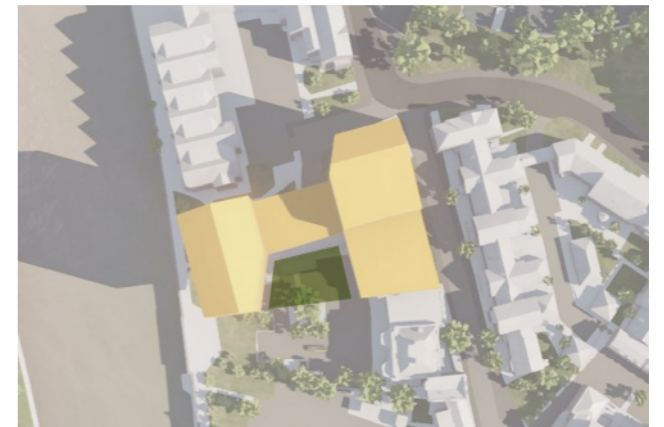
7 Proposed buildings should not dominate or overshadow nearby listed heritage assets. Their material palette should also be sympathetic to the character of nearby historic buildings.



8 Proposed buildings should respect the height of existing buildings. The proposed massing may rise towards the river but not exceed the roof line of nearby waterside buildings.



11 Urban greening should be maximised via green roofs and the planting of new trees in the private courtyard and along the edges of the site.



12 A soft landscaped public courtyard could replace the existing surface car park on site.

Development potential

Indicative scheme description

- A3.176 Two linear blocks interlinked by maisonettes.
- A3.177 The horseshoe arrangement reflects the site's geometry and maximises daylight into the courtyard.
- A3.178 Offices are along Church Street with residential uses along the river and above ground.
- A3.179 Building heights range between 2-5 storeys.
- A3.180 The riverside block references the character of nearby pitched roofs.
- A3.181 A new community facility is proposed at ground floor.
- A3.182 The indicative density reports the Floor Area Ratio (FAR) and Dwellings Per Hectare (DPH). FAR expresses the built density of a scheme and is calculated by dividing the proposed gross floor area by the total site area. Whereas DPH expresses the residential density of a scheme and is calculated by dividing the number of proposed dwellings by the total site area.
- A3.183 Appropriate FAR/DPH density ranges have been provided as a reasonable benchmark but should be tested further through detailed design.

Indicative schedule of accommodation

Capacity

Residential	14 units
Business	2,300 sqm GEA
Retail (F&B)	350 sqm GEA
Community	430 sqm GEA

Open space

280 sqm private amenity space

Indicative density

FAR	0.56
DPH	17

Appropriate density range

FAR	0.25 - 1.5
DPH	10 - 80

Appropriate building types

Perimeter courtyard blocks, mansion blocks, linear blocks, deck access flats

Appropriate building heights

2 - 6 storeys *any proposals for tall buildings should refer to Local Plan Policy and the Tall Buildings Study.



Illustrative scheme showing how the design guidance can be used to maximise the development potential for the site in one scenario. Note: this illustrative scheme does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

Airspace sites

Site description

A3.184 Airspace sites are upwards extensions that can be added to existing buildings to increase existing residential space or create new dwellings. Appropriate sites are located throughout the borough and are often found on primary roads where there is more precedent for increasing storey height. This can often be on corner or end of terrace sites where the prevailing height might step down, lending the opportunity to develop upwards.

A3.185 Some airspace sites are now considered to be permitted development, and as such require an application for the Local Authority's prior approval in respect of design as opposed to full planning permission.

Key issues

A3.186 Airspace development must be sensitive to both neighbouring buildings and host building in terms of materiality, detailing, height, privacy, daylight and sunlight. Applicants may wish to consider modular construction methods to minimise construction disruption on existing residents. Circulation, access and amenity space must be carefully considered with regards to the host building and conflicting uses such as residential and commercial space.

A3.187 As these sites are working with existing buildings they are most suitable to be developed by the existing owner, who may work with an SME developer. In most instances it will also be important to work closely for a suitably experienced building engineer to ensure that any upward extension does not compromise the structural stability of the host building.

<input type="checkbox"/>	GREEN OPEN SPACE
<input checked="" type="checkbox"/>	URBAN CENTRES
<input checked="" type="checkbox"/>	LOCAL PARADES
<input type="checkbox"/>	BIG BOX RETAIL
<input type="checkbox"/>	INDUSTRY
<input checked="" type="checkbox"/>	INSTITUTIONS / HQ
<input checked="" type="checkbox"/>	VILLA / SPACIOUS
<input type="checkbox"/>	SUBURBAN
<input checked="" type="checkbox"/>	URBAN TERRACE
<input type="checkbox"/>	GARDEN CITY
<input checked="" type="checkbox"/>	MODERN URBAN
<input type="checkbox"/>	CUL-DE-SAC
<input checked="" type="checkbox"/>	MANSION BLOCKS
<input checked="" type="checkbox"/>	SLAB ESTATES
<input checked="" type="checkbox"/>	TOWER

Host area types which are relevant to airspace sites

conserve reinforce enhance repair transform reimagine



Indication of where airspace sites fall within the growth spectrum

Design guidance

All new developments must provide suitable internal and external space as set out by the Hounslow Local Plan Policy SC5 as well as take into account the Mayor of London’s Housing Design Standards LPG

Access

- A3.188 Access to additional storeys should be achieved through extending the existing circulation or providing a new separate access core.
- A3.189 In existing residential buildings it is preferable to extend the existing circulation, using a shared entrance and lobby for all dwellings.
- A3.190 Extending the existing circulation is appropriate for roof conversions and subdivisions of existing buildings e.g. urban terraces, townhouses.
- A3.191 Where separate access cores are proposed, these should be concentrated at the rear or side of buildings away from the principal frontages.
- A3.192 Applicants should demonstrate how access from separate cores at ground level is safe and well lit.
- A3.193 It will also be necessary to liaise with building control officers early on to inform the design of necessary fire safety measures and means of escape.

Form of development

- A3.194 Proposals should respond to the scale and roof form of the existing, neighbouring and opposite buildings.
- A3.195 The massing and height of the proposal should be subordinate and complementary to the existing building.
- A3.196 Proposals often work best when set back from the host building envelope, preserving both the street profile and using this variation in massing to distinguish between ‘new’ and ‘old’ and improve sunlight access at street level and/ or adjacent rear gardens..
- A3.197 Being flush with the host building envelope

can be acceptable when this does not negatively impact the street profile and is distinguished from the existing building in other ways e.g. materials, fenestration, palette etc.

- A3.198 Setbacks should provide a well-resolved massing from all view points and not result in overcomplicated volumes that can detract from the integrity of the original building.
- A3.199 Proposals will abut party walls unless there is scope for open amenity space.

Relationship to surroundings

- A3.200 New development must not compromise the amenity of existing dwellings and should provide sufficient daylight and sunlight to new and surrounding development that is appropriate for its context, while avoiding overheating, minimising overshadowing and maximising the usability of outside amenity space.
- A3.201 Proposals should respond sensitively to their context through the careful consideration of the existing and appropriate building types.
- A3.202 Corner sites should provide frontage and/or fenestration to the flank wall, to provide natural surveillance and security.
- A3.203 In sites with poor accessibility, building height and density should be limited to limit demand for car parking and amenity space.
- A3.204 Proposals should consider the use of modular construction methods to reduce construction time and minimise the impact of construction on existing residents.
- A3.205 Proposals should demonstrate a sympathetic response to the materials of existing buildings as well as their roof lines, angles and pitch orientation.

- A3.206 New developments may provide internal connections to existing buildings if these do not compromise access and circulation arrangements.

Street level experience

- A3.207 Where the street ratio and public realm experience may be negatively impacted, proposals should set back massing from the original building envelope.
- A3.208 Applicants should demonstrate how the local microclimate is not negatively impacted, particularly wind and overshadowing.
- A3.209 Space for adequate bike storage, refuse collection and car parking, if applicable, should be integrated into the scheme.
- A3.210 New residential entrances at ground floor should be welcoming and clearly visible, particularly if lower floors occupy non-residential uses.

Wider contribution

- A3.211 This site condition is suited to repairing and re-imagining local character through high quality, innovative design, and should be used to help mediate between building heights, as well as well as proposing massing, materials and façades that positively contrast with the existing building.
- A3.212 If the site is within an estate, applicants should demonstrate how the development would provide wider contributions to the estate.
- A3.213 Applicants should consider the integration of community facilities as part of their proposals.

Climate mitigation and adaptation

- A3.214 Proposals should aim to exceed the minimum Urban Greening Factor standard set out in the London Plan, maximising opportunities for green roofs.

- A3.215 House extensions and new dwellings delivered through this form of development should aim to achieve the highest possible energy efficiency ratings, taking a fabric-first approach.

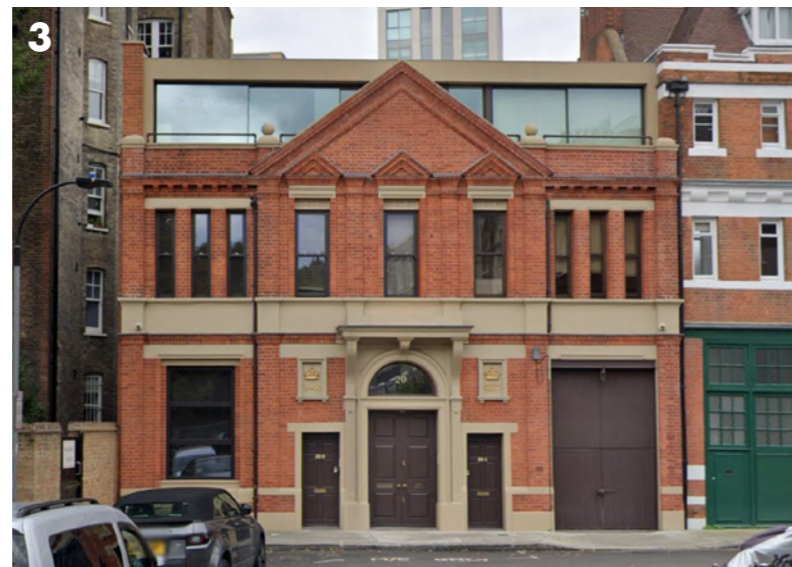
Relationship to host typologies

- A3.216 These sites make use of existing buildings to provide new homes in accessible locations, plus, present opportunity to evolve local character. The approach to massing, materials and fenestration should positively contrast with the existing host building . Set-back massing and roof form should avoid being overcomplicated, detracting from the original building.
- A3.217 Airspace extensions should reference key architectural characteristics from the existing building such as complimentary colour or material palette and vertical and horizontal facade rhythms.
- A3.218 When there are a variety of building designs, proposals should reflect that of the host to avoid creating a fragmented composition and retain continuity of the streetscape.
- A3.219 When positively resolved, introducing an innovative roof form, materiality and design can positively contrast with the host building, enhancing local character and distinctiveness.
- A3.220 Consideration should also be given to removing or altering existing design features which would have previously denoted the transition from building façade to roof form, such as corbelled brickwork or parapets/cornice detailing.

Good practice precedents



1 John Bush House, Isleworth © Hunters - Two storey extension over a former 1980s office building, featuring a set-back, balconies and flat roof overhang, with materials inspired by the original brick



- 2 Hanover House, Bradford © Kraus Schoenberg Architects - Lively roof landscape that reinterprets surrounding roof forms, bay windows and dormers, contrasting but complementing the 19th C host building
- 3 20 Avonmore Road, West Kensington by GSV © Allies and Morrison - Single storey extension over a former post office building, materials matching the existing building and large glazed openings providing a backdrop for the tympanum
- 4 58 Canning Road - Roof extension on a corner site, where the innovative design positively contrasts with the host Victorian terrace and respects the scale, form and character of the surroundings.
- 5 Jerwood Space, Southwark by Munkenbeck + Partners Architects © Dennis Gilbert/VIEW - Upwards extension appears lightweight in comparison to the main building through its contrasting materiality and slight set back.



- 6 Roof extension, Oval, Private developer © Allies and Morrison - Private development of airspace extension repairing the corner condition to meet the existing height.
- 7&8 Mile End East Estate © ritchie*studio - Development of 142 new homes through 2 storey roof extensions across the estate for Gracewood and Eastend Homes. The design maintains the vertical articulation and form, as well as providing a sustainable approach to intensification.
- 9 Roof extension, Shoreditch, Duggan Morris Architects © Jack Hobhouse - Retains 3 storey brick warehouse, with additional 3 storey lightweight contemporary roof top extension. Storey heights and 3 bay brick facade articulation remain consistent. Staggered terraces allow light to filter into the space at different levels.
- 10 & 11 53 Great Suffolk Street © Hawkins/Brown - The refurbishment and extension is sympathetic to the existing building's shape and form as well as creating a contemporary and contextual composition. The roof extension does not detract from the historic fabric as it is partially hidden from the gable ended structure.



Example site

Development context

A3221 The three storey purpose built residential block was constructed as part of the late 1960s estate. The site is eligible for upwards extension under permitted development, allowing the addition of two storeys. There garage storage to the rear.

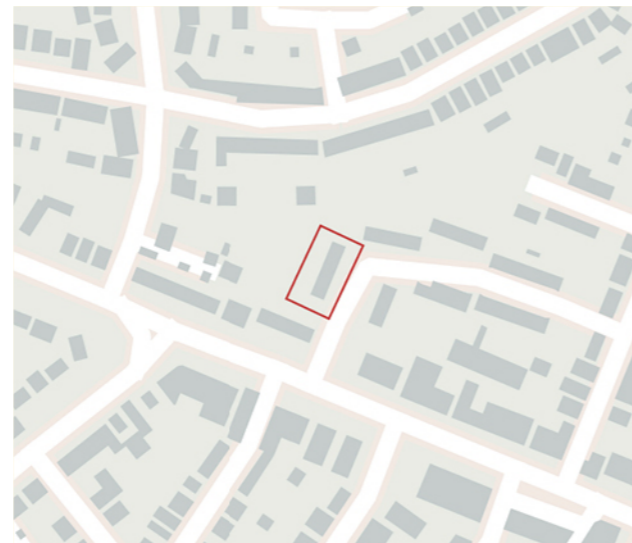
Character and identity

A3222 The site sits in a predominantly residential area, and is accessed via a local road connected to an arterial route. The site is a short distance from a neighbourhood centre. Buildings in proximity to the site are linear blocks, matching the site typology. Other buildings on the estate are two storey terraced houses.

A3223 The prevailing height of nearby buildings is three storeys, with some two storey buildings further afield; all have flat roofs. There are a number of mature trees situated within the green spaces around the site and the wider estate. Some amenity space is situated to the rear of the site, in addition to a number of garages.

Suitability and sensitivity issues

- Within 5 minutes walk of a local centre
- A number of existing amenity spaces serve the existing homes
- Structural conditions of the existing building should be addressed in any development
- Poor PTAL (1a), however the site is within walking distance of a bus stop with services to Hounslow, Hanworth, Ashford and Hatton Cross



Location plan

Key information

Ownership	Public
Existing use	Residential
Size	0.14 ha
PTAL	1a
Prevailing height	3 storeys

Suitable land uses

Residential Units	Yes
Business	No
Community	Yes
Retail	No
Industrial	No



Proposed upwards extensions should provide wider contributions to the estate, including upgrades to open space or pedestrian routes.



Proposals should respond to the scale of the existing buildings, introducing incremental height that has a minimal impact on the cross sectional profile of the street.



Spatial considerations



1 The upward extension roof form responds to the flat roofs of the existing, neighbouring development.



2 The upward extension is set back to preserve the street profile and to distinguish new and old. The massing and height is subordinate to the existing buildings.



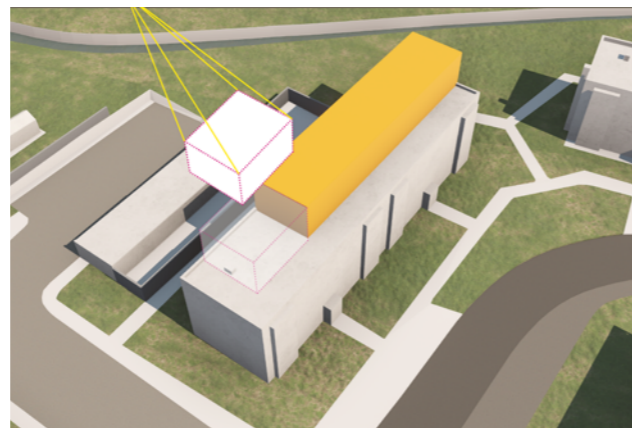
5 The setback area is used to provide balcony amenity space for the new dwellings.



6 Access to the additional storey is achieved through extending the existing circulation.



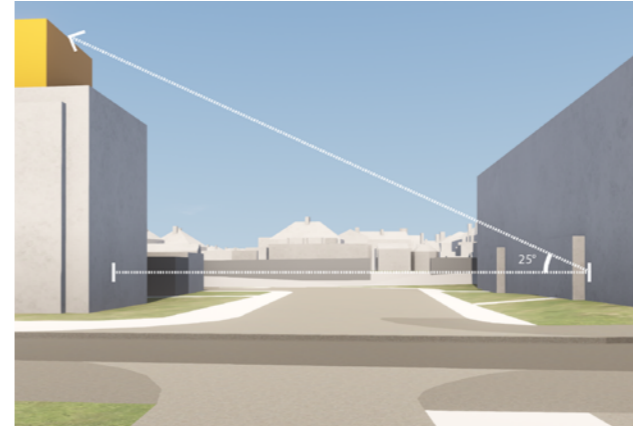
9 The proposal responds to the climate emergency by adding a new green roof.



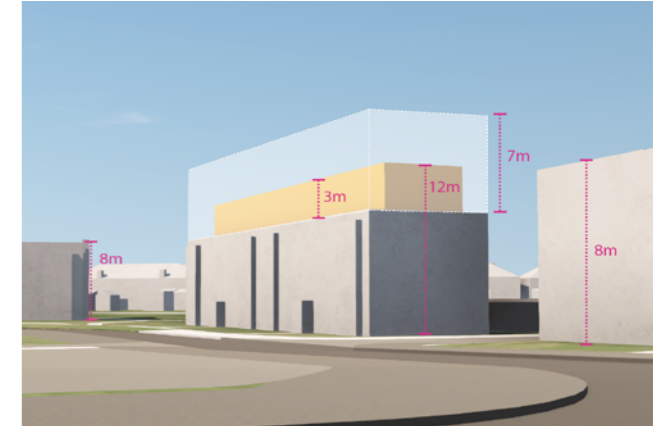
10 Modular construction methods are used to reduce construction time and minimise the impact of construction on existing residents.

The design codes below illustrate twelve key urban design considerations that applicants should consider relating to: phasing, urban grain, frontages, building lines, vehicular and pedestrian routes, access, building heights,

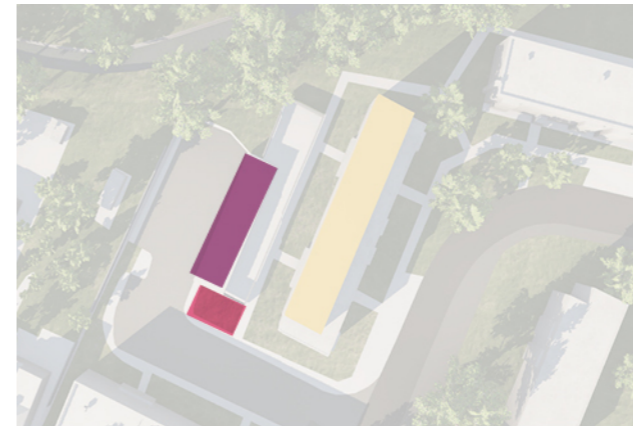
heritage, set-backs, urban greening and open space amenities. These codes are not exhaustive. Applicants are advised to review these alongside the relevant site specific design guidance.



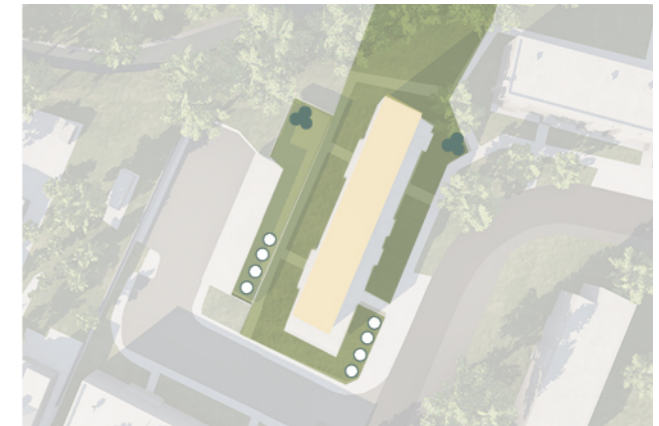
3 The upwards extension complies with the 25° rule and so does not compromise the amenity of existing dwellings or adversely impact levels of daylight or privacy.



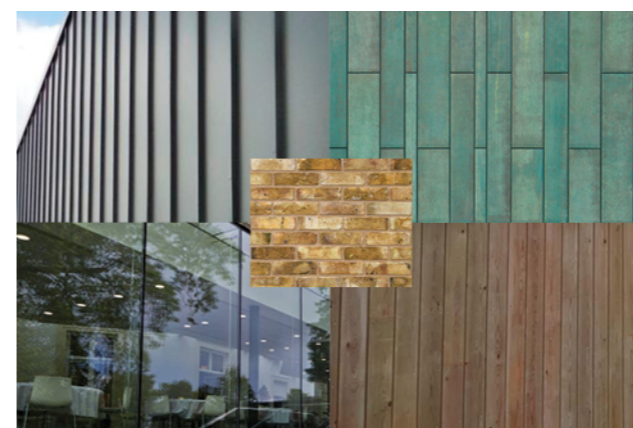
4 Although PD rights allow the addition of 2 storeys and an additional 7m (detached, purpose built blocks of flats), the proposal responds to the scale of the neighbouring buildings.



7 New cycle storage is proposed and existing parking arrangements are re-configured to accommodate the new dwellings.



8 The upwards extension proposal sets out improvements to the existing amenity space, such as quality planting, food growing space and play areas.



11 Sustainable materials that contrast, but complement, with the existing building could be used to clearly distinguish the upwards extension from the existing building.



12 A flat roof overhang is added to add visual interest to the proposed extension. Details should respond to the original building.

Development potential

Indicative scheme description

- A3224 A one-storey upwards extension, adding a 4th storey to the existing residential block, containing residential apartments.
- A3225 The extension is set back from the existing building in order to maintain residential amenity for nearby dwellings, distinguish the extension from the existing building and to maintain the street profile.
- A3226 A flat roof is used to complement the roof form of the existing and neighbouring buildings.
- A3227 Amenity space is provided in the form of balconies for the new residential apartments.
- A3228 High-quality sustainable materials are used that contrast with the existing building, but complement the existing materiality.
- A3229 Detailing is considered in the proposal in order to complement the host building.
- A3230 The host building is considered as part of a wider site and therefore upgrades are made to the existing green space and parking arrangements. Cycle storage is added to accommodate the existing and new dwellings.
- A3231 A green roof is added in order to improve the sustainability credentials of the host building.

Indicative schedule of accommodation

Capacity

Residential	3 units
Business	N/A
Retail	N/A
Industrial	N/A
Community	N/A

Open space

115 sqm private amenity space (balconies)

Indicative density (extension)

FAR	0.14
DPH	22

Appropriate density range (extension)

FAR	0.1 - 0.25
DPH	10 - 50

Appropriate building types

Flat roof upwards extension

Appropriate building heights (extension)

1 additional storey



Illustrative scheme showing how the design guidance can be used to maximise the development potential for the site in one scenario. Note: this illustrative scheme does not represent a pre-determination of Development Management decisions. Each site and proposal will ultimately be judged on a case-by-case basis.

